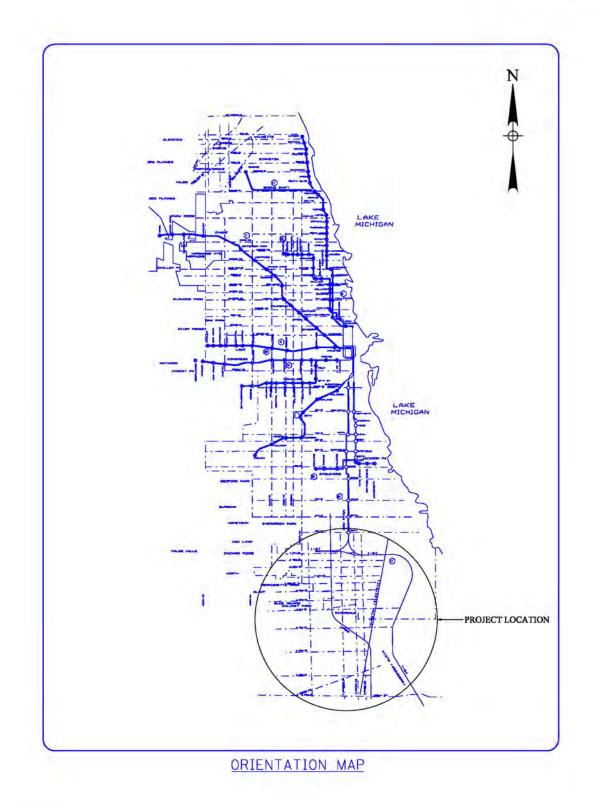


Appendix F Plans and Profiles

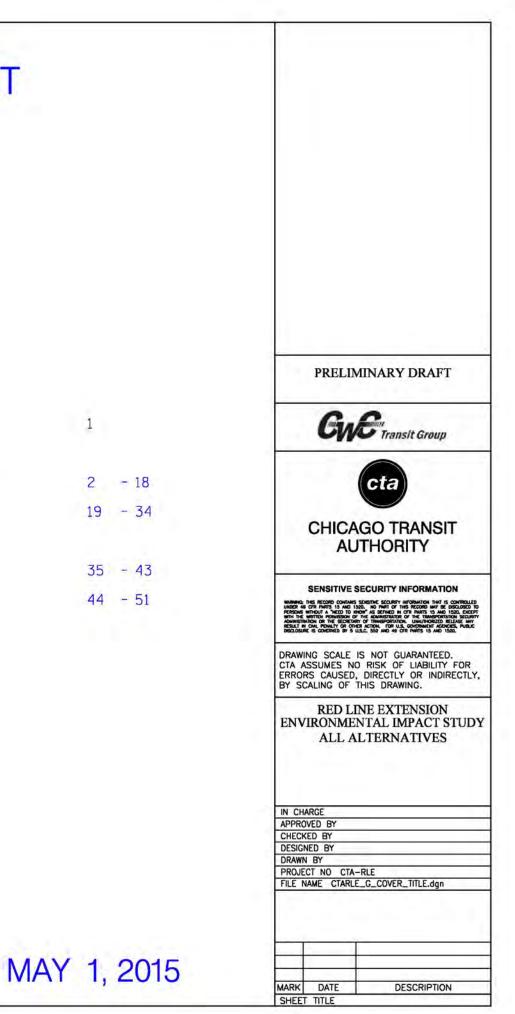


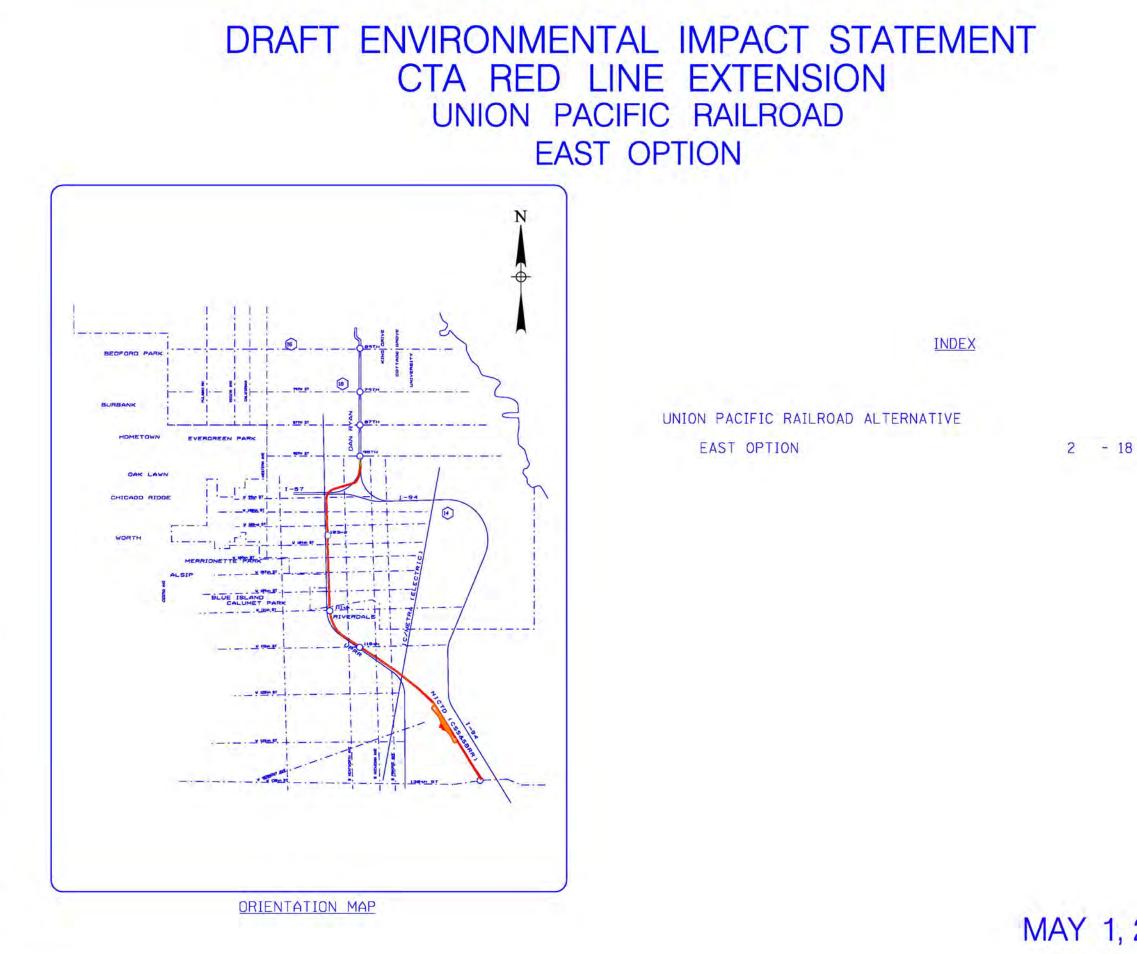
## DRAFT ENVIRONMENTAL IMPACT STATEMENT CTA RED LINE EXTENSION ALL ALTERNATIVES



## INDEX

COVER SHEET	
UNION PACIFIC RAILROAD ALTERNATIVE	
EAST OPTION	
WEST OPTION	
ALL ALTERNATIVES	
PIER LOCATIONS	
SECTIONS	

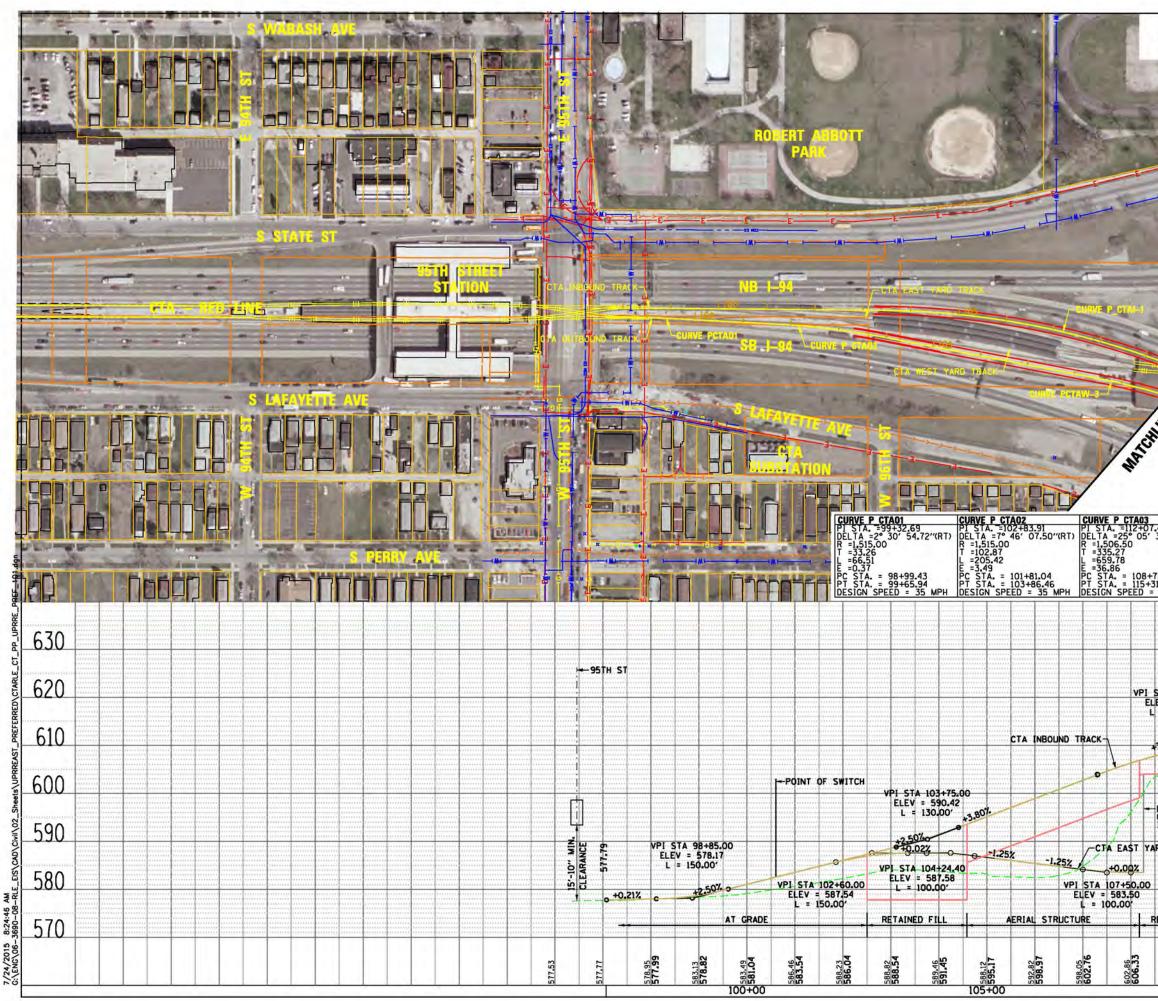




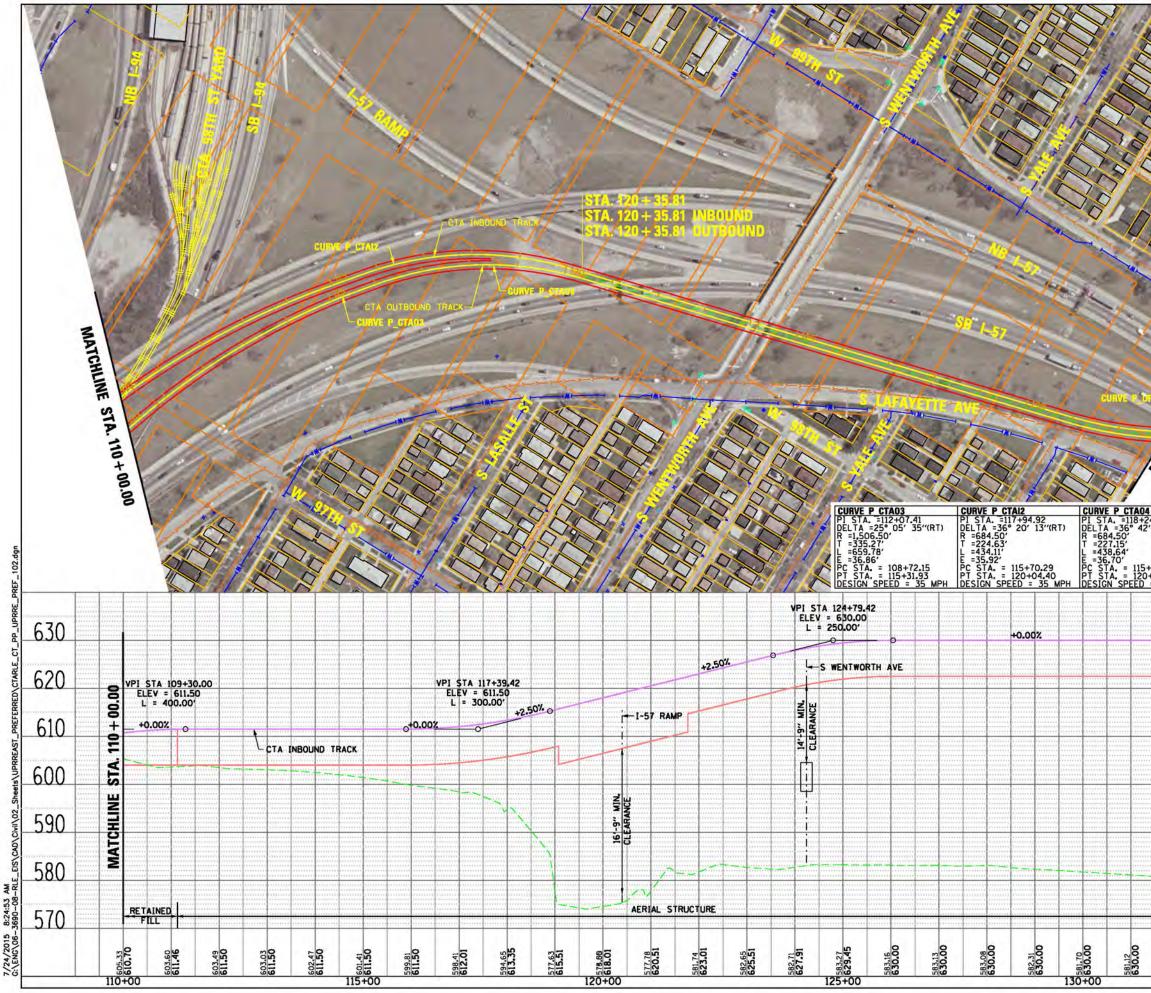
7/24/2015 8:24:40 AM G:\ENG\06-3690-08-RLE

PRELIMINARY DRAFT PRELIMINARY DRAFT PRELIMINARY DRAFT PRELIMINARY DRAFT PRELIMINARY DRAFT PRESIDENT CONTENT PRESIDENT CON			
CONSTRAINTS OF ALL AND A CONSTRAINTS OF A CONSTRAINT OF A CONSTRAINTS OF A CONSTRAINT OF A CONSTRAINT OF A CONSTRAINTS OF A CONSTRAINT OF A CONSTRAINTS OF A CONSTRAINT OF A CONSTRAINT OF A CONSTRAINT OF A CONST			
CONSTRAINTS OF ALL AND A CONSTRAINTS OF A CONSTRAINT OF A CONSTRAINTS OF A CONSTRAINT OF A CONSTRAINT OF A CONSTRAINTS OF A CONSTRAINT OF A CONSTRAINTS OF A CONSTRAINT OF A CONSTRAINT OF A CONSTRAINT OF A CONST			
CONSTRAINTS OF ALL AND A CONSTRAINTS OF A CONSTRAINT OF A CONSTRAINTS OF A CONSTRAINT OF A CONSTRAINT OF A CONSTRAINTS OF A CONSTRAINT OF A CONSTRAINTS OF A CONSTRAINT OF A CONSTRAINT OF A CONSTRAINT OF A CONST			
CONSTRAINTS OF ALL AND A CONSTRAINTS OF			
CONSTRAINTS OF ALL AND A CONSTRAINTS OF			
CONSTRAINTS OF ALL AND A CONSTRAINTS OF			
CONSTRAINTS OF ALL AND A CONSTRAINTS OF			
CONSTRAINTS OF ALL AND A CONSTRAINTS OF			
CONSTRAINTS OF ALL AND A CONSTRAINTS OF			
CHICKED BY CALCERED ADDIED BY CHARGE AUTOCIAL CALCED BY CONTACT NO CTA-RLE		PRELIN	MINARY DRAFT
CHICKED BY CALCERED ADDIED BY CHARGE AUTOCIAL CALCED BY CONTACT NO CTA-RLE		Gu	En Transit Groun
AUTHORITY SENSITIVE SECURITY INFORMATION WHENG IN THE RECORD COMMANS STREETING SICLED'S INFORMATION THAT IS CONTRUCTED WHENG WHENCH IS AND ISC. NO MAIL OF THE RECORD WIT IS CONTRUCTED PREMIUM INFORMATION AND INFORMATION INFORMATION INFORMATION SECTION OF THE DESCRIPTION ACCOUNT OF THE RECORD WIT IS CONTRUCTED SECTION OF THE DESCRIPTION ACCOUNT OF THE RECORD WIT IS CONTRUCTED CONTRUCT IN CONTRUCT OF THE RECORD WIT IS CONTRUCTED. SECTION OF THE DESCRIPTION ACCOUNT OF THE RECORD WIT IS CONTRUCTED SECTION OF THE DESCRIPTION ACCOUNT OF THE RECORD WITH IS CONTRUCT. SECTION OF THE DESCRIPTION ACCOUNT OF THE RECORD WITH IS CONTRUCT. SECTION OF THE DESCRIPTION OF THE RECORD WITH IS CONTRUCT. SECTION OF THE DESCRIPTION OF THE RECORD WITH IS CONTRUCT. DRAWNING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST OPTION 2 OF 51 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DESIGNED BY DESIGNED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE	-		
AUTHORITY SENSITIVE SECURITY INFORMATION WHENG IN THE RECORD COMMANS STREETING SICLED'S INFORMATION THAT IS CONTRUCTED WHENG WHENCH IS AND ISC. NO MAIL OF THE RECORD WIT IS CONTRUCTED PREMIUM INFORMATION AND INFORMATION INFORMATION INFORMATION SECTION OF THE DESCRIPTION ACCOUNT OF THE RECORD WIT IS CONTRUCTED SECTION OF THE DESCRIPTION ACCOUNT OF THE RECORD WIT IS CONTRUCTED CONTRUCT IN CONTRUCT OF THE RECORD WIT IS CONTRUCTED. SECTION OF THE DESCRIPTION ACCOUNT OF THE RECORD WIT IS CONTRUCTED SECTION OF THE DESCRIPTION ACCOUNT OF THE RECORD WITH IS CONTRUCT. SECTION OF THE DESCRIPTION ACCOUNT OF THE RECORD WITH IS CONTRUCT. SECTION OF THE DESCRIPTION OF THE RECORD WITH IS CONTRUCT. SECTION OF THE DESCRIPTION OF THE RECORD WITH IS CONTRUCT. DRAWNING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST OPTION 2 OF 51 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DESIGNED BY DESIGNED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE			cta
AUTHORITY SENSITIVE SECURITY INFORMATION WHENG IN THE RECORD COMMANS STREETING SECOND WITH IS CONTRAINED AND INC. TO PARTIE IN THE RECORD WITH IS CONTRAINED AND INC. TO PARTIE IN THE RECORD WITH IS CONTRAINED AND INC. THE RECORD WITH IND		~	
DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO ISS. NO NUMBER SECTION OF THIS RECORD WE BOOM OF THE ISS. THE WE WE WITH AND ISS. NO NUMBER SECTION OF THE ISS. DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNITON PACIFIC RAILROAD ALTERNATIVE EAST OPTION 2 OF 51 IN CHARGE APPROVED BY DESIGNED BY DRAWING BY PROJECT NO CTA-RLE			
DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST OPTION 2 OF 51 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE			
CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST OPTION 2 OF 51 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE	WARNING: UNDER 40 PERSONS WITH THE ADMINISTIF RESULT IF DISCLOSU	THE RECORD CONTAINS I CIR PARTS 15 AND 1: WITHOUT A "NEED TO K WATTON PERMISSION ON ANTON OR THE SECRETA I CALL PERMITY OR OTH RE IS CONTRIBUTY OF DI	S SHEIME SECLIRY WORKING INT IS CONTINUED 300, NO PART OF THE RECOMM UNIT BE DECLORED TO NOM'AS BETHEND IN OF PARTS 15 AND 1520, DECEM- THE ADMENTION OF THE INVESTIGATION DECLORING THE ADMENTION OF THE INTERNATION DECLORING TO STOKE THE INC. UNDERFORMED BELOS INT OF THE THE INTERNATION OF THE INTERNATION OF THE INTERNATION OF THE INTERNATION OF THE USLC 552 AND 49 OF PARTS 15 AND 1520.
ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST OPTION 2 OF 51 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE	CTA A	SSUMES NO	O RISK OF LIABILITY FOR
ALTERNATIVE EAST OPTION 2 OF 51 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE		IRONME	NTAL IMPACT STUDY
IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE	l	AL	TERNATIVE
APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE			2 OF 51
DESIGNED BY DRAWN BY PROJECT NO CTA-RLE	APPRO	OVED BY	
PROJECT NO CTA-RLE	DESIG	NED BY	
FILE NAME CTARLE_G_COVER.dgn	PROJE	CT NO CTA	-RLE
	FILE N	AME CTARL	E_G_COVER.dgn
		-	

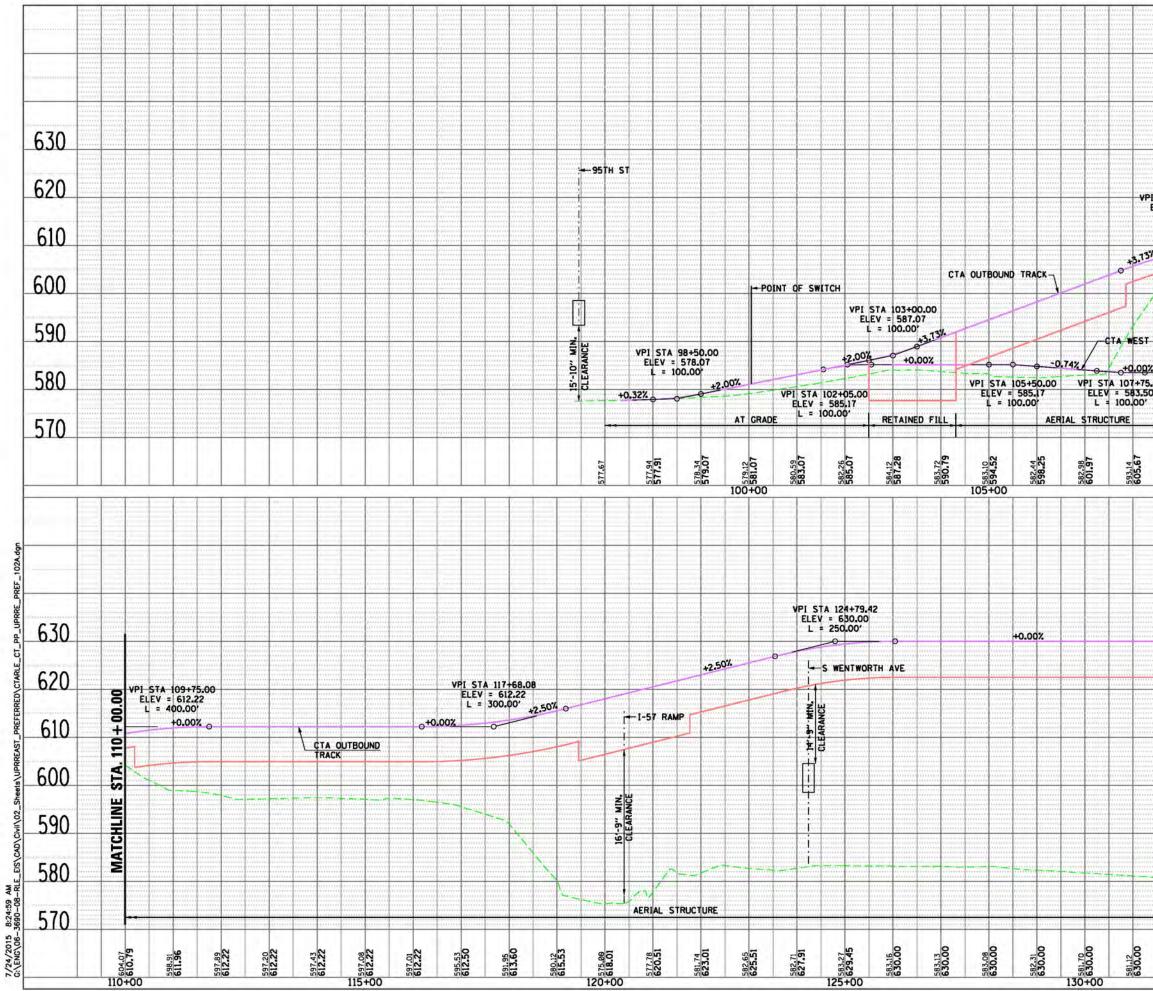
## MAY 1, 2015



z-			LEGEND Plan	1000
	- Sullin		PR PR PR PR PR PR PR PR PR PR PR PR PR P	CENTERLINE CENTERLINE TRACK CENTERLINE TRACK CENTERLINE TRACK CENTERLINE TRACK CENTERLINE TRACK CENTERLINE ROADWAY RIGHT-OF-WAY RAILROAD RIGHT-OF-WAY PERMANENT ENVELOPE ROADWAY/HARDSCAPE STATION PLATFORM BUILDING OUTLINE BUILDING OUTLINE STATION FLATFORM BUILDING OUTLINE STATION FLATFORM STATION FLATFORM STING ELECTRIC LINE STING STING STING STING STING STING STING STING STING STING STING STING S
STA.			Gu	Erransit Group
				Cta
		19+39.42 10' 36" (RT)	AL	GO TRANSIT JTHORITY
	CURVE P CTA PI STA. = 10 DELTA = 38° R = 1,722.62 T = 596.12' L = 1,147.80' E = 100.23' P.C. STA = 1 DESIGN SPEE		AL SENSITIVE	
		03+43.30 14+91.10 <u>0 = 35 мрн</u> 630	SENSITIVE: UNDER A CIT AND TO TO WITHOUT AN UNIT TO TO WITHOUT AND TO WITH	SECURITY INFORMATION SECURITY INFORMATION Security And Control of the Secure way of Decided to and Control of the Secure way of Decided to and Secure of the Secure way of Decided to and Secure of the Secure way of Decided to and Secure of the Secure of the Secure secure of The Secure of the Secure Like Secure of the Secure of Secure Like Secure of Secure of Secure Secure of Secure of Secure of Secure of Secure of Secure of Secure Secure of Secure
+72.14 -31.92 = 35 MPH <u>51A 109+33</u> LEV = 611.5 L = 400.00	0.000	оз+43.30 14+91.10 <u>0 = 35 мрн</u> 630 620	AL SENSITIVE WARDER OF THE RECORD CONTANT WARDER OF THE RECORD CONTANT OF THE RECORD CONTA	SECURITY INFORMATION Security INFORMATION Security INFORMATION Security Security INFORMATION Security Security INFORMATION Security INFORMATION Security INFORMATION Security INFORMATION INFORMATION INFORMATION SECURITY INFORMATION INFORMATION SECURITY INFORMATION INFORMATION SECURITY INFORMATION INFORMATION INFORMATION INFORMATION OF THE INFORMATION INFORMANIA INFORMATION INT
+72.14 -31.92 = 35 MPH <u>51A 109+33</u> LEV = 611.5 L = 400.00	0.000 0.0000 0.0000 0.0000 0.000000	озназао 14-91-10 0 = 35 мрн 630 620 610	AL SENSITIVE: UNDER STOLE AND TO PROMOVE AND TO UNDER STOLE AND TO ADDRESS AND TO ADDRESS AND TO ADDRESS AND TO ADDRESS AND TO ADDRESS AND TO ADDRESS AND ERRORS CAUSED BY SCALING OF RED LI ENVIRONME UNION PA AL	SECURITY INFORMATION SECURITY INFORMATION SOME OF ANY INFORMATION OF ANY INFORMATION SOME OF ANY INFORMATION OF ANY INFORMATION SOME OF ANY INFORMATION OF ANY INFORMATION ANY INFORMATION ANY INFORMATION ANY INFORMATION ANY INFORMATION ANY INFORMATION AN
+72.14 -31.92 = 35 MPH STA 109+3/ LEV = 611.5 L = 400.00 <sup>-4</sup>	0.000 0.0000 0.0000 0.0000 0.0000 0.0000 0.0000 0.0000 0.000000	оз+43.30 14+91.10 <u>0 = 35 мрн</u> 630 620	AL SENSITIVE: UNDER STOLE AND TO PROMOVE AND TO UNDER STOLE AND TO ADDRESS AND TO ADDRESS AND TO ADDRESS AND TO ADDRESS AND TO ADDRESS AND TO ADDRESS AND ERRORS CAUSED BY SCALING OF RED LI ENVIRONME UNION PA AL	SECURITY INFORMATION SECURITY INFORMATION LEVENTRE SECURITY INFORMATION LEVENTRE SECURITY INFORMATION SOL AD OTHER SECURITY INFORMATION SOL AD OTHER SECURITY INFORMATION SOL AD OTHER SECURITY INFORMATION IN
+72.14 -31.92 = 35 MPH STA 109+30 LEV = 611.5 L = 400.00 = 400.00 = 400.00 = 400.00 = 100 + 30 = 400.00	0.000 0.0000 0.0000 0.0000 0.0000 0.0000 0.0000 0.0000 0.000000	озназао 14-91-10 0 = 35 мрн 630 620 610	AL SENSITIVE : UNDER 49 CHI AND TO PROMOVE AND TO TO AND TO THE SECOND CONTAMINATION UNDER 49 CHI AND TO AND THE SECOND AND TH	SECURITY INFORMATION SECURITY INFORMATION A GRIDA STOLEY WITH A STOLEY STATUS STA
+72.14 -31.92 = 35 MPH STA 109+34 LEV = 611.5 L = 400.00 = 5.80% 04 = EXISTING CTA/SB I-94 TUNN ARD TRACK	0.000 0.0000 0.00000 0.00000 0.0000 0.0000 0.0000 0.00	оз+43.30 14-91.10 0 = 35 мрн 630 620 610 610 590 580	SENSITIVE : UNDER 40 CFF ANTE 15 AOC PRESON 40 CFF ANTE 15 AOC ADD 10 CFF ANTE 15 AOC PRESON 40 CFF ANTE 15 AOC PRESON 40 CFF ANTE 15 AOC PRESON 40 CFF ANTE 15 AOC ADD 10 CFF ANTE 15 AOC PRESON 40 CFF ANTE 15 AOC ADD 10 CFF ANTE 15 AOC ADD 10 CFF ANTE 15 AOC PRESON 40 CFF ANTE 15 AOC ADD 10 CF	SECURITY INFORMATION SECURITY INFORMATION SUBJECT SECURITY OR INDIRECTLY, THIS DRAWING. SUBJECT SECURITY OR INDIRECTLY, THIS DRAWING. SUBJECT SECURITY OR INDIRECTLY, THIS DRAWING. SUBJECT SECURITY OR INDIRECTLY, SUBJECT SECURITY OR INDIRECT SECURITY SUBJECT SECURITY OR INDIRECTLY, SUBJECT SECURITY OR INDIRECT SECURITY SUBJECT SECURITY SE
+72.14 +31.92 = 35 MPH STA 109+3/ LEV = 611.5 L = 400.00 <sup>+</sup>	0.000 0.0000 0.00000 0.00000 0.0000 0.0000 0.0000 0.00	озназао 14-91-10 6-30 6-30 6-20 6-10 6-10 6-00 5-90	SENSITIVE : UNDER 40 CFF ANTE 15 AOC PRESON 40 CFF ANTE 15 AOC ADD 10 CFF ANTE 15 AOC PRESON 40 CFF ANTE 15 AOC PRESON 40 CFF ANTE 15 AOC PRESON 40 CFF ANTE 15 AOC ADD 10 CFF ANTE 15 AOC PRESON 40 CFF ANTE 15 AOC ADD 10 CFF ANTE 15 AOC ADD 10 CFF ANTE 15 AOC PRESON 40 CFF ANTE 15 AOC ADD 10 CF	SECURITY INFORMATION SECURITY INFORMATION SUBJECT OF THE RECORD WAY & DURING UNDER SECURITY INFORMATION SUBJECT OF THE RECORD WAY & DURING UNDER SECURITY WAY SUBJECT OF THE RECORD WAY & DURING UNDER SECURITY WAY SUBJECT OF THE RECORD WAY & DURING UNDER SECURITY FOR THIS DRAWING. STATUL IMPACT STUDY ACIFIC RAILROAD TERNATIVE ST OPTION 3 OF 51



			LEGENI	)	_
		6	PLAN	Sub-marker and	
			_,	- EX CENTERLINE - PR CENTERLINE	
				PR TRACK CENTERLINE	
	X		-	PR TRACK STRUCTURE EX ROADWAY RIGHT-OF-WA	Y
	1			- EX RAILROAD RIGHT-OF-W	
	/			PR PERMANENT ENVELOPE PR ROADWAY/HARDSCAPE	
	Ň			PR STATION PLATFORM EX BUILDING OUTLINE	
				PR BUILDING OUTLINE	
X		S V	a contraction		
Q	7		UTILITIES	PR UPRR TRACK CENTERLI	NE
7	11/2 1			- EXISTING WATER LINES	
SIN		7 -1	1-101-	- EXISTING GAS LINE	
118	17	1		<ul> <li>EXISTING ELECTRIC LINE</li> <li>EXISTING TELEPHONE LINE</li> </ul>	
181		-3	ENTRANCES	PRIMARY ENTRANCE	
SV.	1	1	4	SECONDARY ENTRANCE	
25			44	EXIT ONLY PROPOSED PARKING ENTRA	NCE
1	1		PROFILE	EX GROUND	
120				PR TOP OF RAIL	DEDT
	00			PLATFORM LOCATION	DEPTH
43	33+0		PF	RELIMINARY DRAFT	٦.
	FIC		(	BAC Transit Group	
MA TCHI	and the series of the			cta	
.32 59"(RT)	CURVE P UPE1 PI STA. =136 DELTA =74° 5 R =782.29' T =599.34' L =1022.85'	+93.32 54' 52"(LT)	Сн	IICAGO TRANSIT AUTHORITY	
.32 59"(RT)	CURVE P UPE1 PI STA. =136 DELTA =74° 5 R =782.29' T =599.34' L =1022.85'	+93.32 54' 52"(LT)	SEN	AUTHORITY	
.32 59"(RT)	CURVE P UPE1 PI STA. =136 DELTA =74° 5 R =782.29' T =599.34' L =1022.85' E =203.20' PC STA. = 13	+93.32 54' 52"(LT)	SEN	AUTHORITY	
.32 59"(RT)	CURVE P UPE1 PI STA. =136 DELTA =74° 5 R =782.29' T =599.34' L =1022.85'	+93.32 54' 52"(LT)	SEN	AUTHORITY	
.32 59"(RT)	CURVE P UPE1 PI STA. =136 DELTA =74° 5 R =782.29' T =599.34' L =1022.85'	+93.32 64' 52''(LT) 60+93.97 11+16.82 0 = 35 MPH	SEN: WARNER RES HER DE WORDS ADMENTATION OF RESULT IN OM. PD DECLOSURE & COM	AUTHORITY SITIVE SECURITY INFORMATION Re COMMENS SERVICE SOLIEFY INFORMATION TO COMMENS SERVICE SOLIEFY INFORMATION THAT S CON- TRADED OF THE SERVICE AND THE SOLIE AND THAT SCHORTHON OF THE SERVICE AND THAT SOLIE SCHORTHON OF THE SERVICE AND THAT SOLIEFY INFORMATION SCHORTHON OF THE SERVICE AND THE SERVICE AND THAT SOLIE SCHORTHON OF THE SERVICE AND THE SERVICE AND THE SERVICE AND THE SCHORTHON OF THE SERVICE AND THE SERVICE AND THE SCHORTHON OF THE SERVICE AND THE SERVICE AND THE SERVICE AND THE SCHORTHON OF THE SERVICE AND THE SERVICE AND THE SERVICE AND THE SCHORTHON OF THE SERVICE AND THE SERVICE AND THE SERVICE AND THE SCHORTHON OF THE SERVICE AND THE SERVICE AND THE SERVICE AND THE SCHORTHON OF THE SERVICE AND THE SERV	
.32 59"(RT)	CURVE P UPE1 PI STA. =136 DELTA =74° 5 R =782.29' T =599.34' L =1022.85'	+93.32 54' 52"(LT)	SEN: WURDER 69 CPI PW PRESOL 90 CPI PW PRESOL 90 CPI PW RESAL	AUTHORITY SITIVE SECURITY INFORMATION THE DECURITY INFORMATION THE DECURITY INFORMATION THE DECURITY INFORMATION THE DECURITY OF THE SECOND WHI E DOG NEED DECURITY OF THE SECOND WHI E	NTROLLED LOSED TO J. DECIPT SCONTY SC MAY FUBLC
.32 59"(RT)	CURVE P UPE1 PI STA. =136 DELTA =74° 5 R =782.29' T =599.34' L =1022.85'	+93.32 64' 52''(LT) 60+93.97 11+16.82 0 = 35 MPH	SEN: Water of or new Pressor without a demension or DECLOSURE is con DRAWING S CTA ASSUM ERRORS C	AUTHORITY SITILE SECURITY INFORMATION INFO COMMENTS SECTION INFORMATION INFO COMMENTS SECTION INFORMATION INFO COMMENTS AND AND AND AND AND AND INFO COMMENTS INFORMATION INFO INFORMATION INFO COMMENTS INFORMATION INFORMATION INFO COMMENTS INFORMATION	WINDLED JOSED TO A. DEEPT SECURITY FUELC
.32 59"(RT)	CURVE P UPE1 PI STA. =136 DELTA =74° 5 R =782.29' T =599.34' L =1022.85'	+93.32 64' 52''(LT) 60+93.97 1+16.82 D = 35 MPH 630	SEN: Water of or new Pressor without a demension or DECLOSURE is con DRAWING S CTA ASSUM ERRORS C	AUTHORITY SITIVE SECURITY INFORMATION THE DEAD DATE OF THE RECORD WAY BY DO NEED TO BOOK AS ENDER IN FORMATION THE SECOND WAY BY DO NEED TO BOOK AS ENDER IN AND THE RECORD WAY BY DO NEED TO BOOK AND AND TO THE RECORD WAY BY DO NEED TO BOOK AND AND THE RECORD WAY BY DO NEED TO BOOK AND THE RECORD W	WINDLED JOSED TO A. DEEPT SECURITY FUELC
.32 59"(RT)	CURVE P UPE PI STA. =136: DELTA =74° 5 R =782.29' T =599.34' L =1022.85' E =203.20' PC STA. = 13 PT STA. = 14 DESIGN SPEEI	+93.32 64' 52''(LT) 60+93.97 11+16.82 0 = 35 MPH	SEN: WURDER AS A PECO	AUTHORITY SITIVE SECURITY INFORMATION TO DATA SECURITY INFORMATION TO DATA SECURITY INFORMATION TO DATA SECURITY INFORMATION TO DATA SECURITY OF THE RECORD WIT IS DATA SECURITY OF THE RECORD	TOR CTLY,
.32 59"(RT)	CURVE P UPE PI STA. =136: DELTA =74° 5 R =782.29' T =599.34' L =1022.85' E =203.20' PC STA. = 13 PT STA. = 14 DESIGN SPEEI	+93.32 64' 52''(LT) 60+93.97 1+16.82 D = 35 MPH 630	SENS WURDERS AS ACCOUNT OF THE PERSON AS OFFICIAL SECTION OF THE SECTION OF THE DESCRIPTION OF THE DESCRI	AUTHORITY SITIVE SECURITY INFORMATION TO DATA SECURITY INFORMATION TO DATA SECURITY INFORMATION TO DATA SECURITY INFORMATION TO DATA SECURITY OF THE RECORD WIT IS DO TO DATA SECURITY OF THE RECORD WIT IS DO TO DATA SECURITY OF THE RECORD WIT IS DO SECURITY OF THE RECORD	TOR CTLY,
.32 59"(RT)	CURVE P UPE PI STA. =136: DELTA =74° 5 R =782.29' T =599.34' L =1022.85' E =203.20' PC STA. = 13 PT STA. = 14 DESIGN SPEEI	+93.32 64' 52''(L T) 0+93.97 11+16.82 <u>0 = 35 мрн</u> 630 620	SENS WURDERS AS ACCOUNT OF THE PERSON AS OFFICIAL SECTION OF THE SECTION OF THE DESCRIPTION OF THE DESCRI	AUTHORITY SITIVE SECURITY INFORMATION TO DATA SECURITY INFORMATION TO DATA SECURITY INFORMATION TO DATA SECURITY INFORMATION TO DATA SECURITY OF THE RECOMMENT SECURITY OF THE SECURITY OF THE SECURITY TO OTHER SECURITY OF THE RECOMMENT SECURITY OF THE SECURITY OF THE SECURITY SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY SECURITY OF THE SECURITY OF T	TOR CTLY,
.32 59"(RT)	CURVE P UPE PI STA. =136: DELTA =74° 5 R =782.29' T =599.34' L =1022.85' E =203.20' PC STA. = 13 PT STA. = 14 DESIGN SPEEI	+93.32 64' 52''(LT) 60+93.97 1+16.82 D = 35 MPH 630	SENS WURDERS AS ACCOUNT OF THE PERSON AS OFFICIAL SECTION OF THE SECTION OF THE DESCRIPTION OF THE DESCRI	AUTHORITY SITIVE SECURITY INFORMATION TO DATA SECURITY INFORMATION TO DATA SECURITY INFORMATION TO DATA SECURITY INFORMATION TO DATA SECURITY OF THE RECORD WIT IS DO TO DATA SECURITY OF THE RECORD WIT IS DO TO DATA SECURITY OF THE RECORD WIT IS DO SECURITY OF THE RECORD	TOR CTLY,
.32 59"(RT)	CURVE P UPE PI STA. =136: DELTA =74° 5 R =782.29' T =599.34' L =1022.85' E =203.20' PC STA. = 13 PT STA. = 14 DESIGN SPEEI	+93.32 64' 52''(LT) 0+93.97 1+16.82 0 = 35 MPH 630 620 610	SENS WURDERS AS ACCOUNT OF THE PERSON AS OFFICIAL SECTION OF THE SECTION OF THE DESCRIPTION OF THE DESCRI	AUTHORITY SITILE SECURITY INFORMATION THE SECURITY INFORMATION THE DESCRIPTION OF THE SECOND WIT & CON- THE DESCRIPTION OF THE SECOND WIT & CON- THE SECOND OF THE SECOND WIT	TOR CTLY,
.32 59″(RT)	CURVE P UPE PI STA. =136: DELTA =74° 5 R =782.29' T =599.34' L =1022.85' E =203.20' PC STA. = 13 PT STA. = 14 DESIGN SPEEI	+93.32 64' 52''(LT) 0+93.97 1+16.82 0 = 35 MPH 630 620 610	SENS WURDERS AS ACCOUNT OF THE PERSON AS OFFICIAL SECTION OF THE SECTION OF THE DESCRIPTION OF THE DESCRI	AUTHORITY SITILE SECURITY INFORMATION THE SECURITY INFORMATION THE DESCRIPTION OF THE SECOND WIT & CON- THE DESCRIPTION OF THE SECOND WIT & CON- THE SECOND OF THE SECOND WIT	TOR CTLY,
.32 59"(RT)	CURVE P UPE PI STA. =136 DELTA =74° 5 R =782.29' T =599.34' L =1022.85' E =203.20' PC STA. = 13 PT STA. = 14 DESIGN SPEET 00.00 + E E L VL S S S S S S S S S S S S S	+93.32 64' 52''(L T) 0+93.97 11+16.82 0 = 35 мрн 630 620	SEN: WINDOW OF THE INCOMENT PRESONAL PARTY OF THE WINDOW OF THE INCOMENT PRESONAL PARTY WINDOW OF THE INCOMENT PRESONAL PARTY SEAL PARTY OF THE INCOMENT PRESONAL PARTY SEAL PARTY OF THE INCOMENT PRESONAL PARTY PRESONAL PARTY PRESON	AUTHORITY SITIVE SECURITY INFORMATION The GOMMAN SECURITY INFORMATION THE IS AND INC. NO NET OF THE RECORD HAVE IS DO NET OF THE AND INC. NO NET OF THE RECORD HAVE IS DO NET OF THE AND INC. NO NET OF THE RECORD HAVE IS DO NET OF THE AND INC. NO NET OF THE RECORD HAVE IS DO NET OF THE AND INC. NO NET OF THE RECORD HAVE IS AND INC. NO NET OF THE RECORD HAVE IS DO NET OF THE AND INC. NOT AND INC. SCALE IS NOT GUARANTEED. MES NO RISK OF LIABILITY F AUSED, DIRECTLY OR INDIREC G OF THIS DRAWING. ED LINE EXTENSION NMENTAL IMPACT ST ON PACIFIC RAILROA ALTERNATIVE EAST OPTION 4 OF 51	TOR CTLY,
.32 59″(RT)	CURVE P UPE PI STA. =136 DELTA =74° 5 R =782.29' T =599.34' L =1022.85' E =203.20' PC STA. = 13 PT STA. = 14 DESIGN SPEET 00.00 + E E L VL S S S S S S S S S S S S S	++93.32 64' 52''(L т) 0++93.97 11+16.82 0 = 35 мрн 630 620 610 600	SEN: WINDOW OF PAST NECK WINDOW OF PAST NECK WINDOW OF PAST WINDOW OF PAST	AUTHORITY SITIVE SECURITY INFORMATION COMMINS SECURITY INFORMATION THE IS AND INC. NO NET OF THE RECOMMENDATION WILL DI BOOK AS EMAND IN TO THE RECOMMENDATION AND TO THE SECOND IN THE RECOMMENDATION AUSED, DIRECTLY OR INDIREC G OF THIS DRAWING. ED LINE EXTENSION NMENTAL IMPACT ST ON PACIFIC RAILROA ALTERNATIVE EAST OPTION 4 OF 51 BY	TOR CTLY,
.32 59″(RT)	CURVE P UPE PI STA. =136 DELTA =74° 5 R =782.29' T =599.34' L =1022.85' E =203.20' PC STA. = 13 PT STA. = 14 DESIGN SPEET 00.00 + E E L VL S S S S S S S S S S S S S	+93.32 64' 52''(LT) 0+93.97 1+16.82 0 = 35 MPH 630 620 610	SEN: WINDOW OF THE INCOMENT PRESONAL PARTY OF THE WINDOW OF THE INCOMENT PRESONAL PARTY WINDOW OF THE INCOMENT PRESONAL PARTY SEAL PARTY OF THE INCOMENT PRESONAL PARTY SEAL PARTY OF THE INCOMENT PRESONAL PARTY PRESONAL PARTY PRESON	AUTHORITY SITIVE SECURITY INFORMATION TO DATA SERVICE SECURITY INFORMATION TO DATA SERVICE SECURITY INFORMATION BUT IS DO TO DATA SERVICE SECURITY OF HEREINAL BUT IS DO SECURITY OF THE SECURITY OF HEREINAL BUT IS DO SECURITY OF THE SECURITY OF HEREINAL INFORMATION SECURITY OF THE SECURITY OF HEREINAL INFORMATION SECURITY OF THIS DRAWING. ED LINE EXTENSION NMENTAL IMPACT ST ON PACIFIC RAILROAD ALTERNATIVE EAST OPTION 4 OF 51 BY Y	TOR CTLY,
.32 59″(RT)	CURVE P UPE PI STA. =136 DELTA =74° 5 R =782.29' T =599.34' L =1022.85' E =203.20' PC STA. = 13 PT STA. = 14 DESIGN SPEET 00.00 + E E L VL S S S S S S S S S S S S S	++93.32 64' 52''(L т) 0++93.97 11+16.82 0 = 35 мрн 630 620 610 600	SENS WINDOWS RESERVENCE SCHOOL STATES SCHOOL SCHOOL STATES SCHOOL STATES SCHOOL STATES SCHOOL STATES SCHOOL SCHOOL SCHOOL STATES SCHOOL SCHOOL	AUTHORITY SITIVE SECURITY INFORMATION THE DECIDENT STORE STOREY INFORMATION THE DECIDENT OF THE STOREY INFORMATION THAT IS AND THE DECIDENT OF THE STOREY INFORMATION THAT IS AND THE DECIDENT OF THE STOREY INFORMATION THAT IS STORE TO POST OF THE STOREY INFORMATION THAT IS AUTO TO OWNER TO THE STOREY INFORMATION THAT INFORMATION OF THE STOREY INFORMATION OF THE STORE OF THE STORE OF THE STOREY INFORMATION AUTO TO OWNER STOREY OF THE STOREY INFORMATION STOREY OF THIS DRAWING. ED LINE EXTENSION NMENTAL IMPACT STO ON PACIFIC RAILROA ALTERNATIVE EAST OPTION 4 OF 51 BY Y	TOR CTLY,
.32 59″(RT)	CURVE P UPE PI STA. =136 DELTA =74° 5 R =782.29' T =599.34' L =1022.85' E =203.20' PC STA. = 13 PT STA. = 14 DESIGN SPEET 00.00 + E E L VL S S S S S S S S S S S S S	+93.32 64' 52'(LT) 0+93.97 1+16.82 0 = 35 МРН 630 620 610 600 590	SENS WINDOWS PARTICLE WINDOWS PARTICLE PRESERVICE SEASE OF PARTICLE PRESERVICE DRAWING S CTA ASSUM ERRORS C BY SCALIN R ENVIRO UNIC IN CHARGE APPROVED CHECKED B DESIGNED E DRAWN BY PROJECT NO	AUTHORITY SITIVE SECURITY INFORMATION THE DISOL NO AND STORES TO DISOL NO AND STORES STORE	TOR CTLY, UDY D
32 59"(RT)	CURVE P UPE PI STA. =136: DELTA =74° 5 R =782.29' T =599.34' L =1022.85' E =203.20' PC STA. = 13 PT STA. = 14 DESIGN SPEEI	++93.32 64' 52''(L т) 0++93.97 11+16.82 0 = 35 мрн 630 620 610 600	SEN: WINDOW OF PASS NECT WINDOW OF PASS NECT WINDOW OF PASS WINDOW OF PASS WINDOW OF PASS WINDOW OF PASS WINDOW OF PASS CTA ASSUM ERRORS C. BY SCALIN RI ENVIRO UNIC IN CHARGE APPROVED CHECKED B DESIGNED E DRAWN BY PROJECT NE FILE NAME	AUTHORITY SITIVE SECURITY INFORMATION COMMUNICATION AND AND AND AND AND AND AND AND AND AN	OR CTLY, UDY D
.32 59″(RT)	CURVE P UPE PI STA. =136 DELTA =74° 5 R =782.29' T =599.34' L =1022.85' E =203.20' PC STA. = 13 PT STA. = 14 DESIGN SPEET 00.00 + E E L VL S S S S S S S S S S S S S	+93.32 64' 52'(LT) 0+93.97 1+16.82 0 = 35 МРН 630 620 610 600 590	SENS WINDOWS PARTICLE WINDOWS PARTICLE PRESERVICE SEASE OF PARTICLE PRESERVICE DRAWING S CTA ASSUM ERRORS C BY SCALIN R ENVIRO UNIC IN CHARGE APPROVED CHECKED B DESIGNED E DRAWN BY PROJECT NO	AUTHORITY SITIVE SECURITY INFORMATION THE DISOL NO AND STORES TO DISOL NO AND STORES STORE	OR CTLY, UDY D
.32 59″(RT)	CURVE P UPE PI STA. =136 DELTA =74° 5 R =782.29' T =599.34' L =1022.85' E =203.20' PC STA. = 13 PT STA. = 14 DESIGN SPEET 00.00 + E E L VL S S S S S S S S S S S S S	<sup>+93.32</sup> <sup>64' 52'(LT)</sup> 630 630 620 610 600 590 580	SENS WINDOWS PASS RECO WINDOWS PASS RECO WINDOWS PASS RECO WINDOWS PASS RECO WINDOWS PASS RECO BECAUME & COM BECAUME & COM BECAUME & COM BECAUME & COM BY SCALIN RECOUNTS BY SCALIN RECO	AUTHORITY SITIVE SECURITY INFORMATION COMMANS SERVICE AND AND THE RECOMMENDATION TO COMMANS SERVICE AND AND THE RECOMMENDATION TO COMMANS SERVICE AND AND THE RECOMMENDATION TO COMMANS SERVICE AND AND THE RECOMMENDATION WITCH DIRECT AND AND THE RECOMMENDATION THE SERVICE AND AND AND THE RECOMMENDATION AND THE SERVICE AND AND AND THE RECOMMENDATION SCALE IS NOT GUARANTEED. MADE TO RECTLY OR INDIREC G OF THIS DRAWING. ED LINE EXTENSION NMENTAL IMPACT ST ON PACIFIC RAILROAD ALTERNATIVE EAST OPTION 4 OF 51 BY Y SY D CTA-RLE CTARLE_CT_PP_UPRRE_PREF_1 0 100 201 SCALE H 1" = 100'-0"	OR CTLY, UDY D
.32 59″(RT)	CURVE P UPE PI STA. =136 DELTA =74° 5 R =782.29' T =599.34' L =1022.85' E =203.20' PC STA. = 13 PT STA. = 14 DESIGN SPEET 00.00 + E E L VL S S S S S S S S S S S S S	+93.32 64' 52'(LT) 0+93.97 1+16.82 0 = 35 МРН 630 620 610 600 590	SENS WINDOWS PASS RECO WINDOWS PASS RECO WINDOWS PASS RECO WINDOWS PASS RECO WINDOWS PASS RECO BECAUME & COM BECAUME & COM BECAUME & COM BECAUME & COM BY SCALIN RECOUNTS BY SCALIN RECO	AUTHORITY SITIVE SECURITY INFORMATION THE COMMANS SERVICE AND THE SECOND WHEN BODY THE DISOL TO WHEN SECOND WHEN IS AND ISOL THE SECOND TO THE SECOND WHEN IS AND ISOL SECOND TO THE SECOND WHEN IS AND ISOL SECOND TO THE SECOND WHEN ISOL SECOND WHEN THE SECOND WHEN ISOL SECOND WHEN ISOL TO THE SECOND WHEN ISOL TO THE SECOND SECOND WHEN ISOL TO THE SECOND WHEN ISOL TO THE	OR CTLY, UDY D
.32 59″(RT)	CURVE P UPE PI STA. =136 DELTA =74° 5 R =782.29' T =599.34' L =1022.85' E =203.20' PC STA. = 13 PT STA. = 14 DESIGN SPEE OO: OO: OO: OO: OO: OO: OO: O	<sup>+93.32</sup> <sup>64' 52'(LT)</sup> 630 630 620 610 600 590 580	SENS WINDOWS PASS RECO WINDOWS PASS RECO WINDOWS PASS RECO WINDOWS PASS RECO WINDOWS PASS RECO BECAUME & COM BECAUME & COM BECAUME & COM BECAUME & COM BY SCALIN RECOUNTS BY SCALIN RECO	AUTHORITY SITIVE SECURITY INFORMATION COMMANS SERVICE AND AND THE RECOMMENDATION TO COMMANS SERVICE AND AND THE RECOMMENDATION TO COMMANS SERVICE AND AND THE RECOMMENDATION TO COMMANS SERVICE AND AND THE RECOMMENDATION WITCH DIRECT AND AND THE RECOMMENDATION THE SERVICE AND AND AND THE RECOMMENDATION AND THE SERVICE AND AND AND THE RECOMMENDATION SCALE IS NOT GUARANTEED. MADE TO RECTLY OR INDIREC G OF THIS DRAWING. ED LINE EXTENSION NMENTAL IMPACT ST ON PACIFIC RAILROAD ALTERNATIVE EAST OPTION 4 OF 51 BY Y SY D CTA-RLE CTARLE_CT_PP_UPRRE_PREF_1 0 100 201 SCALE H 1" = 100'-0"	OR CTLY, UDY D
.32 59"(RT)	CURVE P UPE PI STA. =136 DELTA =74° 5 R =782.29' T =599.34' L =1022.85' E =203.20' PC STA. = 13 PT STA. = 14 DESIGN SPEET 00.00 + E E L VL S S S S S S S S S S S S S	<sup>+93.32</sup> <sup>64' 52'(LT)</sup> 630 630 620 610 600 590 580	SENS WERE A OFFICE SECONDARY IN COMPANY SECONDARY IN COMPANY SECONDARY IN COMPANY SECONDARY IN COMP SECONDARY IN COMP SE	AUTHORITY SITIVE SECURITY INFORMATION COMMANS SERVICE AND AND THE RECOMMENDATION TO COMMANS SERVICE AND AND THE RECOMMENDATION TO COMMANS SERVICE AND AND THE RECOMMENDATION TO COMMANS SERVICE AND AND THE RECOMMENDATION WITCH DIRECT AND AND THE RECOMMENDATION THE SERVICE AND AND AND THE RECOMMENDATION AND THE SERVICE AND AND AND THE RECOMMENDATION SCALE IS NOT GUARANTEED. MADE TO RECTLY OR INDIREC G OF THIS DRAWING. ED LINE EXTENSION NMENTAL IMPACT ST ON PACIFIC RAILROAD ALTERNATIVE EAST OPTION 4 OF 51 BY Y SY D CTA-RLE CTARLE_CT_PP_UPRRE_PREF_1 0 100 201 SCALE H 1" = 100'-0"	OR CTLY, UDY D



		630	WARNER THE RECORD COMPARY SECTION SOLUTION INFORMATION THAT IS CONTROLLED WARDER 46 OF MARTS 13 AND 1300, IN ANY OF THE RECORD WARD IS DECORDS TO PRESSE WITHOUT A VECTO TO DOOM AS REFINED IN OTR HARTS 13 AND 1300, DECOR MAINTERNING ON THE SECTION OF THE RECORD MADE TO THE RECORD WARD RESULT IN CAM. PONUTY OF OTHER ACTION. UNLIVERED BELOW WAY RESULT IN CAM. PONUTY OF OTHER ACTION. THE U.S. CONTROLLED BELOW DESCRIPTION OF THE SECTION OF THE RECORD WARD WARD TO THE RECORD DESCRIPTION OF THE SECTION OF THE RECORD WARD WARD TO THE RECORD DESCRIPTION OF THE SECTION OF THE RECORD WARD WARD TO THE RECORD DESCRIPTION OF THE SECTION OF THE RECORD WARD WARD TO THE RECORD DESCRIPTION OF THE SECTION OF THE RECORD WARD WARD TO THE RECORD WARD WARD WARD WARD WARD WARD WARD WA
			AUTHORITY SENSITIVE SECURITY INFORMATION
604.02 608.70	610.19 610.19		CHICAGO TRANSIT
20		570	Gy E" Transit Group
0	MATCHLINE	580	PRELIMINARY DRAFT
EXISTING CTA/SB I-94 TUN ARD TRAC	LINE STA.	590	PR TOP OF RAIL PR TOP OF RAIL PR ELEVATED STRUCTURE DEPTH PR ELEVATED STRUCTURE DEPTH
7	110 + 00.00	600	PRIMARY ENTRANCE SECONDARY ENTRANCE EXIT ONLY PROPOSED PARKING ENTRANCE
STA 109+7 EV = 612.3 = 400.00	22	610	EXISTING WATER LINES EXISTING SEWER LINE EXISTING GAS LINE EXISTING ELECTRIC LINE EXISTING TELEPHONE LINE ENTRANCES
•SB [-94		630 620	PR BUILDING OUTLINE PR PARKING PR CTA SUBSTATION PR UPRR TRACK CENTERLINE UTILITIES
			PR TRACK CENTERLINE PR TRACK STRUCTURE EX ROADWAY RIGHT-OF-WAY EX RAILROAD RIGHT-OF-WAY PR PERMANENT ENVELOPE PR ROADWAY/HARDSCAPE PR STATION PLATFORM EX BUILDING OUTLINE

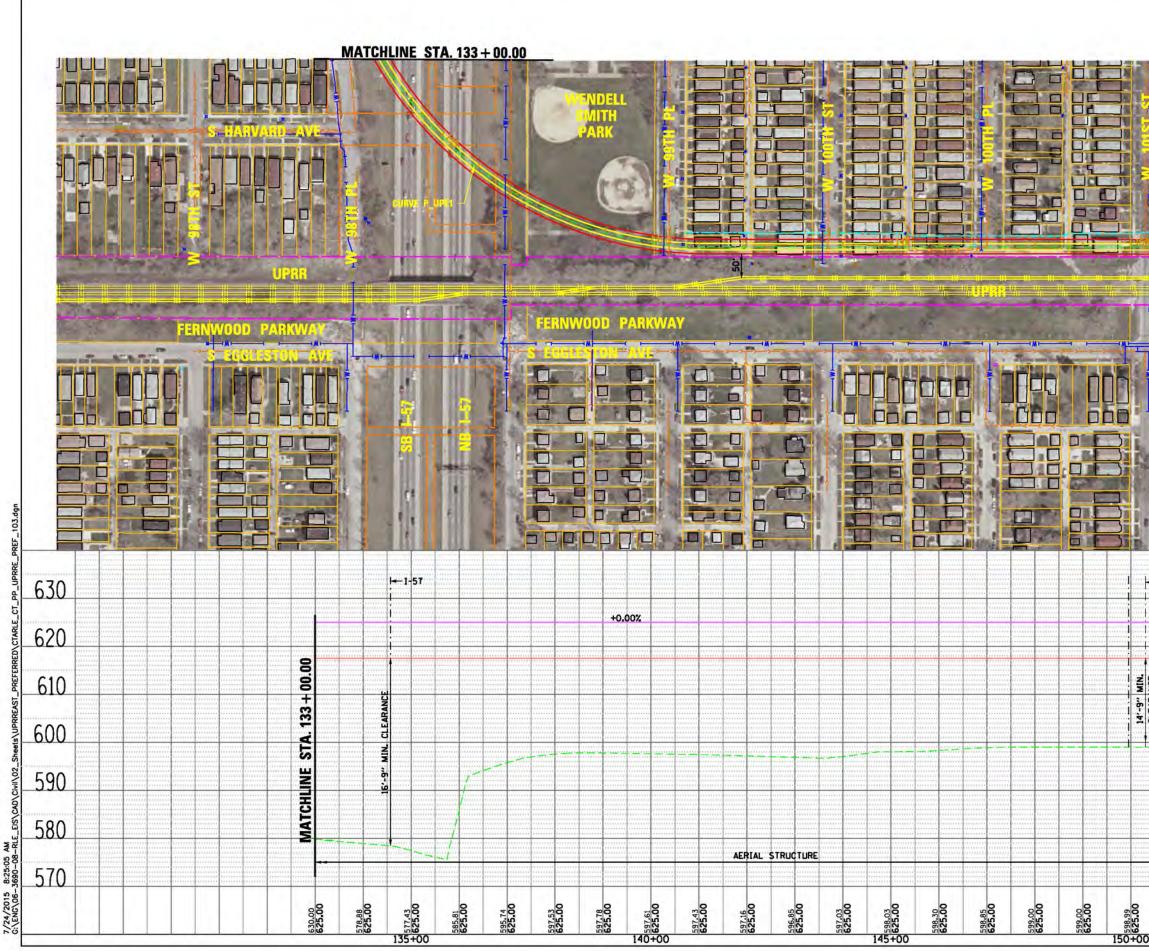
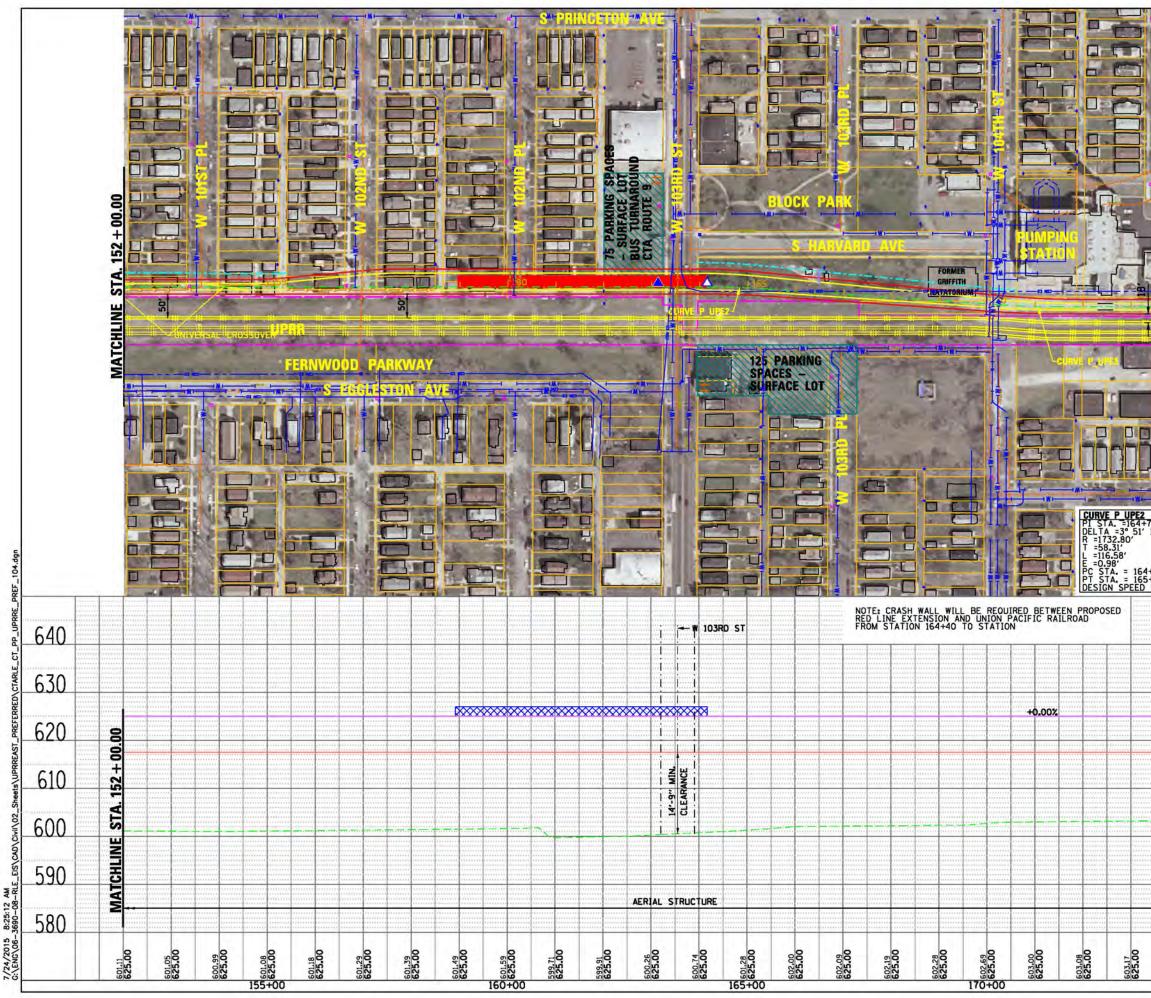
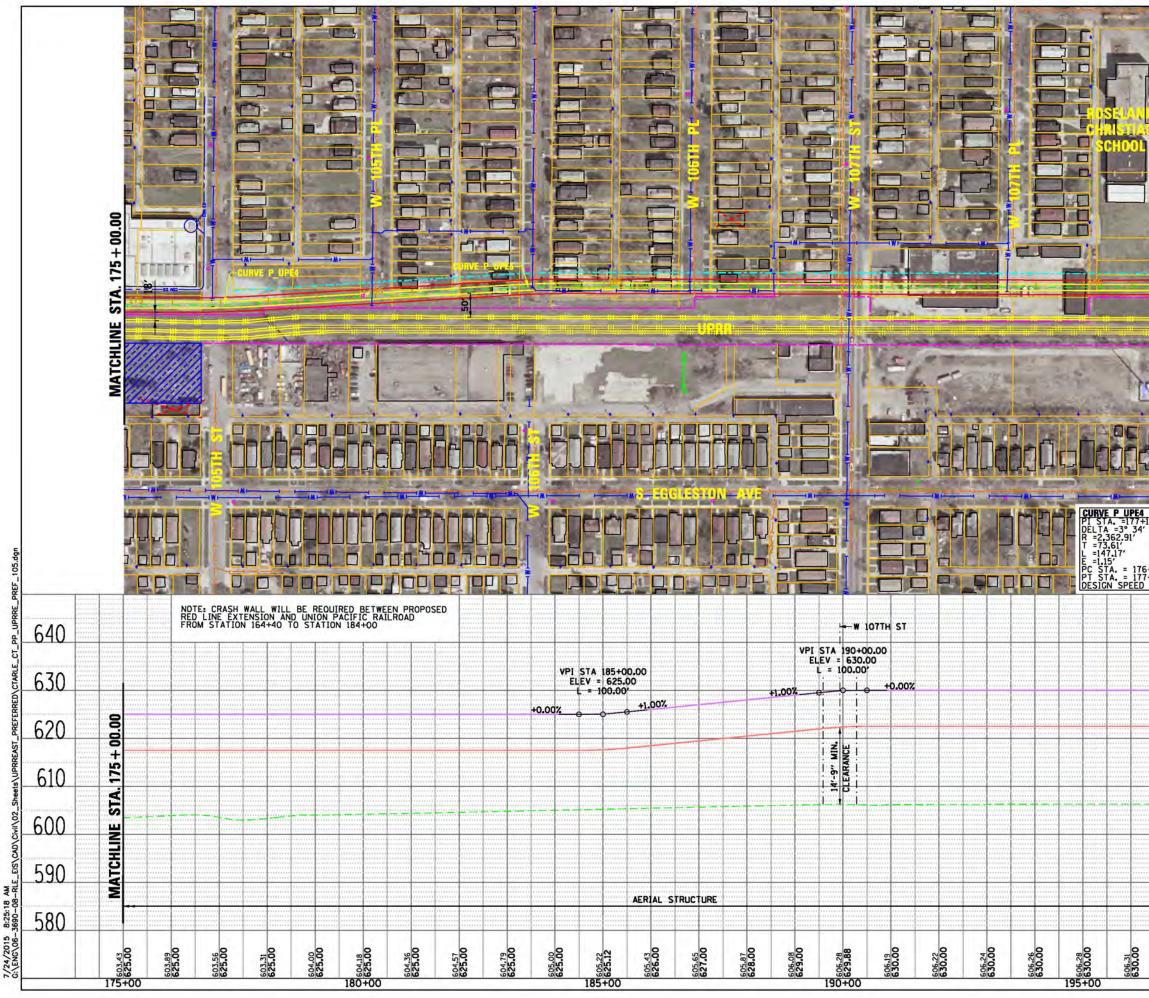


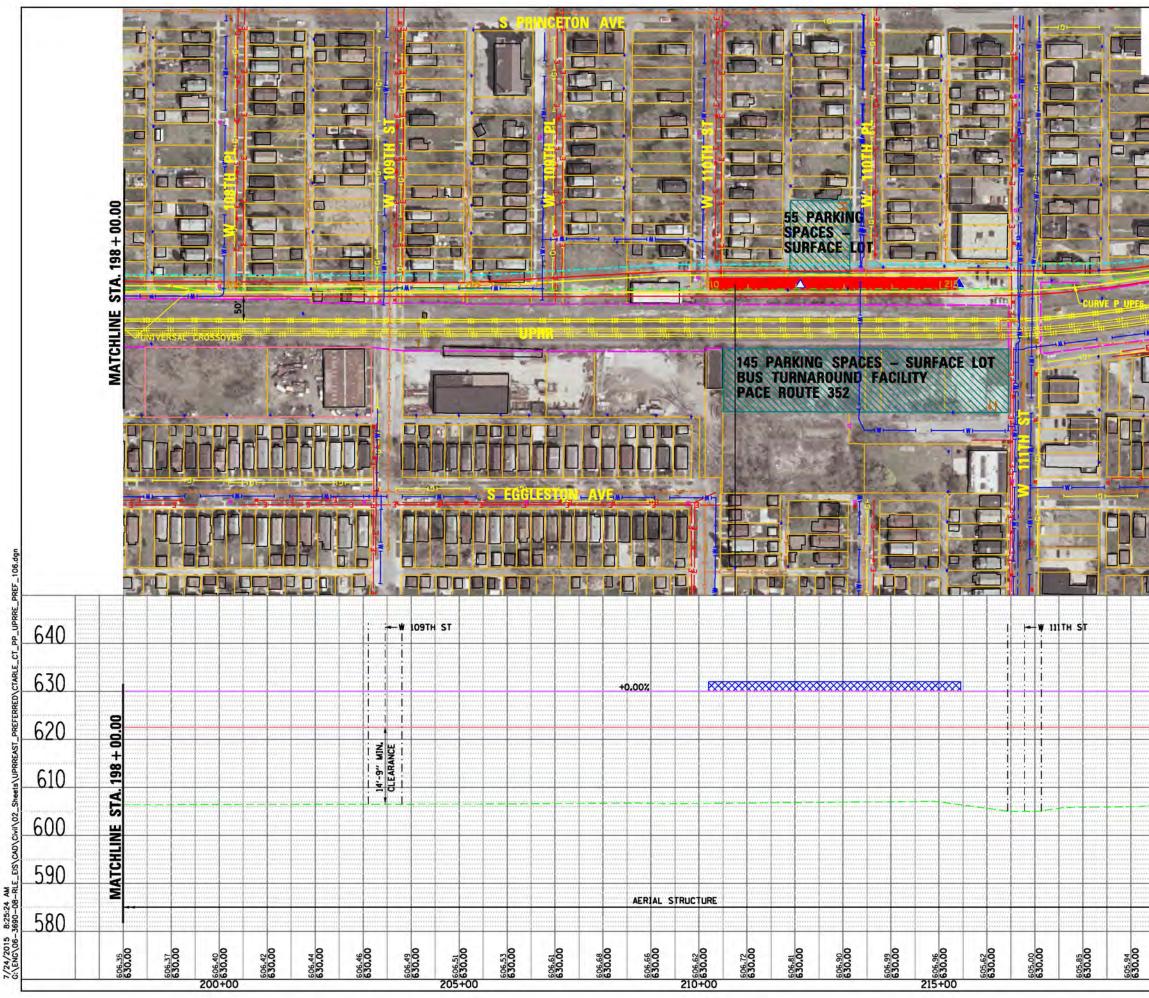
Image: State of the state					
Image: State = 130 + 93.97 PC 5TA. = 100 + 97 PC 5TA. = 100 + 97 PC 5TA. = 100 + 97 PC 04ECKED BY	INE RE -OF-WAY -OF-WAY ELOPE CAPE ORM NE NE NE LINE E LINE E LINE E LINE E LINE E LINE CE ENTRANCE	EX CENTERLINE     FR CENTERLINE     FR CENTERLINE     FR TRACK CENTERLINE     FR TRACK CENTERLINE     FR TRACK CENTERLINE     FR TRACK STRUCTURE     FR TRACK STRUCTURE     FR ROADWAY HARDSCAPE     FR ROADWAY HARDSCAPE     FR ROADWAY HARDSCAPE     FR ROADWAY HARDSCAPE     FR BUILDING OUTLINE     FR BUILDING SEWER LINE     EXISTING SEWER LINE     EXISTING GAS LINE     EXISTING GAS LINE     EXISTING GAS LINE     EXISTING GAS LINE     EXISTING CELEPHONE LIN RANCES     PRIMARY ENTRANCE     SECONDARY ENTRANCE     EXIT ONLY     FROPOSED PARKING ENTR  ILE     EX GROUND     FR COUND     FR COUND     FR ELEVATED STRUCTURE		LINE STA.	
CURVE P. UPE!         PI STA. = :136:493.32         DELTA = :74° 54' 52''(LT)         ST8.2:29'         =:022:265'         :020:07         :020:07         :020:07         :020:07         :020:07         :020:07         :020:07         :020:07         :020:07         :020:07         :020:07	AFT	PRELIMINARY DRAFT		MATC	
AUTHORITY AUTHOR	oup	CWE Transit Group		4	
PT STA. = 141+16.82         DESIGN SPEED = 355 MPH         W 101ST ST         630         DRAWING SCALE IS NOT GUARANTEE         CTA ASSUMES NO RISK OF LIABILITY         EROPS CAUSED, DIRECTLY OR INDI- BELLAND OF HIS SCALE IS NOT GUARANTEE         CTA ASSUMES NO RISK OF LIABILITY         EROPS CAUSED, DIRECTLY OR INDI- BELLAND OF HIS SCALE IS NOT GUARANTEE         CTA ASSUMES NO RISK OF LIABILITY         EROPS CAUSED, DIRECTLY OR INDI- BELLAND OF HIS SCALE IS NOT GUARANTEE         CTA ASSUMES NO RISK OF LIABILITY         EROPS CAUSED, DIRECTLY OR INDI- BELAND OF HIS SCALE IS NOT GUARANTEE         CTA ASSUMES NO RISK OF LIABILITY         EROPS CAUSED, DIRECTLY OR INDI- BELAND OF HIS SCALE IS NOT GUARANTEE         CTA ASSUMES NO RISK OF LIABILITY         EROPS CAUSED, DIRECTLY OR INDI- BELAND OF HIS SCALE IS NOT GUARANTEE         CTA ASSUMES NO RISK OF LIABILITY         EROPS CAUSED, DIRECTLY OR INDI- BELAND OF HIS SCALE IS NOT GUARANTEE         CTA ASSUMES NO RISK OF LIABILITY         EROPS CAUSED, DIRECTLY OR INDI- BELAND OF HIS SCALE IS NOT GUARANTEE         CTA ASSUMES NO RISK OF LIABILITY         EROPS CAUSED, DIRECTLY OR INDI- BELAND OF HIS SCALE IS NOT GUARANTEE         CTA ASSUMES NO RISK OF LIABILITY         EROPS CAUSED, DIRECTLY OR INDI- SCALE HIS SCALE IS NOT GUARANTEE         CTA ASSUMES NO RISK OF LIABILITY			4, 22, (C I)	STA. =136+ TA =74° 54 782.29' 599.34' 1022.85' 203.20' STA. = 130	
w 101ST ST       630         DRAWING SCALE IS NOT GUARANTEE CTA ASSUMES NO RISK OF LIABILITY ETA ORS CAUSED, DIRECTLY OR INDI BY SCALING OF THIS DRAWING.         620       RED LINE EXTENSION ENVIRONMENTAL IMPACT S UNION PACIFIC RAILRO ALTERNATIVE EAST OPTION         590       600         600       6 OF 51         IN CHARGE APPROVED BY         CHECKED BY         DRAWING SCALE IS NOT GUARANTEE CREAST OPTION         BR 610         SS         600         600         600         600         590         SS         580         SS         10         0         100         0         100         0         100         100         100         100         100         100         100         100         100         100         100         100         100			+16.82 = 35 MPH	STA. = 141- SIGN SPEED	
620         80         610         80         610         80         610         80         610         80         610         80         610         80         610         80         610         80         610         80         610         80         610         80         610         80         610         80         610         80         610         80         610         80         610         80         610         80         600         600         600         600         600         600         600         600         600         600         600         600         600         600         600         600	TEED. ILITY FOR	WING SCALE IS NOT GUARANTEED. ASSUMES NO RISK OF LIABILITY ORS CAUSED, DIRECTLY OR INDIRE	630		W 101ST ST
ENVIRONMENTAL IMPACT UNION PACIFIC RAILRO ALTERNATIVE EAST OPTION 600 6 OF 51 N CHARGE APPROVED BY CHECKED BY DRAWN BY PROJECT NO CTA-RLE FILE NAME CTARLE_CT_PP_UPRRE_PRE 100 0 100 SCALE H 1" = 100'-0"	ON	RED LINE EXTENSION	620		
APPROVED BY         590         CHECKED BY         DESIGNED BY	CT STUDY ROAD	VIRONMENTAL IMPACT ST UNION PACIFIC RAILROA ALTERNATIVE	610	2 + 00.00	
APPROVED BY         CHECKED BY         DESIGNED BY         DESIGNED BY         DRAWN BY         PROJECT NO CTA-RLE         FILE NAME CTARLE_CT_PP_UPRRE_PRE         100       0         SCALE H 1" = 100'-0"		IN STREET, NO	600	3TA. 15	i i
100 0 100 SCALE H 1" = 100'-0"		ROVED BY CKED BY IGNED BY WN BY	590	ABRONIAPENERE ABRONS	
	PREF_103.dgr	NAME CTARLE_CT_PP_UPRRE_PREF_	580	MAT	
		SCALE H 1" = 100'-0" V 1" = 10'-0"	570		
	PTION	C DATE DESCRIPTION		625.00	625.00



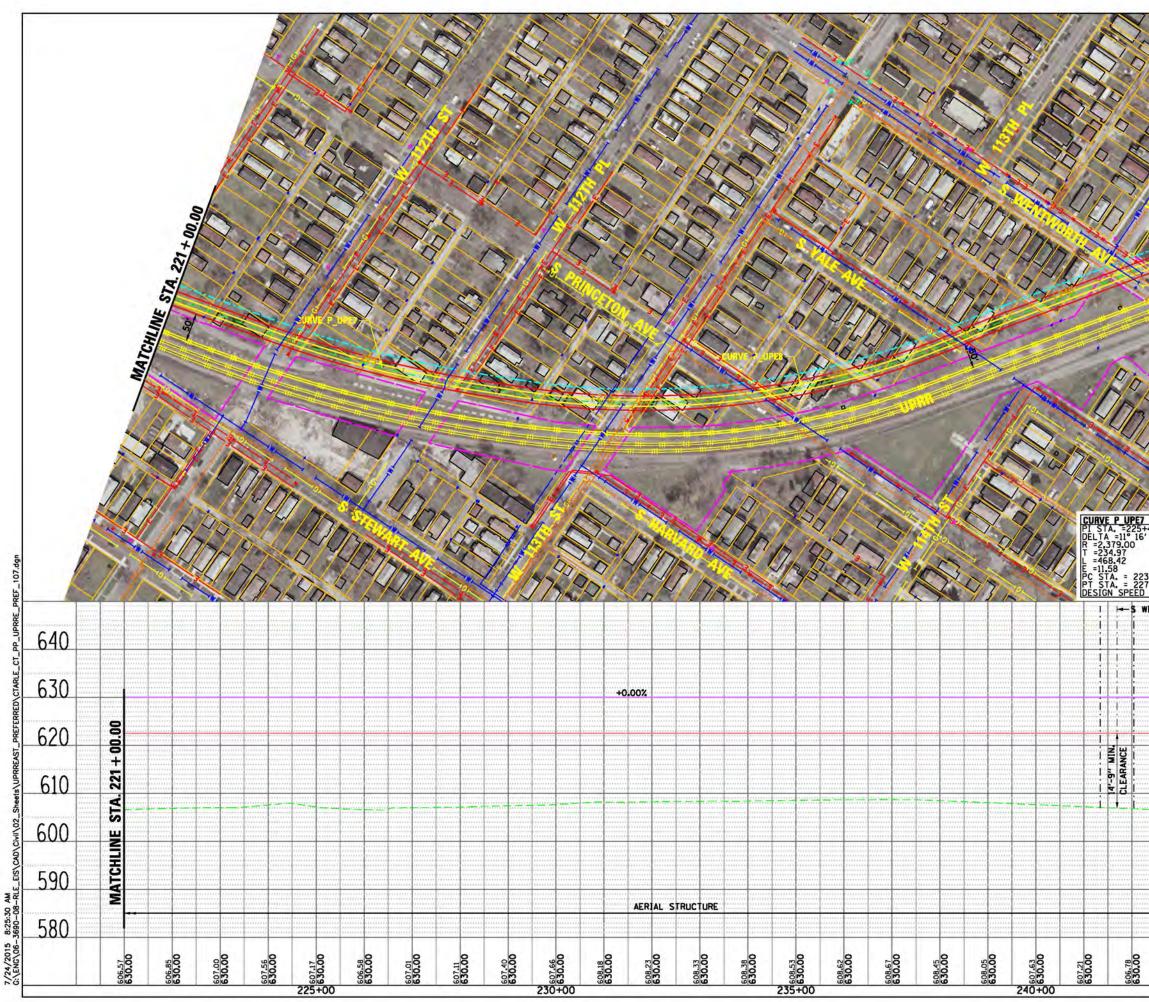
z—	φ-	-	LEGEND PLAN
			III       EX       TRACK       CENTERLINE         PR       TRACK       CENTERLINE         PR       TRACK       STRUCTURE         EX       ROADWAY       RIGHT-OF-WAY         EX       RAILROAD       RIGHT-OF-WAY         PR       PRADAD       RIGHT-OF-WAY         PR       PROADWAY HARDSCAPE         PR       STATION       PLATFORM         EX       BUILDING OUTLINE         PR       BUILDING OUTLINE         PR       PRARKING         VINNUTION       PR         CTA       SUBSTATION
	STA. 175 + 00.00		PR UPRR TRACK CENTERLINE UTILITIES EXISTING WATER LINES EXISTING GAS LINE EXISTING GAS LINE EXISTING ELECTRIC LINE EXISTING TELEPHONE LINE ENTRANCES PRIMARY ENTRANCE EXIT ONLY PROPOSED PARKING ENTRANCE PROFILE
	E		PR TOP OF RAIL PR ELEVATED STRUCTURE DEPTH
17372 4	MATCHLI		PRELIMINARY DRAFT
	Σ		Gy & Transit Group
10032			cta
والمساو ومحر الالتقار			•
R =1, T =58	732.80' 3.39' 6.73'	9 +87.96 ' 35''(LT)	CHICAGO TRANSIT AUTHORITY
R = 1, T = 58 L = 11 E = 00. 7.31 PT S	732.80' 3.39' 6.73' 98' TA. = 17 TA. = 17		AUTHORITY SENSITIVE SECURITY INFORMATION
R = 1, T = 56 L = 11 E = 00. 17.31 PT S	732.80' 3.39' 6.73' 98' TA. = 17 TA. = 17	0+29.57 1+46.30	AUTHORITY SENSITIVE SECURITY INFORMATION MARKET & CONTRACT SUBJECT OF THE SECOND IN THE CONTRACT OF MARKET & CONTRACT SUBJECT OF THE SECOND IN THE CONTRACT OF MARKET & CONTRACT SUBJECT OF THE SECOND IN THE CONTRACT OF MARKET WITTEN FORMESSION OF THE AMARKETISTIC OF THE THREE OF AND THE MARKET WITTEN FORMESSION OF THE AMARKETISTIC OF THE THREE OF AND THE MARKET WITTEN FORMESSION OF THE AMARKETISTIC OF THE THREE OF AND THE MARKET WITTEN FORMESSION OF THE AMARKETISTIC OF THE THREE OF AND THE MARKET WITTEN FORMESSION OF THE AMARKETISTIC OF THE THREE OF AND THE MARKET WITTEN FORMESSION OF THE AMARKETISTIC OF THE THREE OF AND THE MARKET WITTEN FORMESSION OF THE AMARKETISTIC OF THE THREE OF AND THE MARKET WITTEN FORMESSION OF THE AMARKET OF THE THREE OF AND THE MARKET & CONTINUE OF THE SUBJECT OF THREE OF AND THE THREE OF AND DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY,
R = 1, T = 56 L = 11 E = 00. 17.31 PT S	732.80' 3.39' 6.73' 98' TA. = 17 TA. = 17	0+29.57 1+46.30 ) = 55 MPH	AUTHORITY SENSITIVE SECURITY INFORMATION WHEN THE RECORD CONTANTS SPETITE SECURITY INFORMATION WHEN AND ADD TO THE SECURITY INFORMATION THAT IS CONTROLLED WHEN AND ADD THE SECURITY OF THE RECORD WIT & DOCE THE METHOD AND ADD THE SECURITY OF THE RECORD WIT & DOCE THE METHOD AND ADD THE SECURITY OF THE RECORD WIT & DOCE THE METHOD AND ADD THE SECURITY OF THE RECORD WIT & DOCE THE METHOD AND ADD THE SECURITY OF THE RECORD WIT & DOCE THE METHOD AND ADD THE SECURITY OF THE RECORD WIT & DOCE THE METHOD AND ADD THE SECURITY OF THE RECORD WIT & DOCE THE METHOD AND ADD THE SECURITY OF THE SECURITY OF THE METHOD AND ADD THE SECURITY OF THE SECURITY OF THE METHOD AND ADD THE SECURITY OF THE SECURITY OF THE METHOD AND ADD THE SECURITY OF THE SECURITY OF THE METHOD AND ADD THE SECURITY OF THE SECURITY OF THE METHOD AND ADD THE SECURITY OF THE SECURITY OF THE METHOD ADD THE SECURITY OF THE SECURITY OF THE METHOD ADD THE SECURITY OF THE SECURITY OF THE METHOD ADD THE SECURITY OF THE SECURITY OF THE SECURITY SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE METHOD ADD THE SECURITY OF THE
R = 1, T = 56 L = 11 E = 00. 17.31 PT S	(32.80°) 3.39° 6.73° 98° TA. = 17 TA. = 17 N <u>SPEEL</u>	0+29.57 1+46.30 ) = <u>55 мрн</u> 640	AUTHORITY SENSITIVE SECURITY INFORMATION WHEN AN OF THE TRADE SOUTH AND
R = 1, T = 58 L = 11 E = 00. 7.31 PT S	(32.80°) 3.39° 6.73° 7A. = 17 TA. = 17 N SPEEL 00.00 + 52 41 52 41 52 41 52 41 52 41 52 52 52 52 52 52 52 52 52 52	о+29.57 1446.30 <u>0 = 55 мрн</u> 640 630	AUTHORITY SENSITIVE SECURITY INFORMATION WHEN THE RECORD CONTANTS SPENTS SECURITY INFORMATION WHEN AN ADD TO THE SECURITY INFORMATION THAT IS CONTANT AND WHEN AN ADD TO THE SECURITY INFORMATION THAT IS CONTANT AND WHEN AN ADD TO THE SECURITY OF THE SECURITY AND THE CONTANT AND THE WHEN AND ADD TO THE SECURITY OF THE SECURITY AND THE SECURITY MARKED AND ADD TO THE SECURITY OF THE SECURITY AND THE SECURITY MARKED AND ADD THE SECURITY OF THE SECURITY AND THE SECURITY MARKED AND ADD THE SECURITY OF THE SECURITY AND THE SECURITY MARKED AND ADD THE SECURITY OF THE SECURITY AND THE SECURITY MARKED AND ADD THE SECURITY OF THE SECURITY AND THE SECURITY MARKED ADD THE SECURITY OF THE SECURITY AND THE SECURITY AND THE SECURITY MARKED ADD THE SECURITY OF THE SECURITY AND THE SECURTY AND THE SECURITY AND THE SE
R = 1, T = 56 L = 11 E = 00. 17.31 PT S	(32.80°) 3.39° 6.73° 98° 174° = 17 TA° = 17 N° SPEEL 00.00 + 25 00.00 + 25 00.00 - 25 - 2	о+29.57 1446.30 ) = <u>55 мрн</u> 640 630 620	AUTHORITY SENSITIVE SECURITY INFORMATION Weight and the second of the second of the second with a back of the second of the seco
R = 1, T = 56 L = 11 E = 00. 17.31 PT S	(32.80°) 3.39° 6.73° 98° 174° = 17 TA° = 17 N° SPEEL 00.00 + 25 00.00 + 25 00.00 - 25 - 2	о+29.57 1446.30 0 = <u>55 мрн</u> 640 630 620 610	AUTHORITY SENSITIVE SECURITY INFORMATION UNDER SECOND CONTANTS SOMETING SECOND WITGH DECOND THAT IS CONTROLLED UNDER SECOND CONTANTS SOMETING SECOND WITGH DECOND THAT IS CONTROLLED UNDER SECOND CONTANTS SOMETING SECOND WITGH DECOND THAT IS CONTROLLED UNDER SECOND CONTANTS SOMETING SECOND WITGH DECOND THAT IS CONTROLLED UNDERSTITUTION OF THE SECOND WITGH DECOND THAT IS AND ISSUE CONTANTS DECOMPCE IS CONTROLLED BY DELECTION OF THE SECOND WITGH DECONDER TO MENT DECOMPCE IS CONTROLLED BY DELECTION OF THE SECOND WITGH DECONDER TO MENT DECOMPCE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING.  REED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST OPTION  7 OF 51 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE
R = 1, T = 58 L = 11 E = 00. 7.31 PT S	(32.80°) 3.39° 6.73° 7A. = 17 TA. = 17 N SPEEL 00.00 + 52 41 52 41 52 41 52 41 52 41 52 52 52 52 52 52 52 52 52 52	о+29.57 1446.30 0 = <u>55 мрн</u> 640 630 620 610 600	AUTHORITY SENSITIVE SECURITY INFORMATION UNder a CHT PART 13 AD 130. WY ON PART OF THE RECORD WAY & DOUBLED WHEN A WITH A NOTION OF THE SECURITY WAY BUILDEN THAT IS CONTROLLED WHEN A WITH A NOTION OF THE SECURITY OF THE WAY BUILDEN TO THE SECTION TO THE SECTION OF THIS DRAWING. DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALLTERNATIVE EAST OPTION 7 OF 51 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE FILE NAME CTARLE_CT_PP_UPRRE_PREF_104.dgn



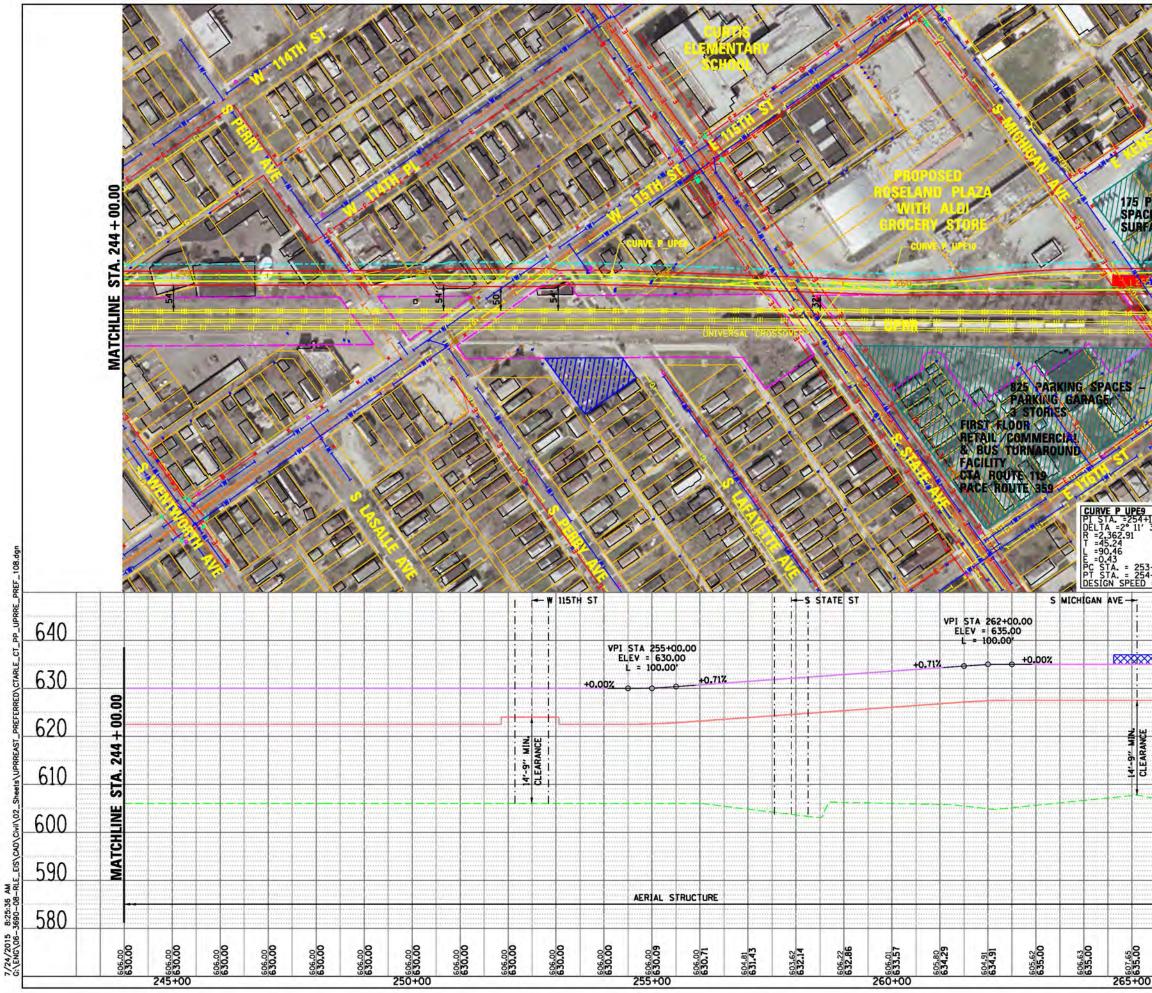
R = 2,362,31 T = 73,61' L = 147,17' E = 1,15' 85,12 PT STA. = 182 PT STA. = 184 : 55 MPH DESIGN SPEED		SENSITIVE SECURITY INFORMATION WHEN IN THE RECENT COMMAN SELECTINE SECURITY INFORMATION THAT IS CONTROLLED WHEN IN COMPANY AND THE DISCO. NO THIS RECORD WAT IE DISCUSSED TO PRESENG WHEN TAY NEED TO DOM'N 45 GENERAL OF THIS THE DISCUSSED WHENTING TO THE SECRETARY OF THEORY OF THEORY TO THE DISCUSSED RELAKE WAY RESULT IN CONTENT OF THEORY OF THEORY OF THEORY TO THE DISCUSSED RELAKE WAY RESULT IN CONTENT OF THEORY OF THEORY OF THEORY OF THEORY OF THEORY OF BELOSUME IS CONTENTED IN THE SECRETARY OF THEORY OF THE DISCUSSED RELAKE WAY RESULT IN CONTENTS OF THE SECRETARY OF THEORY OF THEORY OF THE OFFICE AND THE DISCUSSED RELAKE WAY RESULT IN CONTENTS OF THE SECRETARY OF THEORY OF THE OFFICE AND THE DISCUSSED RELAKED WAY DISCUSSED AND THE SECRETARY OF THEORY OF THEORY OF THEORY OF THEORY OF THE OFFICE AND THE OFFICE AND THE DISCUSSED RELAKED WAY RESULT IN CONTENTS OF THE SECRETARY OF THEORY OF THE OFFICE AND T
Curve P UPes           55           PI STA. =183+           D7" (LT)           PI STA. =183+           DELTA = 3* 34'           R = 2,362,91'           T = 7,361'	70.21 07" (RT)	PRELIMINARY DRAFT PRELIMINARY DRAFT
CHLINE STA. 198 + 00.00		EXISTING WATER LINES EXISTING SEWER LINE EXISTING GAS LINE EXISTING GAS LINE EXISTING ELECTRIC LINE EXISTING TELEPHONE LINE ENTRANCES PRIMARY ENTRANCE SECONDARY ENTRANCE EXIT ONLY PROPOSED PARKING ENTRANCE PROFILE EX GROUND PR TOP OF RAIL PR TOP OF RAIL
	-	LEGEND         PLAN         PR         PR CENTERLINE         PR CENTERLINE         PR TRACK CENTERLINE         PR TRACK CENTERLINE         PR TRACK STRUCTURE         EX ROADWAY RIGHT-OF-WAY         PR PERMANENT ENVELOPE         PR ROADWAY/HARDSCAPE         PR BUILDING OUTLINE         PR PARKING         PR CTA SUBSTATION         PR UTRACK CENTERLINE



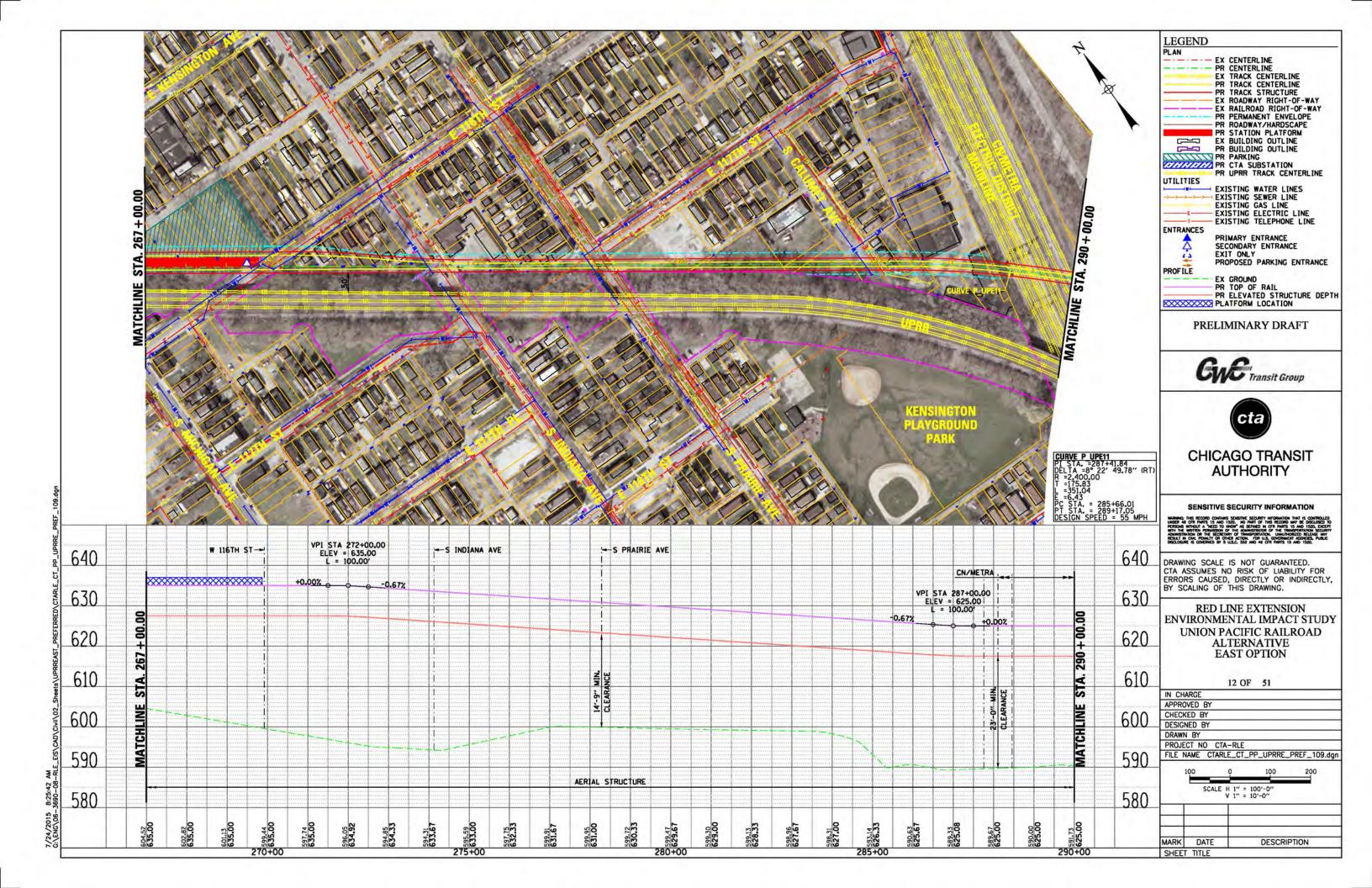
606.81 630.00	630.00	1.	MARK DA	TE	12.00	TION
		580		CALE H 1" = V 1" =	= 100'-0" = 10'-0"	
	2		100	Q	100	200
	Ď	590	FILE NAME	CTARLE_CT	_PP_UPRRE_P	REF_106.dgr
	MATCHLINE		DRAWN BY PROJECT NO		ta a star	
	۳ ۲	600	CHECKED B DESIGNED B			
	S		APPROVED I			
	× .	610	IN CHARGE	9 0	F 51	
	2	C10	45			
	STA. 221 + 00.00	020			OPTION	
	8	620		N PACE	FIC RAILF	
		0.50			EXTENSIONAL IMPAC	
		630	BY SCALING	G OF THIS	DRAWING.	
			CTA ASSUM	USED, DIF	SK OF LIABIL	ITY FOR
		640	Terrature and	1	OT GUARANT	1
		***	PERSONS WITHOUT A WITH THE WRITIEN P ADMINISTRATION OR T RESULT IN CALL POL DISCLOSURE IS COLO	"NEED TO KNOW" AS DRIVESSION OF THE AD HE SECRETARY OF TH ALTY OR OTHER ACTIO	E SECURITY INFORMATION T PART OF THIS RECORD IN DEFINED IN COTR PARTS 15 MISSIPATION, UNAUTHOR MISSIPATION, UNAUTHOR MISSIPATION, UNAUTHOR MISSIPATION, UNAUTHOR AND 49 CFR PARTS 15 /	AND 1520, EXCEPT PORTATION SECURITY ZED RELEASE MAY AGENCIES, PUBLIC MO 1520
PT DE	STA. = 219 STA. = 219 STA. = 22 SIGN SPEED	0+89.28 0+67.52 = 55 MPH	1.		RITY INFORMATION T	
T L	478.23					
	=2.000.00	29.54 2' 01.43"(LT)			ORITY	
10	RVE P UPE6	-(0	CH		D TRAN	SIT
				C	ta	
				(		
	TOL		6	WC	Transit Gro	up
	130			2 0		
			PR	ELIMIN	ARY DRA	FT
- Mit	221+00.00				ATED STRUC	IURE DEPTH
	22				OF RAIL	
55'	STA.2		PROFILE	EXIT ON PROPOSE	ED PARKING E	NTRANCE
-	S			SECOND	ENTRANCE	E
	E				G ELECTRIC I G TELEPHONE	
	TCH		->->->->->->	EXISTIN	G SEWER LIN G GAS LINE	E
	20		UTILITIES	PR UPRF	G WATER LIN	
L. 17.				PR PAR	SUBSTATION	
				EX BUIL	DING OUTLIN	E
				PR PERM	WAY/HARDSC	APE
<b>1</b> 8				- EX ROAD	CK STRUCTUR WAY RIGHT-C ROAD RIGHT-	F-WAY
-				EX TRAC	CK CENTERLIN	Æ
	Þ	C	PLAN	EX CENT		
-						

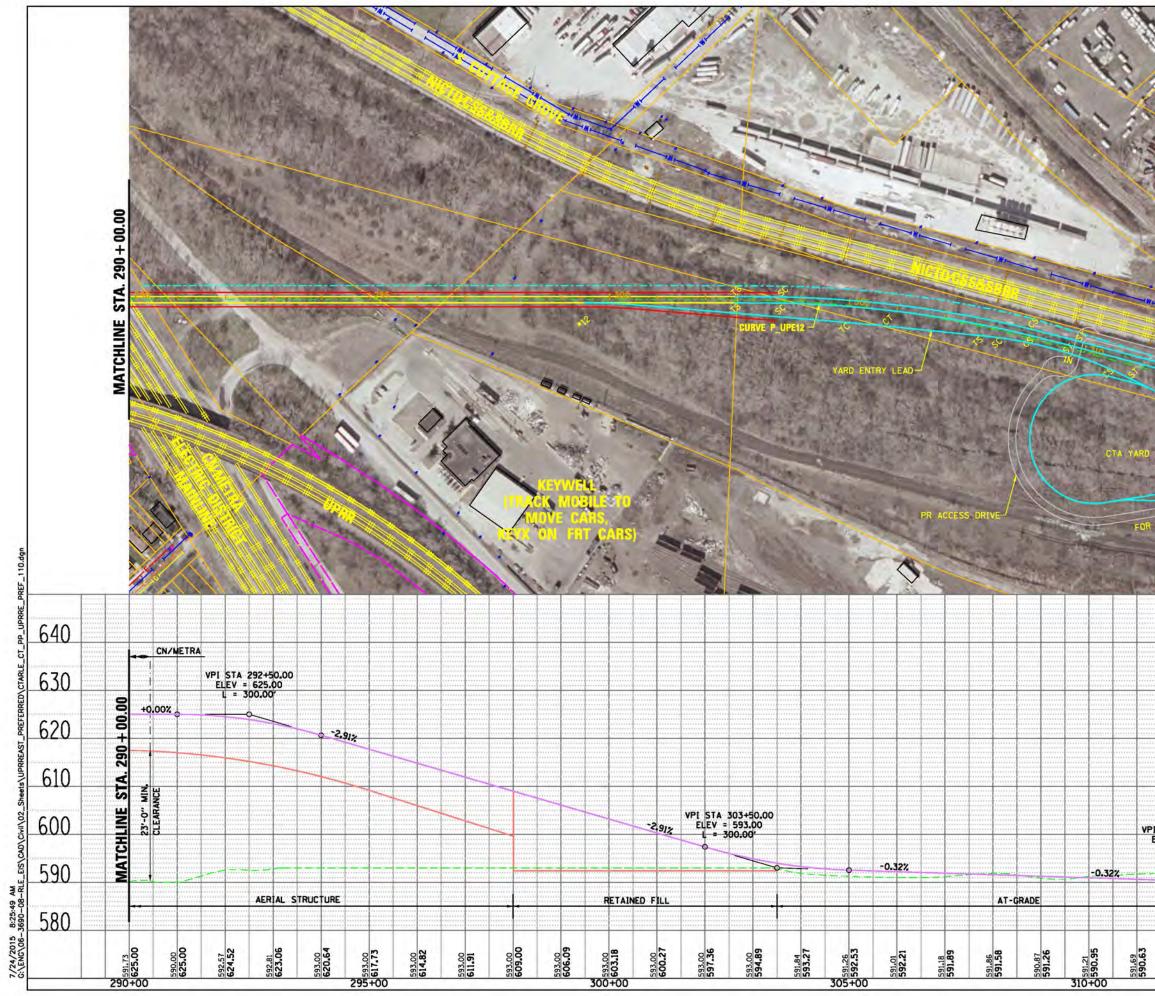


			LEGEND PLAN EX CENTERLINE	
A.	MATE	•	PR CENTERLINE PR CENTERLINE PR TRACK CENTERLINE PR TRACK CENTERLINE PR TRACK STRUCTURE EX ROADWAY RIGHT-OF-WAY EX RAILROAD RIGHT-OF-WAY PR PERMANENT ENVELOPE PR ROADWAY/HARDSCAPE PR STATION PLATFORM EX BUILDING OUTLINE PR PARKING CONTACTOR PR PARKING	
	HIME STA.		UTILITIES EXISTING WATER LINES EXISTING SEWER LINE EXISTING GAS LINE EXISTING ELECTRIC LINE ENTRANCES PRIMARY ENTRANCE	
	244+01-2	3	A SECONDARY ENTRANCE     SECONDARY ENTRANCE     EXIT ONLY     PROPOSED PARKING ENTRANC PROFILE     PROPOSED PARKING ENTRANC PROPOSED PARKING ENTRANC PROPOSED PARKING ENTRANCE PROPOSED PARKING ENT	
	205-		PRELIMINARY DRAFT	
	100	27	Cover Transit Group	
1201	45			
	125		Cla	
0.49 52.99‴(∟T)	CURVE P UPE8 PL STA. =233+ DELTA =30° 56 R =1,695.00 T =469.02 L =915.15	10.45 ' 04.69''(LT)	CHICAGO TRANSIT AUTHORITY	
05.52 73.94 55 MPH	L =915.15 E =63.70 PC STA. = 228 PT STA. = 237 DESIGN SPEED		SENSITIVE SECURITY INFORMATION	ED EDT EPT RIY
05.52 73.94 55 MPH	L =915.15 E =63.70 PC STA. = 228 PT STA. = 237 DESIGN SPEED	+41.42	SENSITIVE SECURITY INFORMATION WIDDOR 49 CTP ANTE 13 AND 1300, NO ANT OF THE RECORD HAVE BE OBSCIDED WIDDOR 49 CTP ANTE 13 AND 1300, NO ANT OF THE RECORD HAVE BE OBSCIDED WITH THE WITTEN FORMASSION OF THE ADMINISTRATION OF THE RECORD HAVE BE OBSCIDED WITH THE WITTEN FORMASSION OF THE ADMINISTRATION OF THE RECORD HAVE BE WITH THE WITTEN FORMASSION OF THE ADMINISTRATION OF THE RECORDINATE SEC WITH THE WITTEN FORMASSION OF THE ADMINISTRATION OF THE RECORDINATE SEC WITH THE WITTEN FORMASSION OF THE ADMINISTRATION OF THE RECORDING SEC WITH THE WITTEN FORMASSION OF THE ADMINISTRATION OF THE RECORDINATE SEC WITH THE WITTEN FORMASSION OF THE ADMINISTRATION OF THE RECORDINATE SEC WITH THE WITTEN FORMASSION OF THE ADMINISTRATION OF THE RECORDING SEC WITH THE WITTEN FORMASSION OF THE ADMINISTRATION OF THE RECORDING SEC WITH THE WITTEN FORMASSION OF THE ADMINISTRATION OF THE RECORDING SEC WITH THE WITTEN FORMASSION OF THE ADMINISTRATION OF THE RECORDING SEC WITH THE WITTEN FORMASSION OF THE ADMINISTRATION OF THE RECORDING ADDRESS. AND BESCHOOL OF THE ATTENT FORMASSION OF THE ADMINISTRATION OF THE RECORDING ADDRESS. AND DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR	
05.52 73.94 55 MPH	L =915.15 E =63.70 PC STA. = 228 PT STA. = 237 DESIGN SPEED	+41.42 +56.57 = 55 MPH	SENSITIVE SECURITY INFORMATION WHEN IN THE RECORD CONTAINS SOMETING SCHEME WITHOUT THE RECORD WHEN IN THE INFORMATION INCOMENTATION IN THE RECORD WITH IS CONTAIN WHEN IN THE WITTON POWER OF THE RECORD WITH IS CONTAIN WHEN IN THE WITTON POWER OF THE RECORD WITH IS CONTAIN WHEN IN THE WITTON POWER OF THE RECORD WITH IS CONTAIN ADMINISTRATION OF THE SECOND OF THE RECORD WITH IS CONTAIN ADMINISTRATION OF THE SECOND OF THE RECORD WITH IS CONTAIN ADMINISTRATION OF THE SECOND WITH IS CONTAIN ADMINISTRATION OF THE SECOND WITH IS CONTAIN ADMINISTRATION OF THE SECOND WITH IN THE ADMINISTRATION OF THE INVESTIGATION SECOND ADMINISTRATION OF THE SECOND WITH IN THE ADMINISTRATION OF THE INVESTIGATION SECOND ADMINISTRATION OF THE SECOND WITH IN THE ADMINISTRATION OF THE INVESTIGATION SECOND ADMINISTRATION OF THE SECOND WITH IN THE ADMINISTRATION OF THE INVESTIGATION SECOND DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY BY SCALING OF THIS DRAWING. RED LINE EXTENSION	Y,
05.52 73.94 55 MPH	I = -163.02 E = 915.15 E = 63.70 PC STA. = 228 PT STA. = 237 DESIGN SPEED	+41.42 +56.57 = 55 мрн	SENSITIVE SECURITY INFORMATION WHEN BY THE RECORD CONTAINS SOMETING SCORES WITHOUT IN THE CONTROL WHEN BY THE RECORD CONTAINS SOMETING SCORES IN THE INFORMATION WHEN AND AND AND AND AND AND AND AND AND AN	Y,
05.52 73.94 55 MPH	00000 +	+41.42 +56.57 = 55 мрн 640 630	SENSITIVE SECURITY INFORMATION WERE THE RECORD CONTANT SECURITY INFORMATION WERE IT PARTY IS AND ISOL OF A PARTY INFORMATION THAT IS CONTANT RESOLUTION A VERT IN POOL AS CONTANT SECURITY INFORMATION WITH BE OBLIGHT RESOLUTION A VERT INFORMATION OF THE INFORMATION SECURITY INFORMATION AND ISOL OF AND ADMINISTRATION OF THE INFORMATION SECURITY INFORMATION AND ADMINISTRATION OF THE INFORMATION AND ADMINISTRATION AND ADMINISTRATION	Y,
05.52 73.94 55 MPH	<b>E</b>	+41.42 +56.57 = 55 мрн 640 630 620	SENSITIVE SECURITY INFORMATION WHEN IN THE ADDA SECURITY INFORMATION WHEN IN THE ADDA SECURITY INFORMATION THAT IS CONTROL WHEN INFORMATION AND ISO. NO PART OF THE RECORD WHY BE DESCRET PRESENT AND THE SECURITY OF THE RECORD WHY BE DESCRET PRESENT AND THE SECURITY OF THE RECORD WHY BE DESCRET PRESENT AND THE SECURITY OF THE RECORD WHY BE DESCRET DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTL BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUL UNION PACIFIC RAILROAD ALTERNATIVE EAST OPTION 10 OF 51 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY	Y,
05.52 73.94 55 MPH	U - 103.02 = 915.15 E - 63.70 PC STA. = 228 PT STA. = 237 DESIGN SPEED VE 00:00 + + + + + - - - - - - - - - - - - -	+41.42 +56.57 = 55 мрн 640 630 620 610	SENSITIVE SECURITY INFORMATION           WHERE IN A RECORD CONTANT SECURITY INFORMATION TWI IS CONTANT           WHERE IN A RECORD CONTANT SECURITY INFORMATION TWI IS CONTANT           INFORMATION TWI IS AND ISO. NO PART OF THE RECORD WITH B CONTANT           INFORMATION TWI IS CONTANT           INFORMATION TWICE IS AND THE IS AND ISO.           INFORMATION TO THE STATE ISON           INFORMATION TO THE STATE ISON           INFORMATION TAIL INPACT STUDI           INFORMATION TAIL INPACT STUDI           INFORMATION TAIL INPACT STUDI           INFORMATION TAIL INPACT STUDI           INFORMATION           INFORMA	Y,
05.52	<b>E</b>	+41.42 +56.57 = 55 мрн 640 630 620 610 600	SENSITIVE SECURITY INFORMATION UNderstein States St	Y,

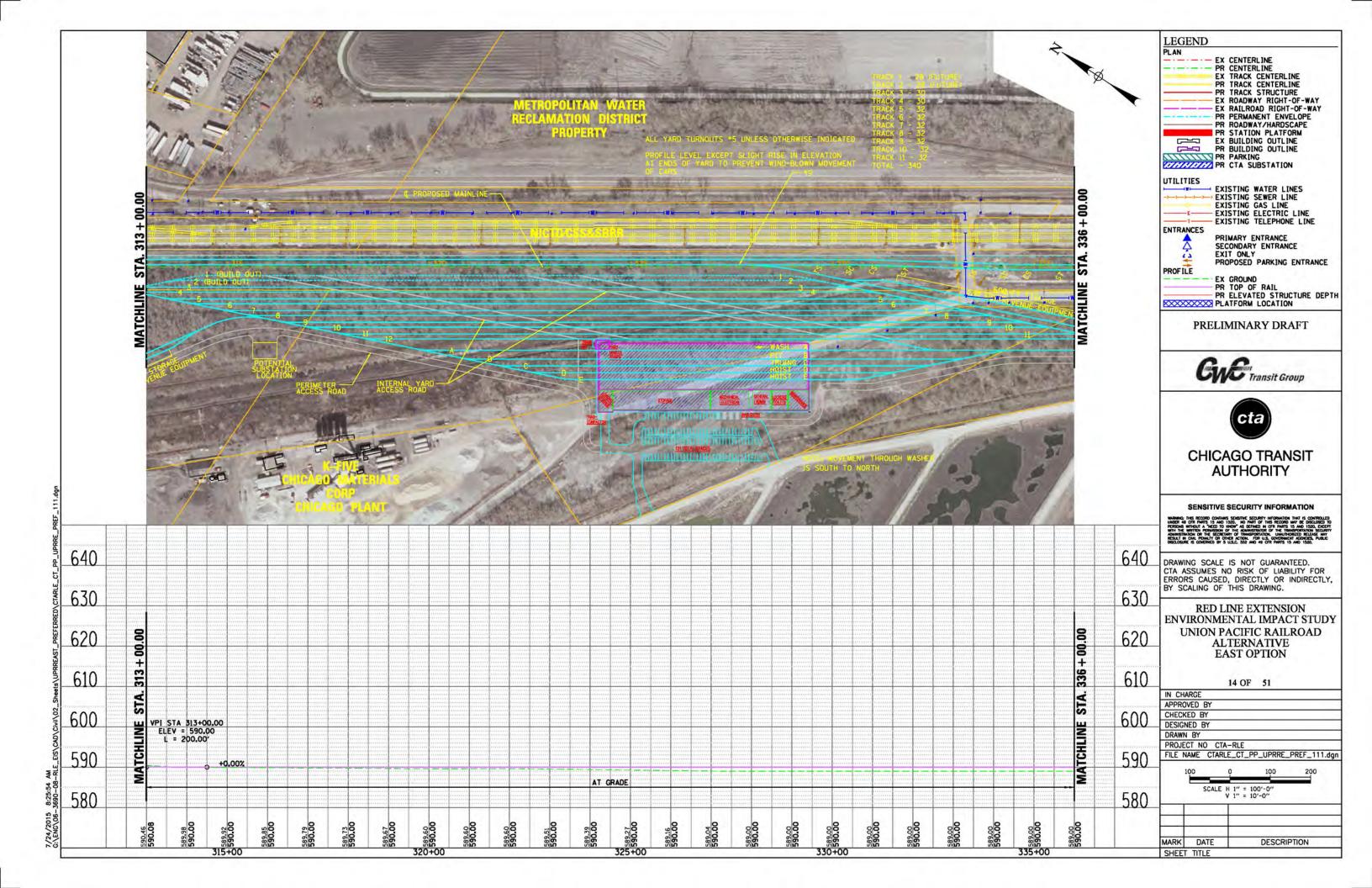


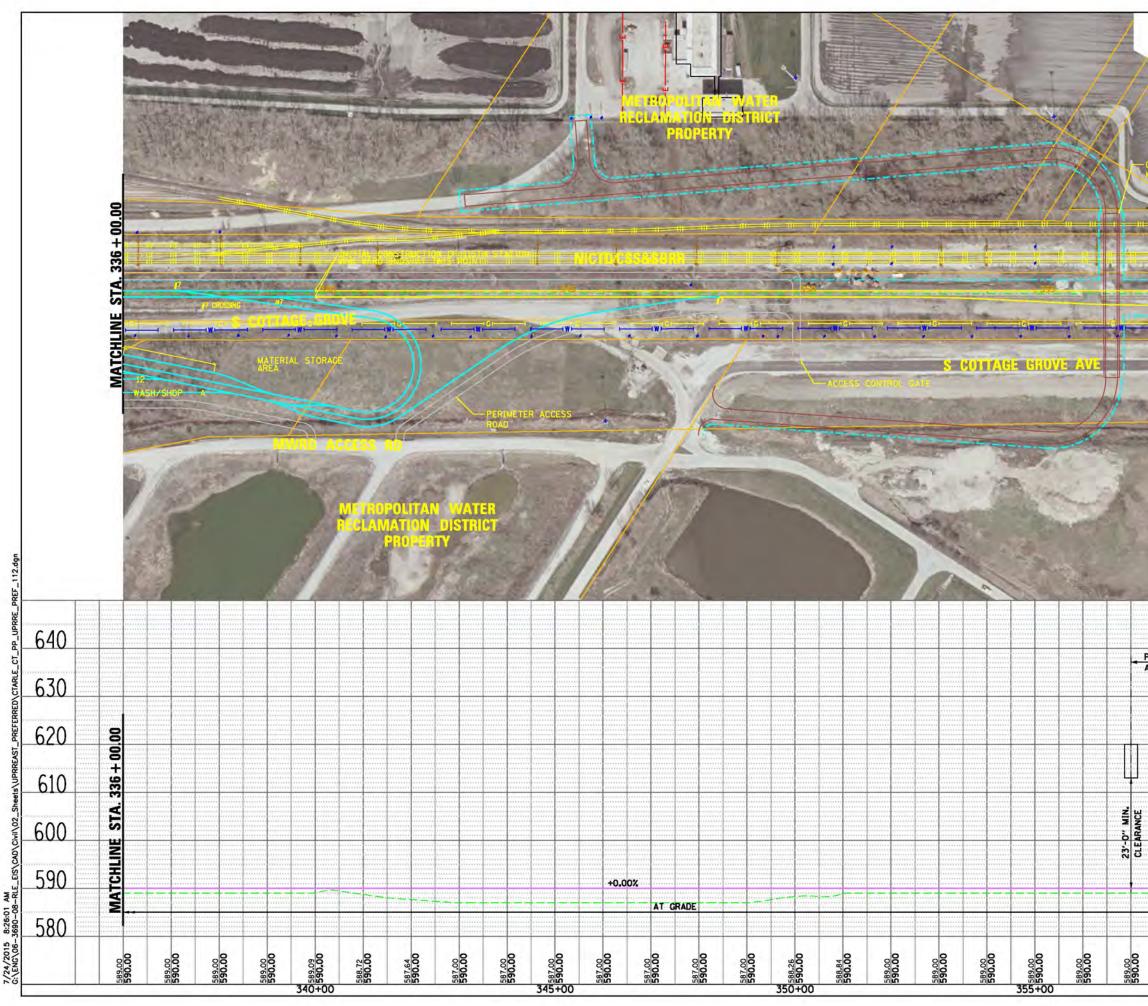
10		LEGEND
H		PLAN       EX CENTERLINE         PR CENTERLINE       PR TRACK CENTERLINE         PR TRACK CENTERLINE       PR TRACK STRUCTURE         PR TRACK STRUCTURE       EX ROADWAY RIGHT-OF-WAY         EX RALROAD RIGHT-OF-WAY       EX RALROAD RIGHT-OF-WAY         PR PERMANENT ENVELOPE       PR OADWAY/HARDSCAPE         PR STATION PLATFORM       EX BUILDING OUTLINE         PR PARKING       PR UILDING OUTLINE         PR PARKING       PR UPRR TRACK CENTERLINE         UTILITIES       PR UPRR TRACK CENTERLINE
00:00		EXISTING WATER LINES EXISTING SEWER LINE EXISTING GAS LINE EXISTING ELECTRIC LINE EXISTING TELEPHONE LINE
STA. 267 -		ENTRANCES PRIMARY ENTRANCE SECONDARY ENTRANCE EXIT ONLY PROFILE PROFILE EX GROUND
		PR TOP OF RAIL PR ELEVATED STRUCTURE DEPTH PR ELEVATED STRUCTURE DEPTH
AATCH		PRELIMINARY DRAFT
		Gy E Transit Group
T		cta
CURVE P UPE10 PI STA. =259+ DELTA =2° 23' R =2,362.91 T =49.35 L =98.69	99.68 35" (LT)	CHICAGO TRANSIT AUTHORITY
E =0.52 PC STA. = 259 PT STA. = 260	+50.32 +49.01	SENSITIVE SECURITY INFORMATION
DESIGN SPEED	- 55 MFH ]	WARKE THE RECORD COMMAN SHELTINE SECTION WORKNOW THAT IS CONTROLLED UNDER 46 CP 10475 13 AM 01 300. NO APRIL OT HIS RECORD WITE DOCUMED TO PRESIDE WITCH A NEED TO DOT AS GRAND WITE TO APRIL 300. 1333. LOCAT ANALYSING ON THE SECTION OF THE SECTION. UNALL DOCUMENT AS A RESULT IN ONL POWERT OF THE SECTION. UNALL DOCUMENT AS A NUME RESOLUTE IS CONSISTED BY AUX. SIST AND A OF HIMTS IN AND 1350.
	640	DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING.
8	630	RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY
57 + 00.	620	UNION PACIFIC RAILROAD ALTERNATIVE EAST OPTION
STA. 2(	610	11 OF 51
AN ALLEADER THE COURT ALL COURSE	600	APPROVED BY CHECKED BY DESIGNED BY DRAWN BY
MAT	590	PROJECT NO CTA-RLE FILE NAME CTARLE_CT_PP_UPRRE_PREF_108.dgn
	580	100 0 100 200 SCALE H 1" = 100'-0" V 1" = 10'-0"
604.52 635.00		MARK DATE DESCRIPTION
	CURVE P UPEIO PIC STA. = 259+ DEL TA. = 259+ DEL TA. = 259+ PIC STA. = 259 PIC STA. = 259	VIS         MIHO           PIL STA = 259499.68 DELITA = 259499.68 DELITA = 259499.68 DELITA = 259499.68 DELITA = 259450.32 PT STA = 260449.01 DESIGN SPEED = 55 MPH           640           640           630           000           630           000           55 MPH           600           57           610           590           590           590           590           580



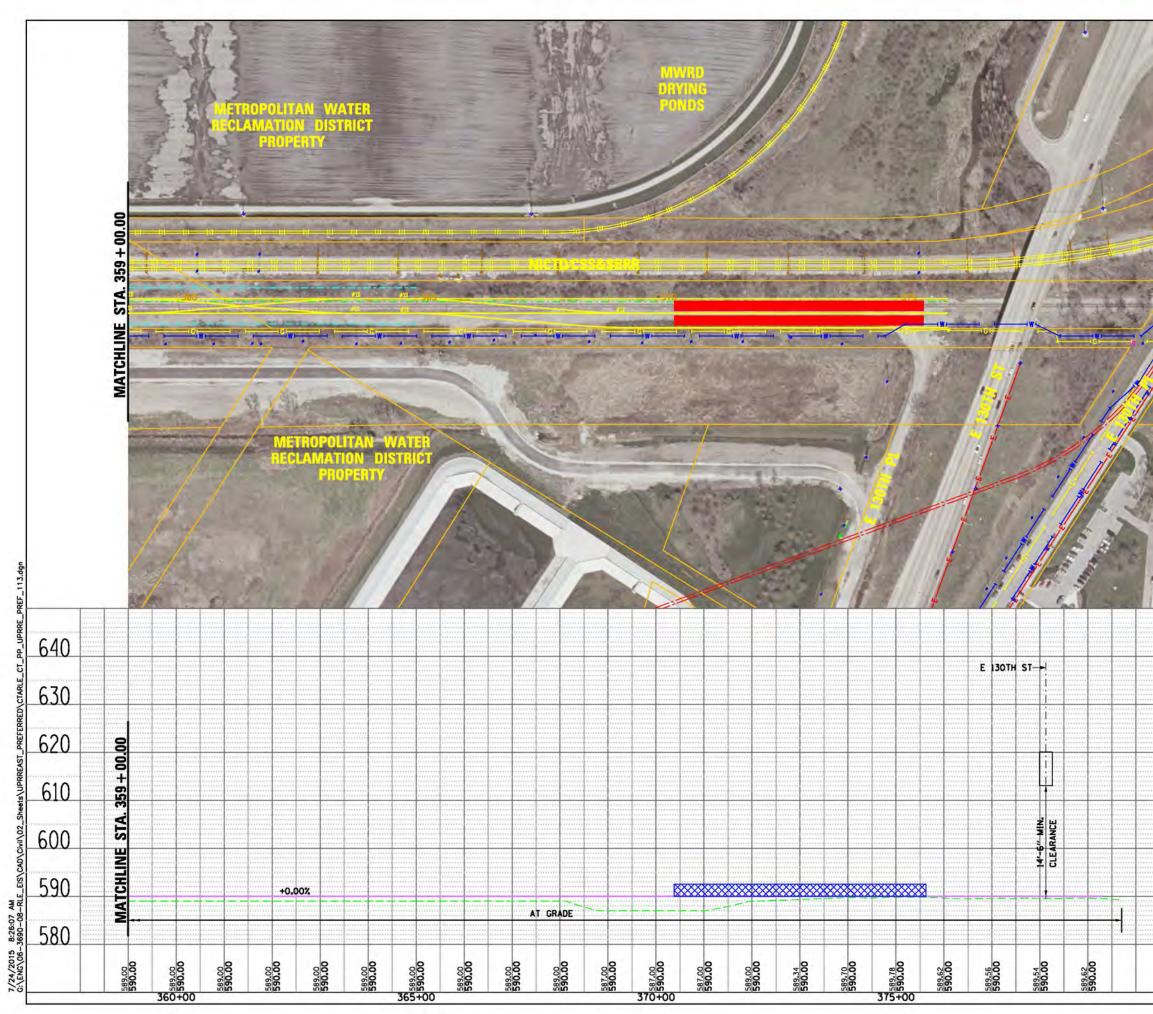


	4		LEGEND
	La concentration of the second	×	PLAN         PR         PR         PR         CENTERLINE         PR         PR
	E STA. 3		GLE
TRACK	IATCHLIN		Gy E Transit Group
NON-REV	<b>CURVE P UP</b> PI STA. =30 DELTA =14° R =2,450.00 T =315.70 L =627.95	<b>E12</b> 5+70.14 41′ 06.85′′(RT)	CHICAGO TRANSIT AUTHORITY
	E =20.26 PC STA. = 3 PT STA. = 3 DESIGN SPEE	302+54.43 308+82.38 ED = 55 MPH	SENSITIVE SECURITY INFORMATION WORKE HIS RECIP COMMAN SALENT MY DIAMATE IN MY SECURITIES INDER 40 CHARTS 13 AND 1500, NO ANY OF HIS RECORD WY E DESLOYED TO PERSON WINDUT A NEED TO NOT AS REVIEW IN O'TH HIS COOK WY E DESLOYED TO ANY ANY OF THE SALENT OF THE SALENT AND THE SALENT ADMINISTRATION OF THE SECOND COMMAND AND THE SALENT RESULT IN CAN. PONULT OF OTHER ACTION. UNAL NOTIONAL DESLOYED RECORDER IS COORDED BY ALL SALE AND A ON A OTHER ACTION. THAT IS RECORDER IS COORDED BY ALL SALE AND A ON A OTHER ACTION.
		640	DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY,
· · · · · · · · · · · · · · · · · · ·	1	******	
	8	630	BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY
	113 + 00.00	630 620	BY SCALING OF THIS DRAWING. RED LINE EXTENSION
	: STA. 313 + 00.00		BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST OPTION 13 OF 51 IN CHARGE
LEV = 590.		620	BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST OPTION 13 OF 51 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE
LEV = 590.		620 610	BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST OPTION 13 OF 51 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE
<u>STA 313+0</u> LEV = 590.00 L = 200.00		620 610 600	BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST OPTION 13 OF 51 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DESIGNED BY PROJECT NO CTA-RLE FILE NAME CTARLE_CT_PP_UPRRE_PREF_110.dgn

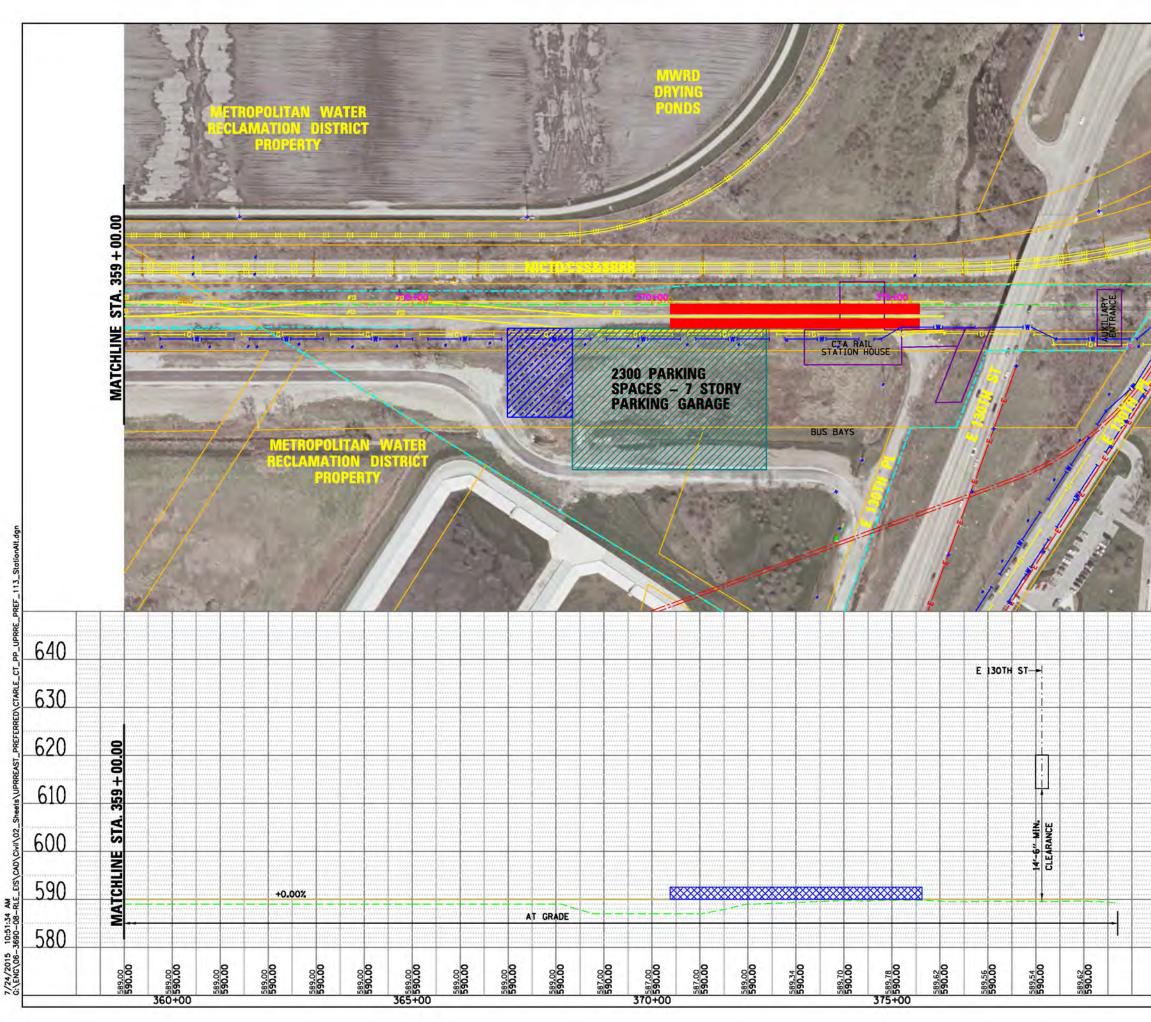




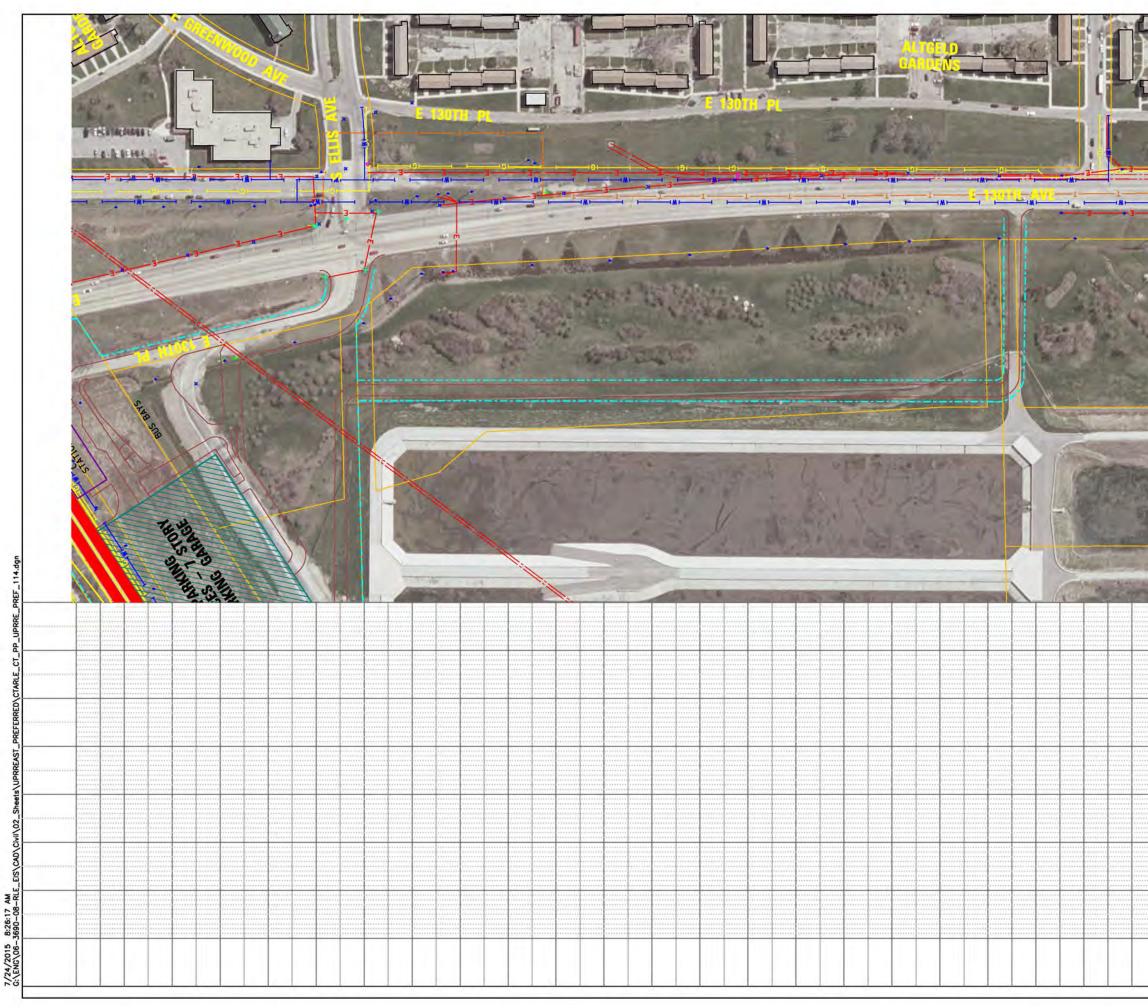
T			
ADE SEPARATEO	MATCHLINE STA. 359 + 00.00	•	PLAN         PR         PR         PR         PR         PR         TRACK CENTERLINE         PR         PR TRACK CENTERLINE         PR TRACK CENTERLINE         PR TRACK CENTERLINE         PR TRACK CENTERLINE         PR TRACK STRUCTURE         EX ROADWAY RIGHT-OF-WAY         PR PERMANENT ENVELOPE         PR ROADWAY/HARDSCAPE         PR BUILDING OUTLINE         PR BUILDING OUTLINE         PR PARKING         EXISTING WATER LINES         PR EXISTING SEWER LINE         EXISTING GAS LINE         EXISTING GAS LINE         EXISTING GAS LINE         EXISTING CLECTRIC LINE         EXISTING CAS LINE         EXISTING TELEPHONE LINE         E
	MATC		PRELIMINARY DRAFT
			<b>Gy C</b> Transit Group
			CHICAGO TRANSIT AUTHORITY INFORMATION SENSITIVE SECURITY INFORMATION WHER 45 APR AND A SECOND CONTACT SECOND AND A SECOND A SECOND A SECOND A SECOND AND A SECOND A S
MWRD		640	DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY,
ESS ROAD		630	RED LINE EXTENSION
	00.00	620	ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST OPTION
	STA. 359 + 00.00	610	15 OF 51
	ATTACAL CONTRACTOR	600	APPROVED BY CHECKED BY DESIGNED BY DRAWN BY
	MATCHLINE	590	PROJECT NO CTA-RLE FILE NAME CTARLE_CT_PP_UPRRE_PREF_112.dgn 100 0 100 200
		580	SCALE H 1" = 100'-0" V 1" = 10'-0"
590.00 590.00 590.00	00.065	000	



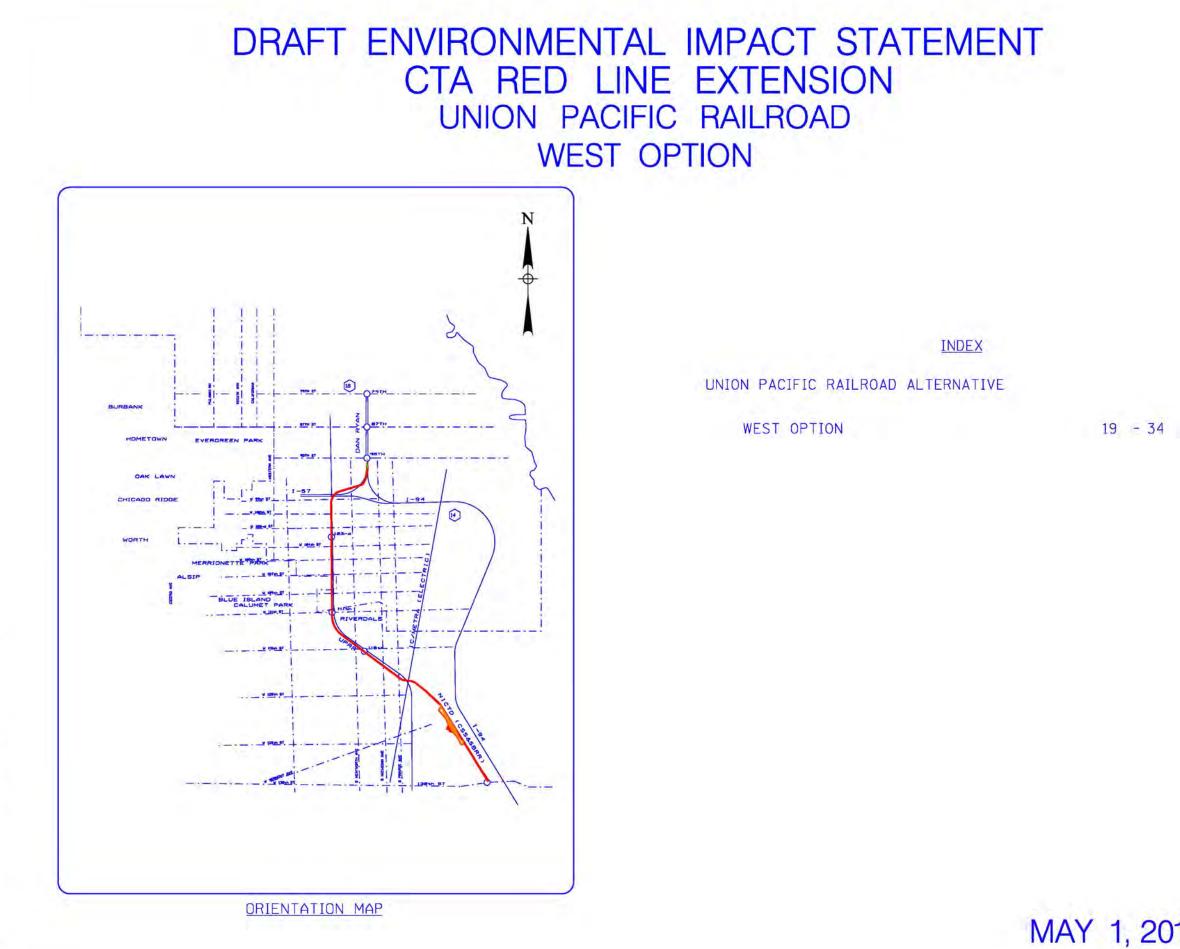
		LEGENE
4		LEGEND PLAN
TX		EX CENTERLINE
× X		PR TRACK CENTERLINE PR TRACK STRUCTURE
		EX ROADWAY RIGHT-OF-WAY
1	•	PR PERMANENT ENVELOPE
		PR ROADWAY/HARDSCAPE
	1 1 12	
		PR BUILDING OUTLINE
	His	PR PARKING PR CTA SUBSTATION
111	$\parallel $	UTILITIES
	Lawrence -	EXISTING WATER LINES
		EXISTING SEWER LINE
12,114		E EXISTING ELECTRIC LINE
	N DUNY	ENTRANCES
1. 11.	A PARTY A	
The Ill of the	The part of the second	EXIT ONLY
11 Design	the second second	PROPOSED PARKING ENTRANCE
31	2	EX GROUND
1112		PR TOP OF RAIL PR ELEVATED STRUCTURE DEPTH
	-	
		PRELIMINARY DRAFT
	2 and	0.0
- Mr Male		Gy En Transit Group
1 main	B - Con BA	(ata)
		Cla
		CHICAGO TRANSIT AUTHORITY
ALTOFLO CALIDORS		SENSITIVE SECURITY INFORMATION
		WHERE THE RECORD COMMENT SECTIONS WITHOUT IN HIS CONTROLLED UNDER 40 FR APRIET 13 AU 1302, IN A HIS TO THE RECORD WITH CONTROLLED 10 PRESON WITHOUT A YEED TO DOOM AS REFINED IN O'T HART 13 AND 1302, DECRT WITH THE WITHOUT APPLICATION OF HIS ADVISORY AND ADVISORY WITHOUT AND ADVISORY OF HIS ADVISORY AND ADVISORY ADVISOR HEST, TH COM, POWLY OF CHER ACTION, COM U.S. CONTROLLED ADVISOR DECLEMENT & COMPRILE DT 9 LISE, 552 AND 40 CT HART 15 AND 1302.
	640	
	040	DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY,
	630	BY SCALING OF THIS DRAWING.
	<u> </u>	RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY
	600	UNION PACIFIC RAILROAD
	620	ALTERNATIVE EAST OPTION
	C10	
	610	16 OF 51
	610	IN CHARGE
		IN CHARGE APPROVED BY
	610 600	IN CHARGE
		IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY
	600	IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE
	600	IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE
		IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE
	600 590	IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DROJECT NO CTA-RLE FILE NAME CTARLE_CT_PP_UPRRE_PREF_113.dgn 100 0 100 200 SCALE H 1" = 100'-0"
	600 590	IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE FILE NAME CTARLE_CT_PP_UPRRE_PREF_113.dgn 100 0 100 200
	600	IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DROJECT NO CTA-RLE FILE NAME CTARLE_CT_PP_UPRRE_PREF_113.dgn 100 0 100 200 SCALE H 1" = 100'-0"
	600 590	IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DROJECT NO CTA-RLE FILE NAME CTARLE_CT_PP_UPRRE_PREF_113.dgn 100 0 100 200 SCALE H 1" = 100'-0"
	600 590	IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DROJECT NO CTA-RLE FILE NAME CTARLE_CT_PP_UPRRE_PREF_113.dgn 100 0 100 200 SCALE H 1" = 100'-0"



*		LEGEND         PLAN         PR         PR CENTERLINE         PR CENTERLINE         PR TRACK CENTERLINE         PR TRACK CENTERLINE         PR TRACK STRUCTURE         PR TRACK STRUCTURE         PR PR TRACK STRUCTURE         PR PR TRACK STRUCTURE         PR PR TRACK STRUCTURE         PR PRAMANENT ENVELOPE         PR ROADWAY HARDSCAPE         PR BUILDING OUTLINE         PR BUILDING OUTLINE         PR PARKING         PR UPRR TRACK CENTERLINE         UTILITIES         EXISTING WATER LINES         EXISTING GAS LINE         EXISTING GAS LINE         EXISTING GAS LINE         EXISTING GAS LINE         EXISTING TELEPHONE LINE         EXISTING TELEPHONE LINE         EXISTING TELEPHONE LINE         PRIMARY ENTRANCE         SECONDARY ENTRANCE         EXIT ONLY         PROPOSED PARKING ENTRANCE         PROPOSED PARKING ENTRANCE
	-	PR TOP OF RAIL PR ELEVATED STRUCTURE DEPTH
-	A STATE OF THE OWNER	
W		PRELIMINARY DRAFT
Sec. 1	S. Aller	Gy E Transit Group
at the		cta
		CHICAGO TRANSIT AUTHORITY
		SENSITIVE SECURITY INFORMATION weeks has record counties solaring security information that is controlled index 40 ref remarks 13 and 1300, in o hard or has record ware to declared to refere the second of the second ware to declared to refere the second of the second ware to declare ware result in our power of these records of use or other and to be compared to the second of the second bar of the second second of the second of the second of the second bar of the second of the second of the second of the second bar of the become is condenied to black down and of the merit is and 1300.
	640	DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR FRORS CAUSED DIRECTLY OR INDIRECTLY
	630	BY SCALING OF THIS DRAWING.
	620	RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST OPTION
	610	17 OF 51
	010	IT OF 51
	600	APPROVED BY CHECKED BY DESIGNED BY DRAWN BY
	590	PROJECT NO CTA-RLE FILE NAME CTARLE_CT_PP_UPRRE_PREF_113_Sto
	580	SCALE H 1" = 100'-0" V 1" = 10'-0"
		MARK DATE DESCRIPTION



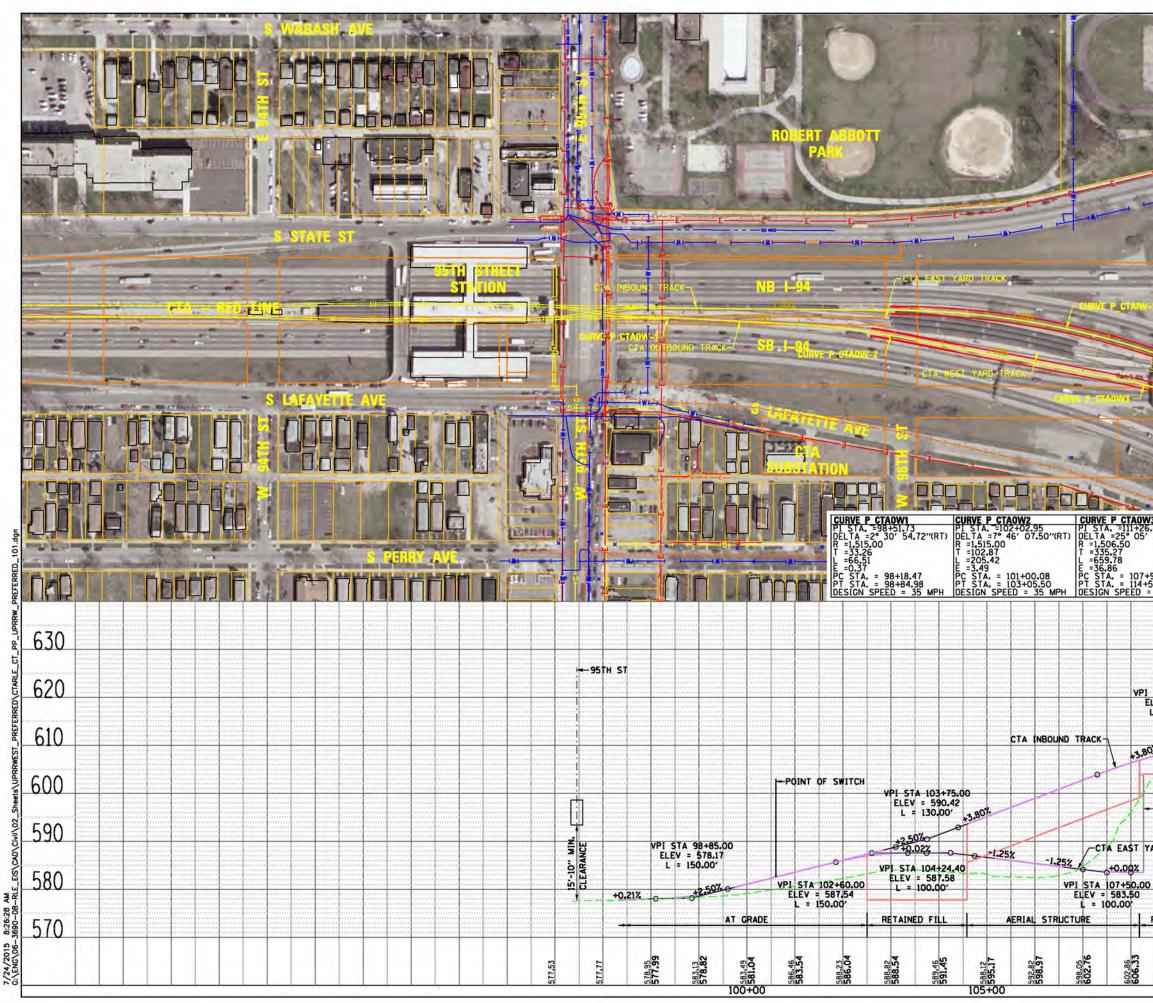
all the				LEGEND
			4	PLAN
	-			
14	28			PR TRACK CENTERLINE
-	in m			PR TRACK STRUCTURE
	-		T	EX ROADWAY RIGHT-OF-WAY
100	-			PR PERMANENT ENVELOPE
				PR ROADWAY/HARDSCAPE
100			NT	
2			IN.	PR BUILDING OUTLINE
2	100			PR UPRR TRACK CENTERLINE
-	-			UTILITIES
π.	-	L		
				EXISTING GAS LINE
		-	-	EXISTING ELECTRIC LINE
			and the second second	ENTRANCES
	1	. 92	2.	
121	and an	- 21	100	EXIT ONLY PROPOSED PARKING ENTRANCE
	A 23			PROFILE
-				PR TOP OF RAIL
2			-	PR ELEVATED STRUCTURE DEPTH
Yes	1-1-1-1		100	EXXXXXX PLATFORM LOCATION
		1	-	PRELIMINARY DRAFT
				6.6
	32000	+	5320	<b>Gyve</b> "Transit Group
1 mil	12			
		SV:C		CHICAGO TRANSIT AUTHORITY
	1000		-	SENSITIVE SECURITY INFORMATION
7-5	6	100	Local .	WARNING: THIS RECORD CONTAINS SENSITIVE SECURITY INFORMATION THAT IS CONTROLLED UNDER 49 CFR PARTS 15 AND 1520. NO PART OF THIS RECORD MAY BE DESCUBED TO PRESONS WITHOUT A "VEDT TO INFORM" AS DEFINED IN CITY PARTS 15 AND 1520. DECEPT
				UNDER HE OTH PARTE 15 MO 1520, NO ART OF THE RECORD WAY BE DEGLOBED TO PRISONS WITHOUT A TWO TO IN OWN AS BOTHOD IN OF RANTE 13 MO 1530, DECENT WITH THE WITTHIN POLYMESON OF THE ADMINISTRATION FRANTE STAND WITH THE WITTHIN POLYMESON OF THE ADMINISTRATION FRANTE STAND RESULT IN DOA PHYLING TO POLYMESON, TOTAL USE ADMINISTRATION SECURITY RESULT IN DOA PHYLING TO POLYMESON, TOTAL USE ADMINISTRATIC SECURITY RESULT IN DOA PHYLING TO POLYMESON, TOTAL USE ADMINISTRATIC SECURITY RESULT IN DOA PHYLING TO POLYMESON, TOTAL USE ADMINISTRATIC SECURITY RESULT IN DOA PHYLING TO POLYMESON, TOTAL TOTAL PHYLING TO POLYMEST RESULT IN DOA PHYLING TO POLYMESON, TOTAL PHYLING TO POLYMEST RESULT IN DOA PHYLING TO POLYMESON, TOTAL PHYLING TO POLYMESON, TOTAL RESULT IN DOA PHYLING TO POLYMESON, TOTAL PHYLING TO POLYMESON, TOTAL PHYLING RESULT IN DOA PHYLING TO POLYMESON, TOTAL PHYLING TO POLYMESON, TOTAL PHYLING RESULT IN DOA PHYLING TO POLYMESON, TOTAL PHYLING TO POLYMESON, TOTAL PHYLING RESULT IN DOA PHYLING TO POLYMESON, TOTAL PHYLING TO POLYMESON, TOTAL PHYLING RESULT IN DOA PHYLING TO POLYMESON, TOTAL PHYLING TO POLYMESON, TO POLYMESON, TOTAL PHYLING RESULT IN DOA PHYLING TO POLYMESON, TOTAL PHYLING TO POLYMESON, TO POLY
		****		DRAWING SCALE IS NOT GUARANTEED.
		****		CTA ASSUMES NO RISK OF LIABILITY FOR
	Turner	****	reality ration	ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING.
		· · · · · · · · · · · · · · · · · · ·	*******	RED LINE EXTENSION
			**************************************	ENVIRONMENTAL IMPACT STUDY
				ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD
				ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE
				ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD
				ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST OPTION
				ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST OPTION 18 OF 51
				ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST OPTION 18 OF 51 IN CHARGE
				ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST OPTION 18 OF 51
				ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST OPTION 18 OF 51 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY
				ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST OPTION  18 OF 51 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY
				ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST OPTION 18 OF 51 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE
				ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST OPTION 18 OF 51 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE FILE NAME CTARLE_CT_PP_UPRRE_PREF_114.dgn
				ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST OPTION 18 OF 51 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE
				ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST OPTION 18 OF 51 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE FILE NAME CTARLE_CT_PP_UPRRE_PREF_114.dgn
				ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST OPTION 18 OF 51 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE FILE NAME CTARLE_CT_PP_UPRRE_PREF_114.dgn
				ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST OPTION 18 OF 51 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE FILE NAME CTARLE_CT_PP_UPRRE_PREF_114.dgn
				ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST OPTION 18 OF 51 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE FILE NAME CTARLE_CT_PP_UPRRE_PREF_114.dgn
				ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST OPTION 18 OF 51 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE FILE NAME CTARLE_CT_PP_UPRRE_PREF_114.dgn



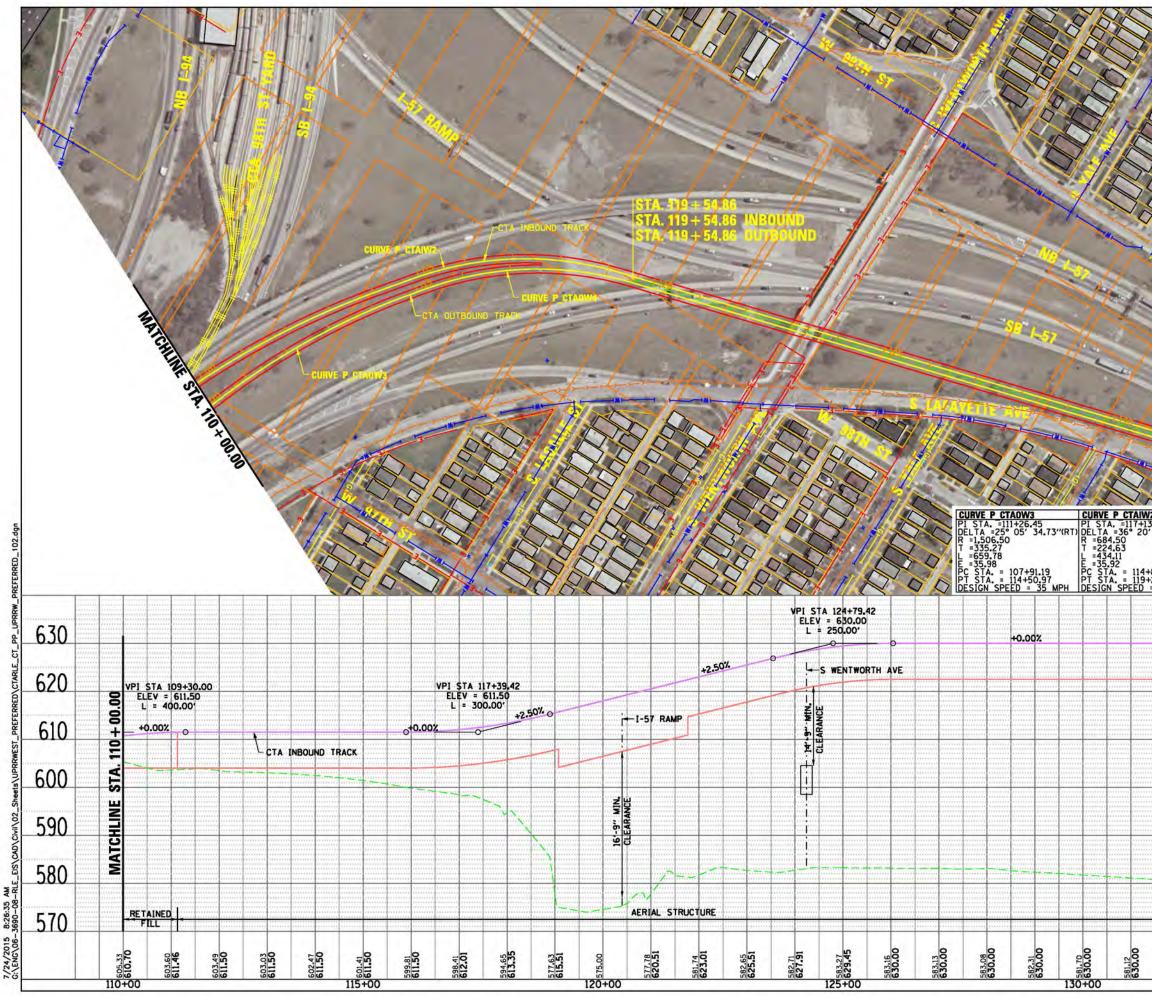
7/24/2015 8:26:24 AM G:\ENG\06-3690-08-RLE

MAY 1, 2015

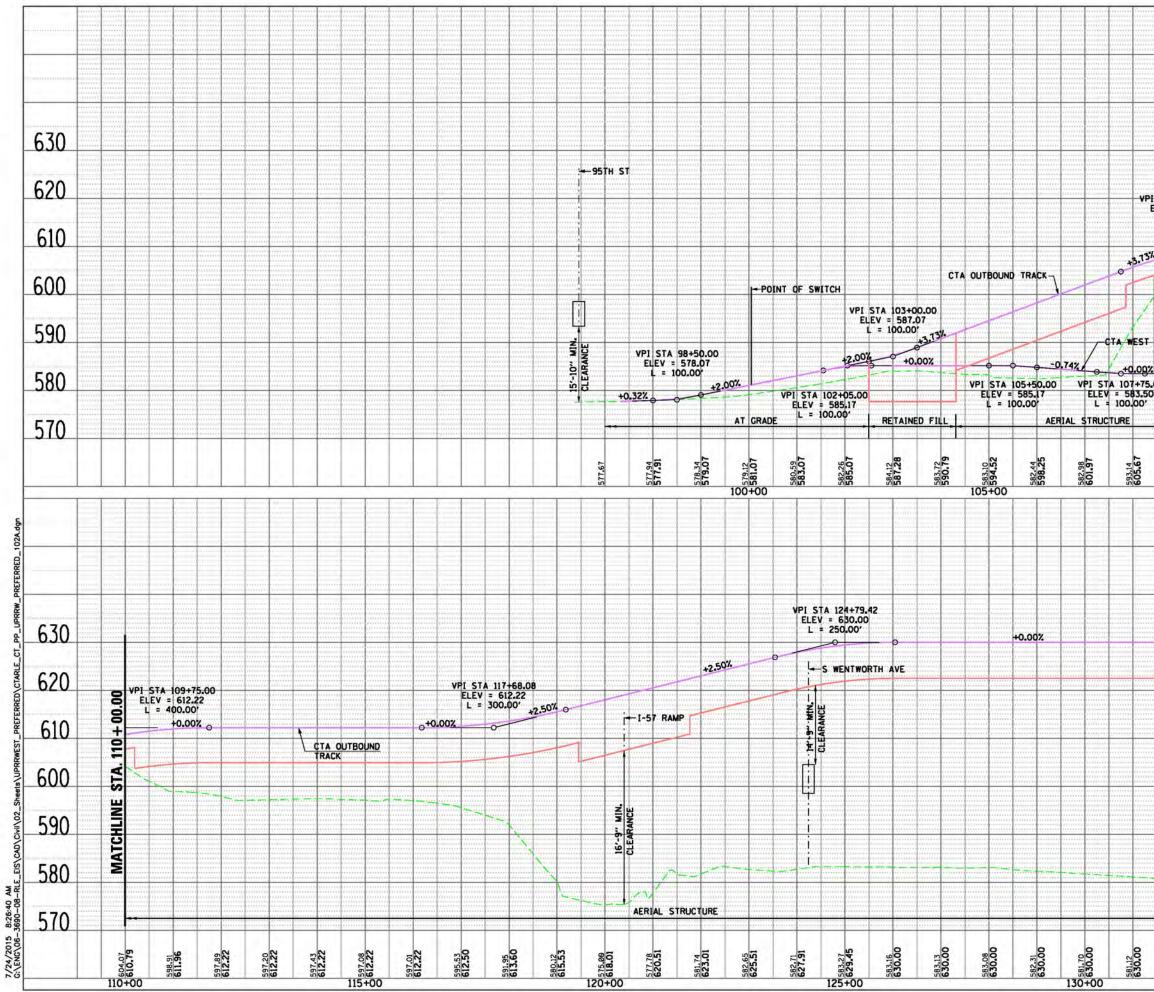
	PRELIN	MINARY DRAFT
	Gw	<b>E</b> Transit Group
		cta
		GO TRANSIT
		SECURITY INFORMATION
UNDER 4 PERSONS WITH THE ADMINIST RESULT I DISCLOSE	Instructure Contrains I CTR PARTS 15 AND 1. WITHOUT A "NEED TO K WITHOUT A "NEED TO K WITHOUT A "NEED TO K MATCH OR THE SECRETA ATCH OR THE SECRETA IN CALL PENALTY OR OTH RE IS COVERNED BY 5 1	I SPEINE SCORT WORKNOW THAT IS CONTROLLD 200, NO PART OF THIS RECORD WAY BE DECORDED TO DOD'AS DETINED IN OF PARTS 13 AND 1320, DECET THE ADMRESTRATION, UNMUTADEDIN BILLAS MAY BY ACTINE TO U.S. ONYDIMENT ADDRESS, PARL SCA ACTINE, TO U.S. ONYDIMENT ADDRESS, PARL U.S.C. 532, MD 49 CR PARTS 15 AND 1550.
CTA /	ASSUMES NO	IS NOT GUARANTEED. O RISK OF LIABILITY FOR , DIRECTLY OR INDIRECTLY, THIS DRAWING.
	IRONME	NE EXTENSION NTAL IMPACT STUDY ACIFIC RAILROAD
		TERNATIVE EST OPTION
		19 OF 51
IN CH	OVED BY	
CHEC	KED BY NED BY	
DRAW	N BY	
	NAME CTARL	-RLE E_G_COVER.dgn
	· · · · · ·	



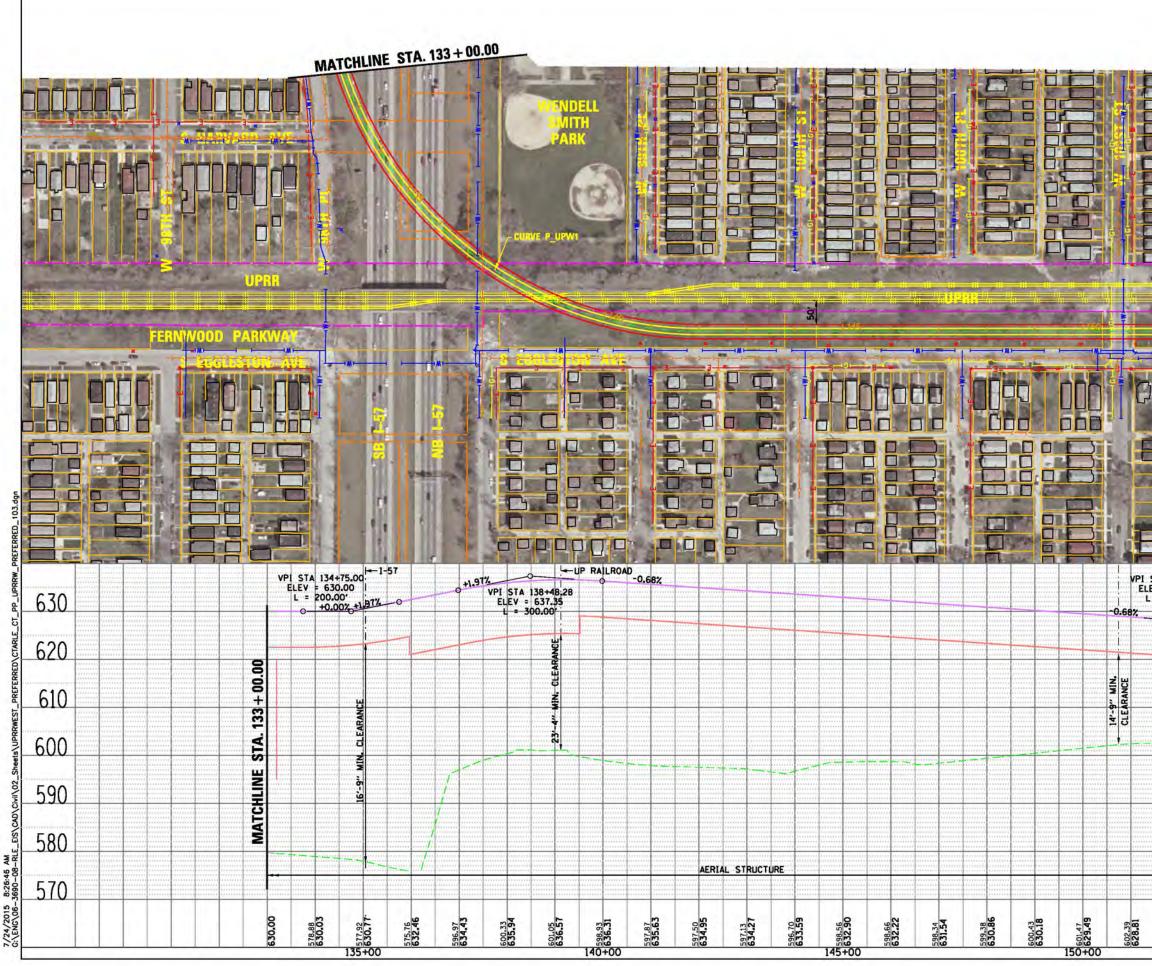
Z-		гđ		LEGEND
L		φ		PLAN PLAN PR
	A H		00:00+011	UTILITIES EXISTING WATER LINES EXISTING SEWER LINE EXISTING GAS LINE EXISTING CAS LIN
		- PLS	011	PRELIMINARY DRAFT
		SANT		Gy & Transit Group
	MAT			cta
3 45 34.73"(RT)	CURVI PI ST DELTA R =1.1 T =59	<b>P CT</b> A. =10 =38° (22.62 (6.12 47.80	<b>AIW1</b> 8+58.46 10' 36.19''(RT)	CHICAGO TRANSIT AUTHORITY
91.19 60.97 35 MPH	E =10 PC ST	0.23 A. = 1	02+62.34 114+10.14 ED = 35 MPH	SENSITIVE SECURITY INFORMATION WHEN HIS RECEIPT COMMAN SUBJECT INFORMATION THAT IS CONTROLLD WHEN HI CTR PARTS 13 AND 1330, IKO PARTS 10 THIS RECEIPT INFORMATION THAT IS CONTROLLD PRESSUR WHICH TAKED IN SECOND AND THE OWN OF THE IS CONTROLLD COMMAND TAKED IN SECOND AND THE OWN OF THE IS TAKED TO COMPANY
			630	Weeke he from contains sections stormer scheme to the monometer of the section with the section of the section with the section of the section with the section of the section section scheme and the section section of the section se
<u>STA 109+3</u> LEV = 611.	50		620	ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION
L = 400.00	0. +0.00%		610	ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE WEST OPTION
		STA. 110 + 00.	600	20 OF 51
EXISTING CTA/SB I-94 TUN ARD TRACK			590	IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY
		MATCHLINE	580	PROJECT NO CTA-RLE FILE NAME CTARLE_CT_PP_UPRRW_PREFERRED_10 100 0 100 200
	0 0000000	Z	<b>F 70</b>	SCALE H 1" = 100'-0" V 1" = 10'-0"
RETAINED	FILL		570	



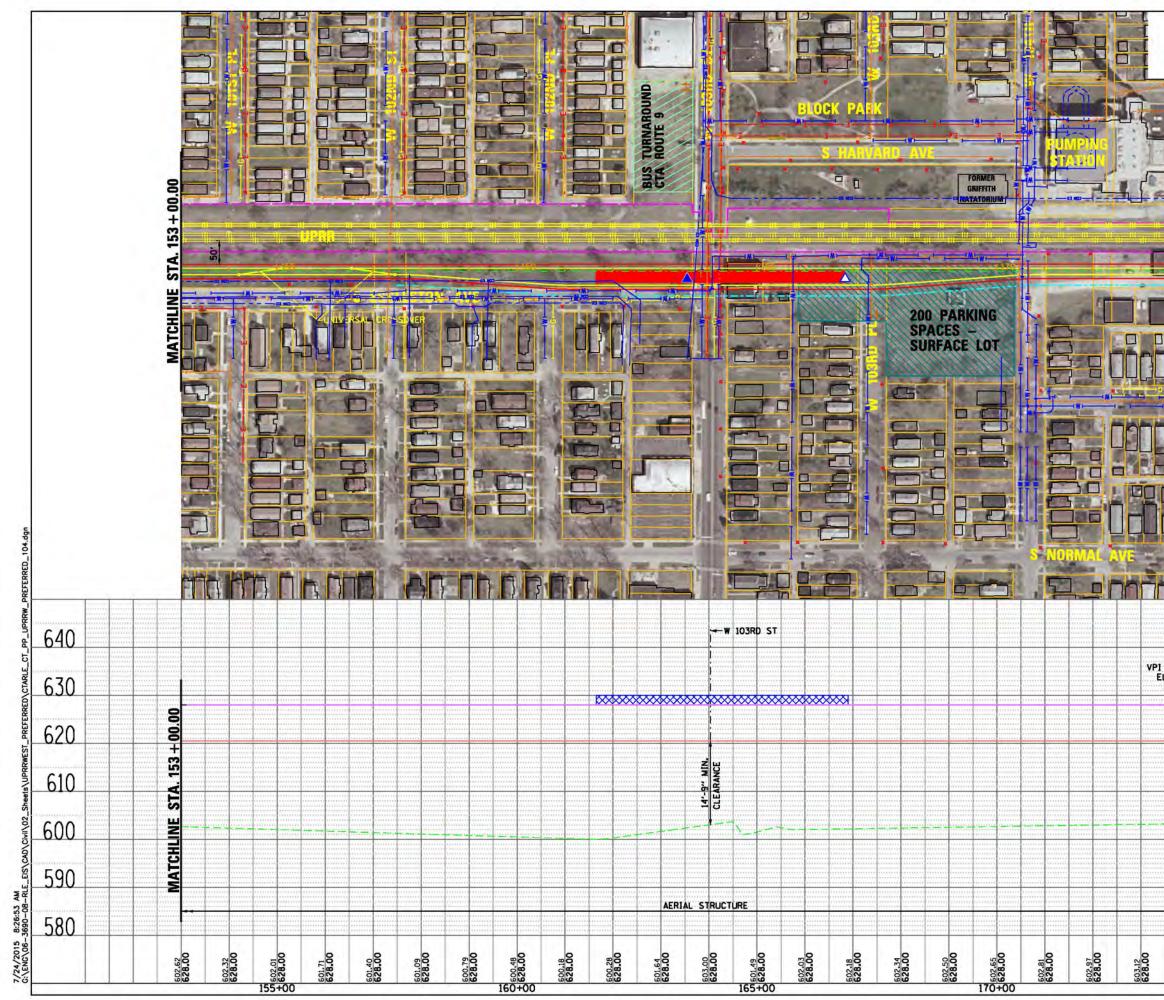
1			LEGEND
	1		PLAN
	1× C	Į.	PR TRACK STRUCTURE EX ROADWAY RIGHT-OF-WAY EX RAILROAD RIGHT-OF-WAY PR PERMANENT ENVELOPE PR ROADWAY/HARDSCAPE
50	V D		PR STATION PLATFORM EX BUILDING OUTLINE PR BUILDING OUTLINE PR PARKING PR CTA SUBSTATION
El l			UTILITIES EXISTING WATER LINES EXISTING SEWER LINE EXISTING GAS LINE EXISTING ELECTRIC LINE EXISTING TELEPHONE LINE
	<u> </u>	4	
		00	PROPOSED PARKING ENTRANCE PROFILE PROFILE PR TOP OF RAIL PR ELEVATED STRUCTURE DEPTH PR ELEVATED STRUCTURE DEPTH
		20+00	PRELIMINARY DRAFT
	ALL		Gy E Transit Group
	MATCH		cta
V2 13.96 D' 12.77"(R	CURVE P CTAC PI STA. =117+ T) DELTA =36° 4 R =684.50 T =227.15	W4 43.36 2' 58.83''(RT)	CHICAGO TRANSIT AUTHORITY
	-470 64		
+89.33 +23.44 = 35 MPH	L = 438.64 E = 36.70 PC STA. = 115 PT STA. = 115 DESIGN SPEED	5+16.22 9+54.86 ) = 35 MPH	SENSITIVE SECURITY INFORMATION WORKS 49 CTR AVENTS SOUTH STOLENT INFORMATION INFORM 49 CTR AVENTS 13 AND 1320. NO NATI OF THIS RECORD MY BE DECURED TO PRESSUR WHITCH ADDITION AND ADDITION AND ADDITIONAL SECURITY WITCH ADDITIONAL ADDITION AND ADMINISTRATION OF THE TRANSPORTATION SECURITY WITCH ADDITION ADDITION AND ADMINISTRATION OF THE TRANSPORTATION SECURITY WITCH ADDITIONAL ADDITION AND ADMINISTRATION OF THE TRANSPORTATION SECURITY WITCH ADDITION OF THE ADMINISTRATION OF THE TRANSPORTATION SECURITY
+89.33 +23.44 = 35 MPH	L =438.64 E =36.70 PC STA. = 115 PT STA. = 115	630	WHENC HIS RECORD COMMANS SOUTH SECURITY INFORMATION THAT IS CONTROLLED UNDER 40 CPR PARTS 13 AND 1300. WID PART OF THES RECORD WAT BE CONSIDER PERSONG WIDTLY ANGED TO COME AS EXPRESSIONED IN CPN PARTS TA AND 1300. COCCUT HOMMISTRINION OR THE SECRETARY OF TIMESFORTATION. UNUMPORTED RELACE MAY RESALT IN CAME POLICY OF COME AS CONTAINED TO USE, CONTROLLED RELACE MAY RESALT IN CAME POLICY OF COME AS CONTAINED TO USE, CONTROLLED RELACE MAY RESALT IN CAME POLICY OF COME AND AND AND AND AND AND RESALT IN CAME POLICY OF COME AND AND AND AND AND AND RESALT IN COME POLICY OF COME AND AND AND AND AND AND RESALT IN COME POLICY OF COME AND AND AND AND AND AND RESALT IN COME POLICY OF COME AND AND AND AND AND AND RESALT IN COME POLICY OF COME AND AND AND AND AND AND RESALT IN COME POLICY OF COME AND AND AND AND AND AND AND RESALT AND
+89.33 +23.44 = 35 MPH	L = 438.64 E = 36.70 PC STA. = 11 PT STA. = 11 DESIGN SPEED	) = 35 MPH	WHENC HIS RECORD CONTANTS SOUTH SECURITY INFORMATION THAT IS CONTROLLED UNDER 40 CPR PARTS 15 AND 1530. NO PART OF THE RECORD WAY BE DECORDED TO PERSONG WHENCE A NEED TO SOOT AS EXTENDED IN CPP PARTS IN AND 1530. DECORT NONHISTINITION OR THE SECRETARY OF TIMESORETINES. UNULLINGEED RELACE MY RESALT IN CAN IPOLITY OF CIPRE ACTION. FOR UNLINGEED RELACE MY RESALT IN CAN IPOLITY OF CIPRE ACTION. FOR UNLING CONTROLLED, NULL DECLOSURE IS CONTRIBUTED BY SUBLE SEC AND 49 CPR PARTS 15 AND 1530. DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR
+89.33 +23.44 = 35 MPH	L = 438.64 E = 36.70 PC STA. = 11 PT STA. = 11 DESIGN SPEED	630	WARNE HS ROOM CALLARS SOLDING SECURITY INFORMATION IN IT IS CONTROLLED INFORMATION IN THE INFORMATION IN THE ADDRESS OF THE INFORMATION IN THE CONTROL IN THE INFORMATION INFORMATION IN THE INFORMATION INFORMATIO
+89.33 +23.44 = 35 MPH	L = 438.64 E = 36.70 PC STA. = 119 DESIGN SPEED 00:000 + EEL V V V V V V V V V V V V V	630 620	WARNE HS RECORD COMARS SOUTH SECURITY INFORMATION HAT IS CONTROLLED WERE AS CHIPMENT IS AND ISSUE AS AN WHIT OF HIGH RECORD HAT IS CONTROLLED WHIT IS AN UNDER THE ADMITTANE AND
+89.33 +23.44 = 35 MPH	L = 438.64 E = 36.70 PC STA. = 119 DESIGN SPEED 00:000 + EEL V V V V V V V V V V V V V	630 620 610	WARNE HS ROOM COMARS SOUTH SECURIT WORKING HT S CONTROLLD DESCRIPTION OF LIDEO 130, WHIT IS NOT THE AND THE ADDRESS OF THE WHIT IS NOT THE ADDRESS OF THE ADDRESS OF THE ADDRESS OF THE WHIT IS NOT THE ADDRESS OF THE ADDRESS OF THE ADDRESS OF THE WHITE ADDRESS OF THE ADDRESS OF THE ADDRESS OF THE WHITE ADDRESS OF THE ADDRESS OF THE ADDRESS OF THE WHITE ADDRESS OF THE ADDRESS OF THE ADDRESS OF THE DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE WEST OPTION 21 OF 51 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY
+89.33 +23.44 <u>= 35 MPH</u>	L = 438.64 E = 36.70 PC STA. = 11 PT STA. = 11 DESIGN SPEED	630 620 610 600	WARNE HS RECORD COMARS SOUTH SECURITY INFORMATION HWT IS CONTROLLED DIRECT AT CHARGE IS NOT 130, WHIT IS NOT RECORD IN THE RECORD AND THE CONSTRUCT OF THE THREE RECORD IN THE RECORD AND THE CAMBER AND THE THREE RECORD AND THE CAMBER AND THE RECORD AND THE CAMBER AND THE RECORD AND THE RECO
+89.33 +23.44 <u>= 35 MPH</u>	L = 438.64 E = 36.70 PC STA. = 119 DESIGN SPEED 00:000 + EEL V V V V V V V V V V V V V	630 620 610 600 590	WARKE HER RECORD CONTARY SCREENE SCREENE WITCHNEED IN THE CONTROL OF THE CONTROL



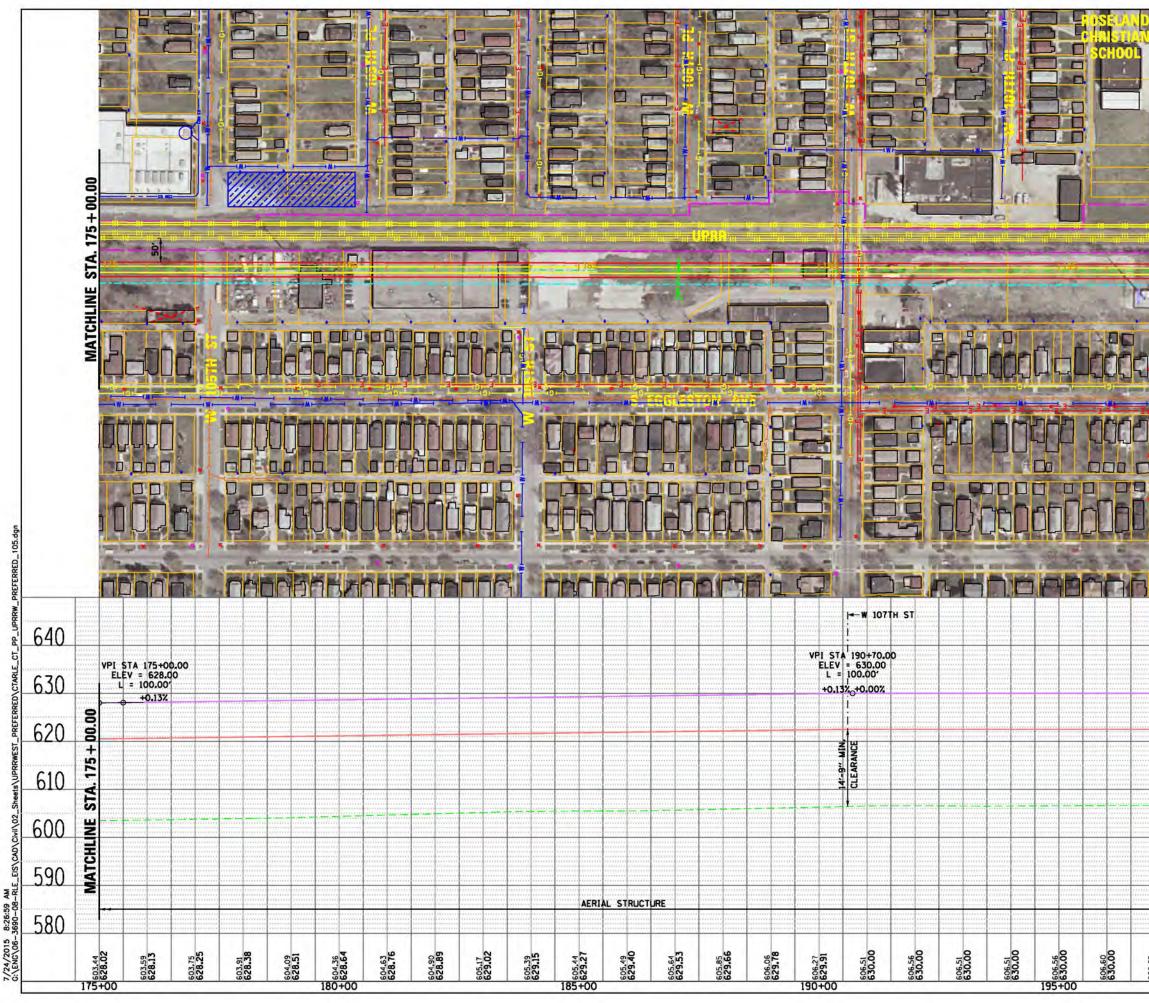
530.00	630.00	570	MARK DATE DESCRIPTION
		580	FILE NAME         CTARLE_CT_PP_UPRRW_PREFERRED_11           100         0         100         200           SCALE         H 1" = 100"-0"         100         200
	MATCHLINE		DESIGNED BY DRAWN BY PROJECT NO CTA-RLE
	100000000 10000	590	IN CHARGE APPROVED BY CHECKED BY
	STA.	600	22 OF 51
	STA. 133 + 00.00	610	UNION PACIFIC RAILROAD ALTERNATIVE WEST OPTION
	00.00		RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY
		620	CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING.
		630	DRAWING SCALE IS NOT GUARANTEED.
			SENSITIVE SECURITY INFORMATION where her record counter scheme scheme working the counter of the record of here to back, no here for the record work to backed to record the record of the record work of the scheme to the the there here to back of the record of the record of the record of the record of the scheme to the scheme to the the scheme to the scheme to the scheme result in our power of the scheme to the scheme to record of the scheme to the scheme to the scheme result in our power of the scheme to the scheme to record of the scheme to the scheme to the scheme to record of the scheme to the scheme to the scheme to record of the scheme to the scheme to the scheme to record of the scheme to the scheme to the scheme to record of the scheme to the scheme to the scheme to the scheme to record of the scheme to the scheme to the scheme to the scheme to scheme to the scheme to the scheme to the scheme to the scheme to scheme to the scheme to the scheme to the scheme to the scheme to scheme to the scheme to the scheme to the scheme to the scheme to scheme to the scheme to the scheme to the scheme to the scheme to scheme to the scheme to the scheme to the scheme to the scheme to scheme to the scheme to the scheme to the scheme to the scheme to scheme to the scheme to the scheme to the scheme to the scheme to scheme to the scheme to the scheme to the scheme to the scheme to scheme to the scheme to the scheme to the scheme to the scheme to scheme to the scheme to the sch
			AUTHORITY
110	+00		CHICAGO TRANSIT
604.02 608.70	610.79		cta
	2	570	Cover Transit Group
	MATCHLINE	580	
RD TRACK	and the base of the strengt	590	PRELIMINARY DRAFT
EXISTING CTA/SB 1-94 TUNNEL	STA. 110 + 00.00	600	PROFILE PR TOP OF RAIL PR ELEVATED STRUCTURE DEPTH
	00 + 0	11 11 1 10 10 1 10 10 1	PRIMARY ENTRANCE SECONDARY ENTRANCE
= 400.00	8	610	EXISTING GAS LINE EXISTING ELECTRIC LINE EXISTING TELEPHONE LINE ENTRANCES
TA 109+75.00		620	UTILITIES 
-58 1-94		630	EX BUILDING OUTLINE PR BUILDING OUTLINE PR PARKING PR CTA SUBSTATION
			EX RAILROAD RIGHT-OF-WAY PR PERMANENT ENVELOPE PR ROADWAY/HARDSCAPE PR STATION PLATFORM
		1111111	EX TRACK CENTERLINE PR TRACK CENTERLINE PR TRACK STRUCTURE EX ROADWAY RIGHT-OF-WAY



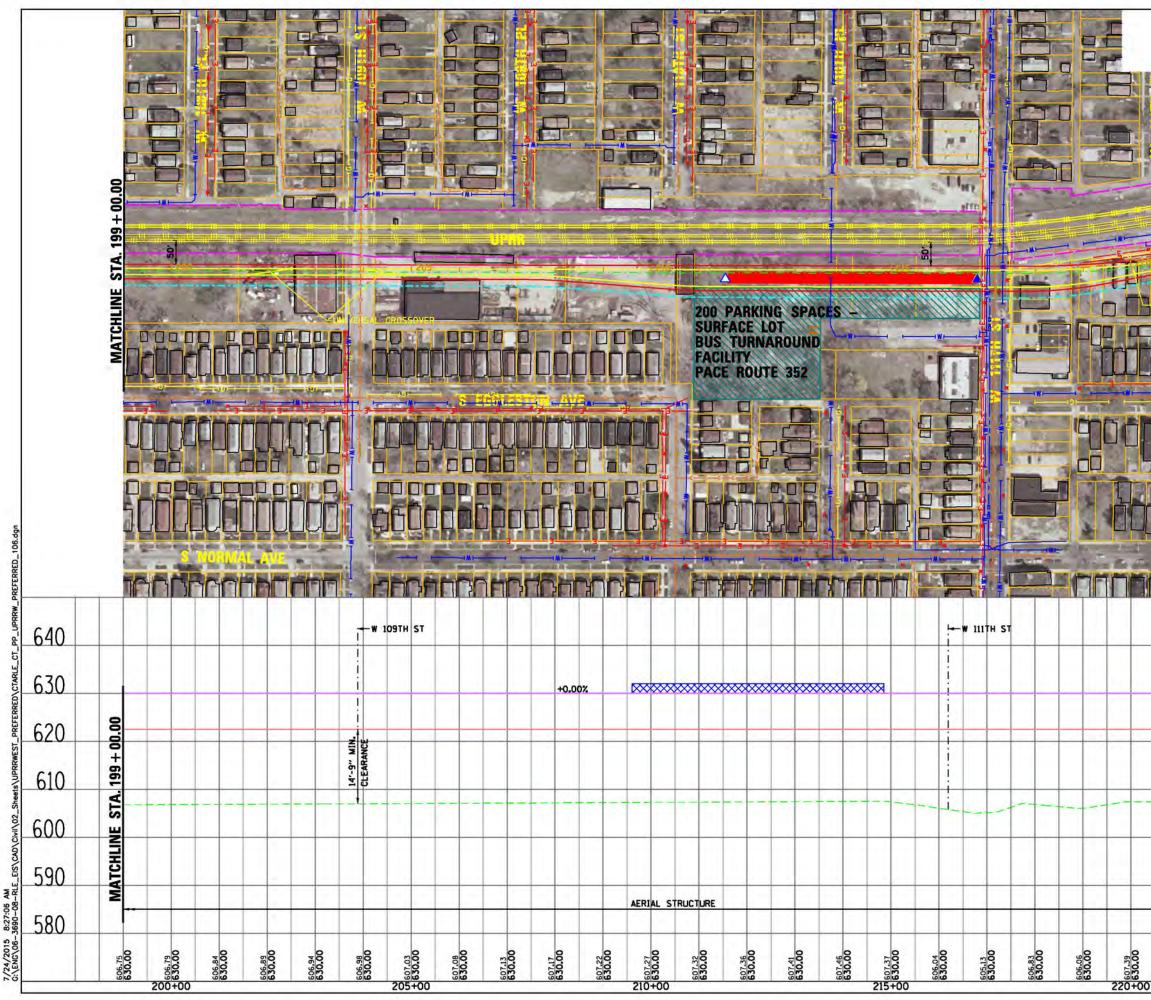
		LEGEND	
	STA. 153 + 00.00		EX CENTERLINE PR CENTERLINE EX TRACK CENTERLINE PR TRACK CENTERLINE PR TRACK STRUCTURE EX ROADWAY RIGHT-OF-WAY EX RAILROAD RIGHT-OF-WAY PR PERMANENT ENVELOPE PR ROADWAY/HARDSCAPE PR STATION PLATFORM EX BUILDING OUTLINE PR BUILDING OUTLINE PR BUILDING OUTLINE PR PARKING PR CTA SUBSTATION EXISTING WATER LINES EXISTING GAS LINE EXISTING SEWER LINE EXISTING GAS LINE EXISTING TELEPHONE LINE PRIMARY ENTRANCE SECONDARY ENTRANCE EXIT ONLY PROPOSED PARKING ENTRANCE EX GROUND PR TOP OF RAIL PR ELEVATED STRUCTURE DEPTH PLATFORM LOCATION
		PRE	LIMINARY DRAFT
	MATCHLINE	G	Transit Group
	MA		cta
R =78	E <b>P UPW1</b> A. =137+81.18 A. =74° 56' 54.61 2.29 19.71 23.31		CAGO TRANSIT AUTHORITY
	3.43 [A. = 131+81.46 [A. = 142+04.78 N SPEED = 35 N		
A 152+19.00 = 628.00 100.00'	r-	and the second sec	COMMENT SUBJECT SUBJECT INFORMATION THAT IS CONTINUED SHOT 1302, NO MART OF THE RECORD TO TO SUBJECT TO DD TO BOOM AS BETHED IN CYT PARTS 15 AND 1520, DECET SEND OF TAS AUXIMISTICUT OF THE TIMEORYTICUTUS SUBJECT SEND OF TAS AUXIMISTICUTUS OF THEORYTICUTUS SUBJECT SEND OF TAS AUXIMISTICUTUS OF THEORYTICUTUS SUBJECT OF OTHER ACTION, TOR U.S. OXYERMENT ADDRESS, NUR.C BY 5 U.S.C. 550 AND 46 CYR PARTS 15 AND 1520.
<del>o o +0.00%</del>	0.	CTA ASSUME	ALE IS NOT GUARANTEED. S NO RISK OF LIABILITY FOR SED, DIRECTLY OR INDIRECTLY, OF THIS DRAWING.
	8 62	20 REI	D LINE EXTENSION MENTAL IMPACT STUDY
	<b>STA. 153</b> 90.00 90 90 90 90 90 90 90 90 90 90 90 90 9	10 UNION	I PACIFIC RAILROAD ALTERNATIVE WEST OPTION
	<b>T</b> 6(		23 OF 51
		APPROVED BY CHECKED BY DESIGNED BY DRAWN BY	
CECCO CECCO CONTRACTOR CONTRACTOR	<b>S</b>	BO FILE NAME C	CTA-RLE TARLE_CT_PP_UPRRW_PREFERRED_10 0 100 200
	Concerning and the second s		ALE H 1" = 100'-0" V 1" = 10'-0"
	5	70	V 1" = 10'-0"
628.64 628.16 602.62			



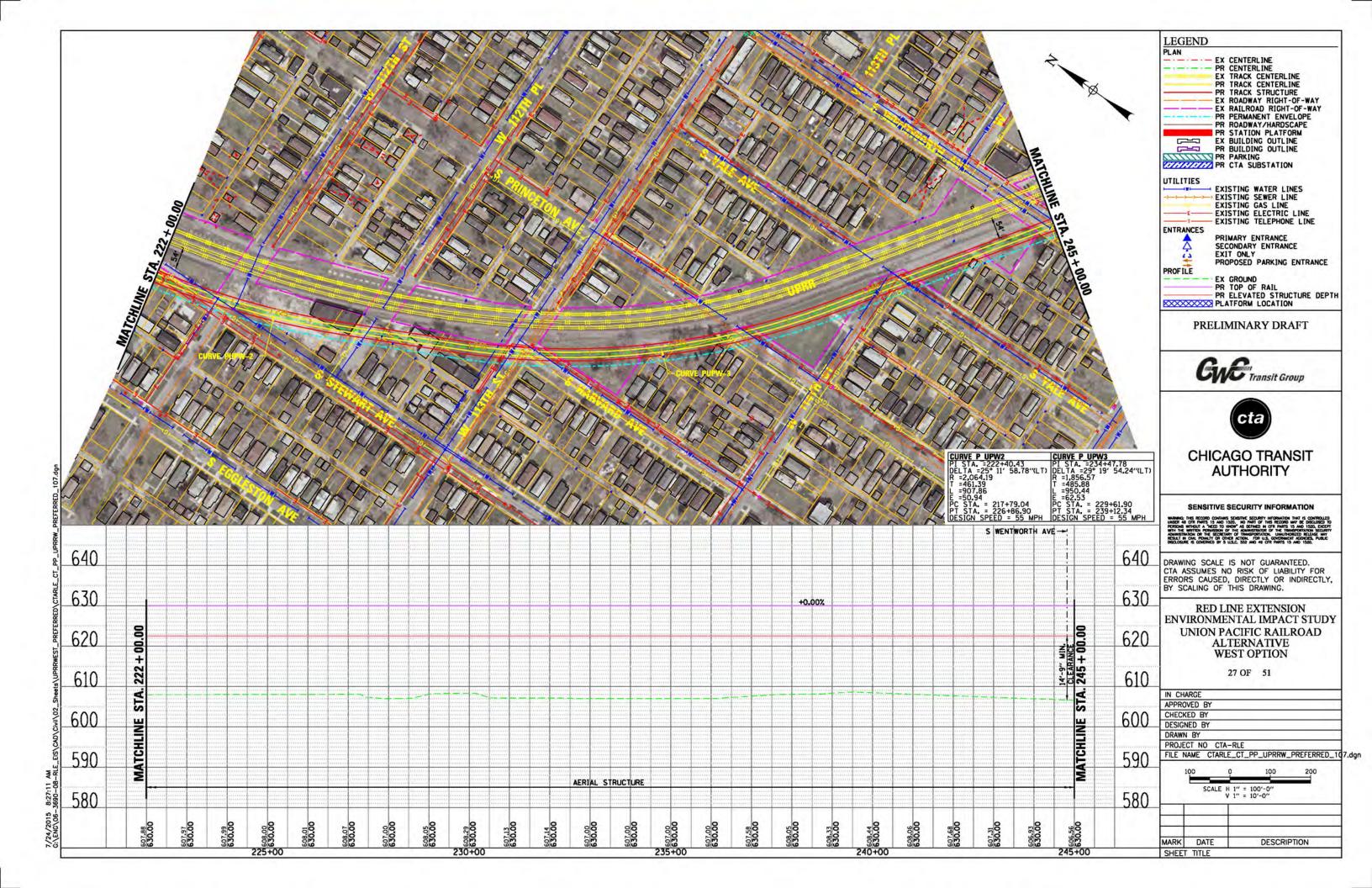
			LEGEND
	MATCHLINE STA. 175 + 00.00	-	PLAN PLAN PLAN PLAN PLAN PLAN PLAN PLAN
┛╧╹┙┝┚┡			
			Cy Cransit Group
			CHICAGO TRANSIT AUTHORITY
			AUTHORITY SENSITIVE SECURITY INFORMATION
		640	AUTHORITY SENSITIVE SECURITY INFORMATION WHEN IN THE ACCORD CONTAINS SECURITY INFORMATION WHEN IN COMPACE AND A LOSS IN THE SECOND WITH THE CONTROL OF WHEN IN COMPACE AND A LOSS IN THE SECOND WITH THE CONTROL OF WHEN IN A WEET IN POWER AND A THE ADAPTISTICAL OF THE THEOREMUM AND A WHEN IN A WEET IN POWER AND A THE ADAPTISTICAL OF THE THEOREMUM AND A WHEN IN A WEET IN POWER AND A THE ADAPTISTICAL OF THE THEOREMUM AND A WHEN IN A WEET IN POWER AND A THE ADAPTISTICAL OF THE THEOREMUM AND A UNIT THE WEETIN POWER AND A THE ADAPTISTICAL OF THE ADAPTISTICAL OF WHEN IN A WEETIN POWER AND A THE ADAPTISTICAL OF THE ADAPTISTICAL OF WHEN IN A WEETIN POWER AND A THE ADAPTISTICAL OF THE ADAPTIST DECLOSERE & CONSINCE OF IS USED AND AN OTHER WHEN IS AND ISSUE DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY,
TA 175+00.00 v = 628.00 = 100.00'		640 630	AUTHORITY SENSITIVE SECURITY INFORMATION WHEN THE RECORD CONTANTS SPECIFIC WORMAND IN IT IS CONTINUED WHEN AND ADDRESS OF A WAY OF THE RECORD WAY & DOUGLOOD R WHEN AND ADDRESS OF A WAY OF THE RECORD WAY & DOUGLOOD R WHEN AND ADDRESS OF A WAY OF THE RECORD WAY & DOUGLOOD R WHEN AND ADDRESS OF A WAY OF THE RECORD WAY & DOUGLOOD R WHEN AND ADDRESS OF A WAY OF THE RECORD WAY & DOUGLOOD R WHEN ADDRESS OF THE SECRET WAY OF THE RECORD WAY & DOUGLOOD R WHEN ADDRESS OF THE SECRET WAY OF THE RECORD WAY & DOUGLOOD R WHEN ADDRESS OF THE SECRET WAY OF THE RECORD WAY ADDRESS DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION
TA 175+00.00 V = 628.00 = 100.00'	-		AUTHORITY SENSITIVE SECURITY INFORMATION UNDER A GOND CONTANT SOUTH OF THE CONTINUES WHEN A REAL AND A DAY OF THE REAL AND AND A DAY OF THE ADDRESS OF THE WHEN ADDRESS OF THE SECURITY WOMENT OF THE THEORETISM ADDRESS WHEN ADDRESS OF THE SECURITY OF THE MADE AND A DAY WHEN ADDRESS OF THE SECURITY OF THE MADE AND A DAY WHEN ADDRESS OF THE SECURITY OF THE MADE AND A DAY WHEN ADDRESS OF THE SECURITY OF THE MADE AND A DAY WHEN ADDRESS OF THE SECURITY OF THE MADE AND A DAY WHEN ADDRESS OF THE SECURITY OF THE MADE AND A DAY WHEN ADDRESS OF THE SECURITY OF THE MADE AND A DAY WHEN ADDRESS OF THE SECURITY OF THE SECURITY OF THE ADDRESS DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OF INDIRECTLY, BY SCALING OF THIS DRAWING.
TA 175+00.00 v = 628.00 = 100.00'	-	630	AUTHORITY SENSITIVE SECURITY INFORMATION UNDER 49 CH RATE 15 AND 1580. BO NATE OF THE SECOND WIT & CONTRACT MEMORY AND THE AND 1580. BO NATE OF THE SECOND WIT & CONTRACT MEMORY AND THE AND 1580. BO NATE OF THE SECOND WIT & CONTRACT MEMORY AND THE SECOND WITH AND THE ADDITION MEMORY AND THE SECOND WITH AND THE ADDITION AND THE SECOND WITH AND THE ADDITION MEMORY AND THE SECOND WITH AND THE ADDITION AND THE SECOND WITH AND THE ADDITION ADDITION AND THE ADDITION AND THE ADDITION ADDITION AND THE ADDITION ADDITION AND THE ADDITION ADDITION AND THE ADDITION ADDITION ADDITION ADDITION ADDITION ADDITION ADDITION ADDITION ADDITION ADDITION ADDITION ADDITION ADDITION ADDITION ADDITION ADDITION ADDITION
TA 175+00.00 v = 628.00 = 100.00 <sup>4</sup> 0.002	STA. 175 + 00.00	630 620	AUTHORITY SENSITIVE SECURITY INFORMATION WHENCE THE RECORD CONTANT SCHEME SECOND WIT GENERATED UNDER 49 OFF NAME 15 AND 150C WIT OF THE SECOND WIT & DOUTED WHENCE THE RECORD CONTANT SCHEME SECOND WIT & DOUTED PROVIDE A WILL DO THE SECOND WIT OF THE SECOND WIT & DOUTED PROVIDE A WILL DO THE SECOND WIT OF THE SECOND WIT & DOUTED WHENTING OF THE SECOND WIT OF THE SECOND WIT & DOUTED DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OF INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE WEST OPTION 24 OF 51 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY
TA 175+00.00 V = 628.00 = 100.00'	-	630 620 610	AUTHORITY SENSITIVE SECURITY INFORMATION WHENCE THE RECORD CONTARTS SOUTH & STORMATTION WHENCE AND THE ADDRESS OF THE SECURITY WORKNOW THE CONTROLLED WHENCE AND THE ADDRESS OF THE SECURITY OF THE WARD THE ADDRESS WHENCE ADDRESS OF THE SECURITY OF THE WARD THE ADDRESS WHENCE ADDRESS OF THE SECURITY OF THE WARD THE ADDRESS WHENCE ADDRESS OF THE SECURITY OF THE WARD THE ADDRESS DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE WEST OPTION 24 OF 51 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWIN BY PROJECT NO CTA-RLE FILE NAME CTARLE_CT_PP_UPRRW_PREFERRED_11
TA 175+00.00 V = 628.00 = 100.00'	STA. 175 + 00.00	630 620 610 600	AUTHORITY SENSITIVE SECURITY INFORMATION WHENCE THE RECORD COMMAND SHERING SECURITY INFORMATION WHENCE AND CONTRACT SHERING SECURITY INFORMATION INT'S CONTRACTOR WHENCE AND CONTRACT SHERING SECURITY OF THE RECORD WAY & CONTRACTOR WHENCE AND CONTRACT SHERING SECURITY OF THE RECORD WAY & CONTRACTOR WHENCE AND CONTRACT SHERING SECURITY OF THE RECORD WAY & CONTRACTOR MANUAL OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY MEDICAL AND CONTRACT SHERING SECURITY OF THE SECURITY OF THE SECURATION MEDICAL AND CONTRACT SECURITY OF THE SECURATION OF T
TA 175+00.00 y = 628.00 = 100.00 <sup>2</sup>	STA. 175 + 00.00	630 620 610 600 590	AUTHORITY SENSITIVE SECURITY INFORMATION WHENCE THE RECORD CONTARTS SOUTH & STORMATION THAT IS CONTROLLED WHEN A THE RECORD CONTARTS SOUTH & STORMATIC THE THE ADDRESS WHEN A THE RECORD CONTARTS SOUTH & STORMATIC THE THE ADDRESS WHEN A THE RECORD OF THE STORMATIC THE THE ADDRESS WHENT A THE STORMATIC THE ADDRESS TO THE ADDRESS DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE WEST OPTION 24 OF 51 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE FILE NAME CTARLE_CT_PP_UPRRW_PREFERRED_10 100 0 100 200 SCALE H 1" = 100'-0"

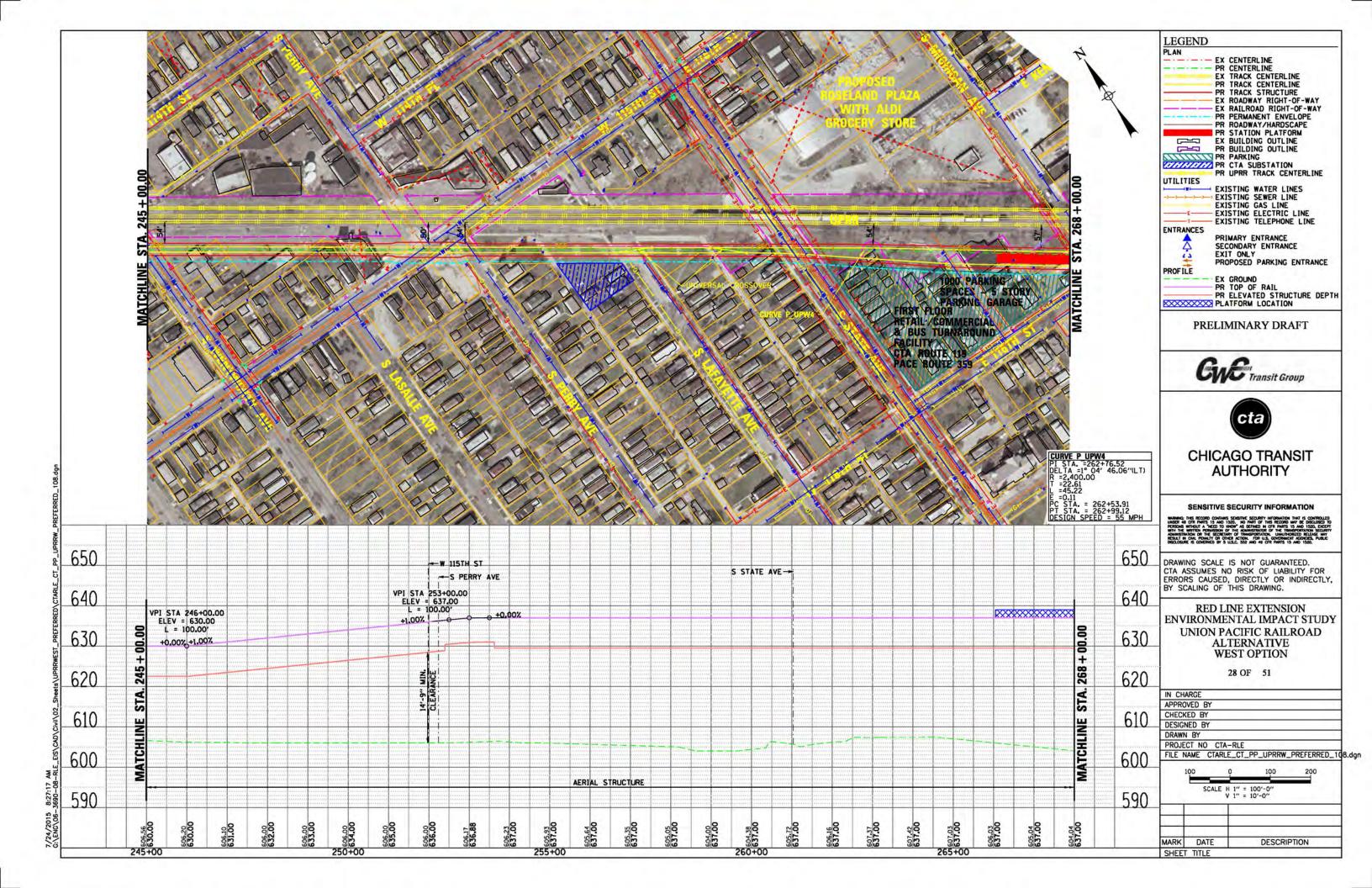


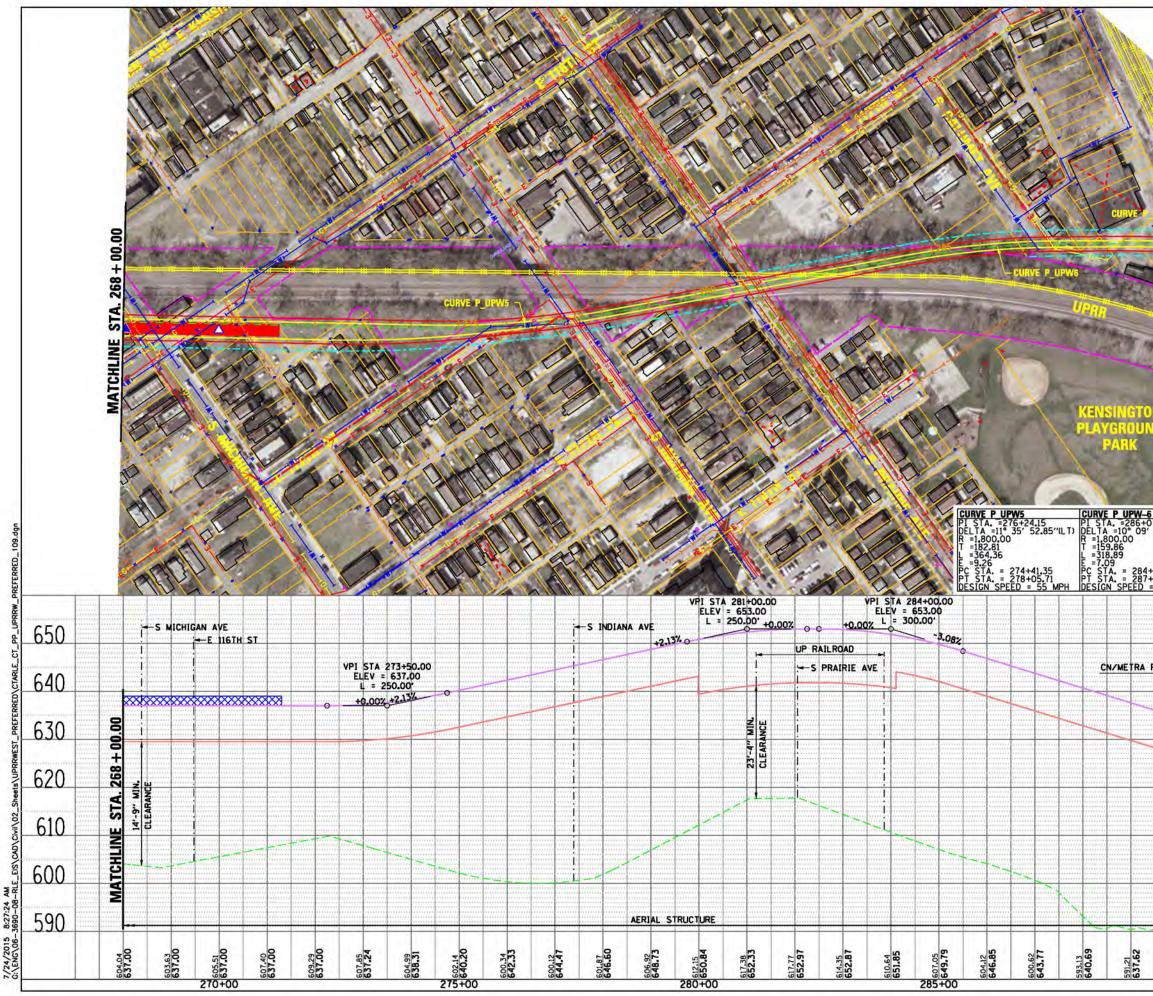
1	7.7		LEGEND
	MATCHLINE STA. 199 + 00.00		PLAN PLAN PR CENTERLINE PR CENTERLINE PR CENTERLINE PR TRACK CENTERLINE PR TRACK CENTERLINE PR TRACK CENTERLINE PR TRACK STRUCTURE PR RADAWAY RIGHT-OF-WAY EX RAILROAD RIGHT-OF-WAY PR PERMANENT ENVELOPE PR ROADWAY/HARDSCAPE PR STATION PLATFORM EX BUILDING OUTLINE PR BUILDING OUTLINE PR BUILDING OUTLINE PR PARKING COMMARY ENTRANCE EXISTING GAS LINE EXISTING CAS LINE EXIST
Į.	W		PRELIMINARY DRAFT
	<u>⊒</u> ≜ 		Gy Entransit Group
1001 No.	SC 2		CHICAGO TRANSIT AUTHORITY SENSITIVE SECURITY INFORMATION SENSITIVE SECURITY INFORMATION MARGING INFORMATION SECTOR INFORMATION MARGING INFORMATION SECTOR INFORMATION MARGING INFORMATION SECTOR INFORMATION INFORMATION SECTOR INFORMATION INFORMATION INFORMATION MARGING INFORMATION INFORMATION INFORMATION INFORMATION SECTOR INFORMATION INFORMATION INFORMATION INFORMATION INFORMATION INFORMATION INFORMATION INFORMATION INFORMATION SECTOR INFORMATIONI
		640	DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY,
	······	630	BY SCALING OF THIS DRAWING. RED LINE EXTENSION
	STA. 199 + 00.00	620	ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE WEST OPTION
	LA. 199	610	25 OF 51
		600	APPROVED BY CHECKED BY DESIGNED BY DRAWN BY
	MATCHLINE	590	PROJECT NO CTA-RLE FILE NAME CTARLE_CT_PP_UPRRW_PREFERRED_1 100 0 100 200
		580	SCALE H 1" = 100'-0" V 1" = 10'-0"



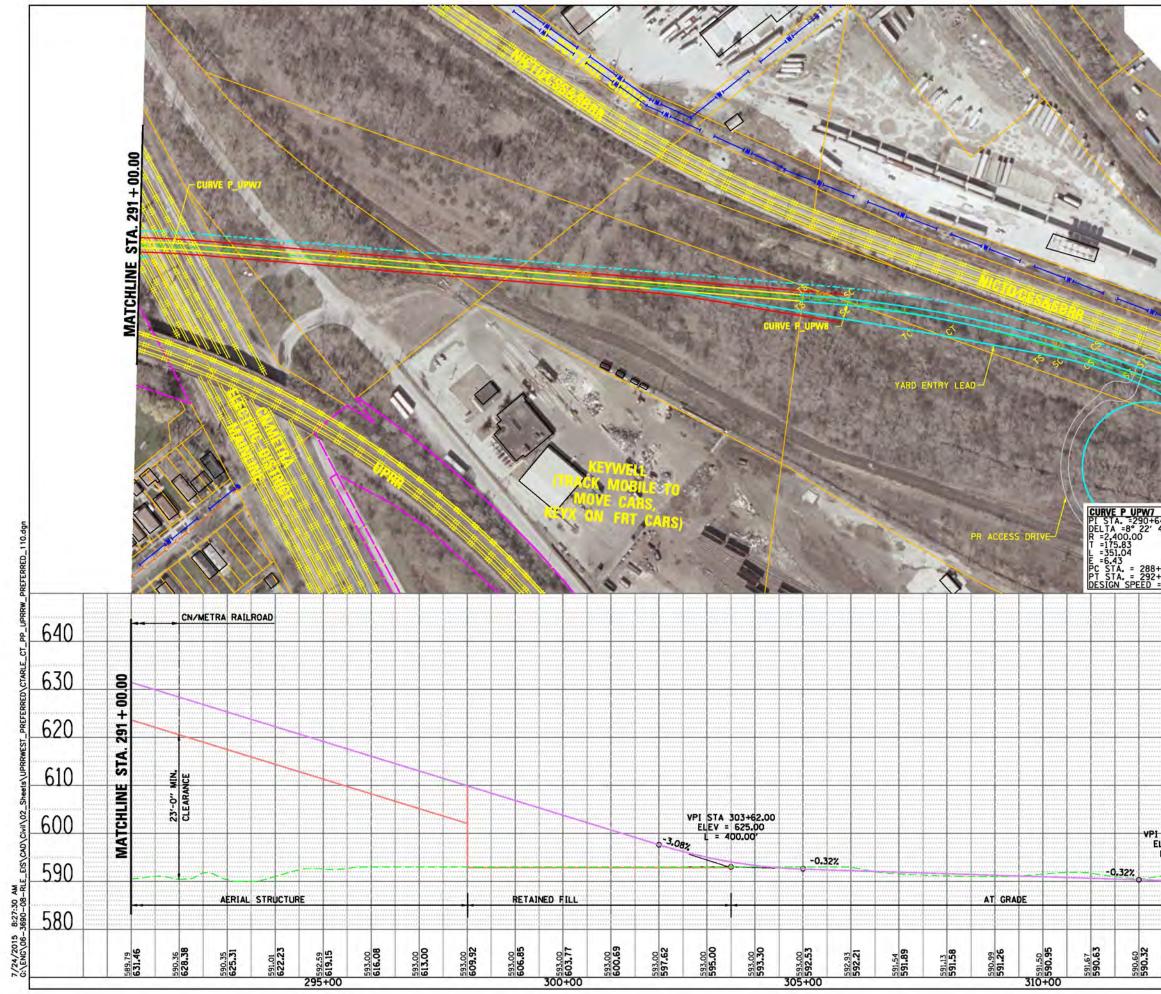
	580	SCALE H 1" = 100'-0" V 1" = 10'-0"
MATCHLINE	590	DRAWN BY PROJECT NO CTA-RLE FILE NAME CTARLE_CT_PP_UPRRW_PREFERRED_10
	600	IN CHARGE APPROVED BY CHECKED BY DESIGNED BY
. 222 +	610	26 OF 51
STA. 222 + 00.00	620	ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE WEST OPTION
	640 630	DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION
PC STA. = 2 PT STA. = 2 DESIGN SPEE		SENSITIVE SECURITY INFORMATION WHENC THE RECORD COMMENT SECTION WOOMLINE IN HIS CONTROLLED WHENC WE FINANCE IN AND INC. WO AND IN THE RECORD WE DECLORED IN PERSON WITHOUT A VECTO TO DOW'A SE REVIEW IN OR WATS 15 AND 1500. DECET WITHING WITHOUT A VECTO TO DOW'A SE REVIEW IN OR WATS 15 AND 1500. THE REVIEW INC. AND INTO AND AND AND AND AND AND AND AND AND REVIEW IN THE WITHOUT AND
R =2,064.19 T =461.39 L =907.86 F =50.94	2+40.43 11' 58.78''(LT)	CHICAGO TRANSIT AUTHORITY
		PRELIMINARY DRAFT
MATCHUNE STA. 222+00.00		PR PERMANENT ENVELOPE PR ROADWAY/HARDSCAPE PR STATION PLATFORM EX BUILDING OUTLINE PR BUILDING OUTLINE PR BUILDING OUTLINE PR PARKING PR CTA SUBSTATION PR UPRR TRACK CENTERLINE UTILITIES EXISTING SAVER LINE EXISTING GAS LINE EXISTING GAS LINE EXISTING CLECTRIC LINE EXISTING ELECTRIC LINE ENTRANCES PRIMARY ENTRANCE SECONDARY ENTRANCE EXIST ONLY PROPOSED PARKING ENTRANCE PROPOSED PARKING ENTRANCE PROFILE PR TOP OF RAIL PR TOP OF RAIL PR ELEVATED STRUCTURE DEPTH
z <del>~~</del> ¢	-	LEGEND         PLAN         PR         EX         EX         EX         EX         TRACK         EX         TRACK         EX         PR         TRACK         EX         PR         TRACK         STRACK         STRACK         PR         TRACK         STRACK         PR         TRACK         STRACK         STRACK         STRACK         PR         TRACK         STRACK         STRACK         PR         TRACK         STRACK         STRACK



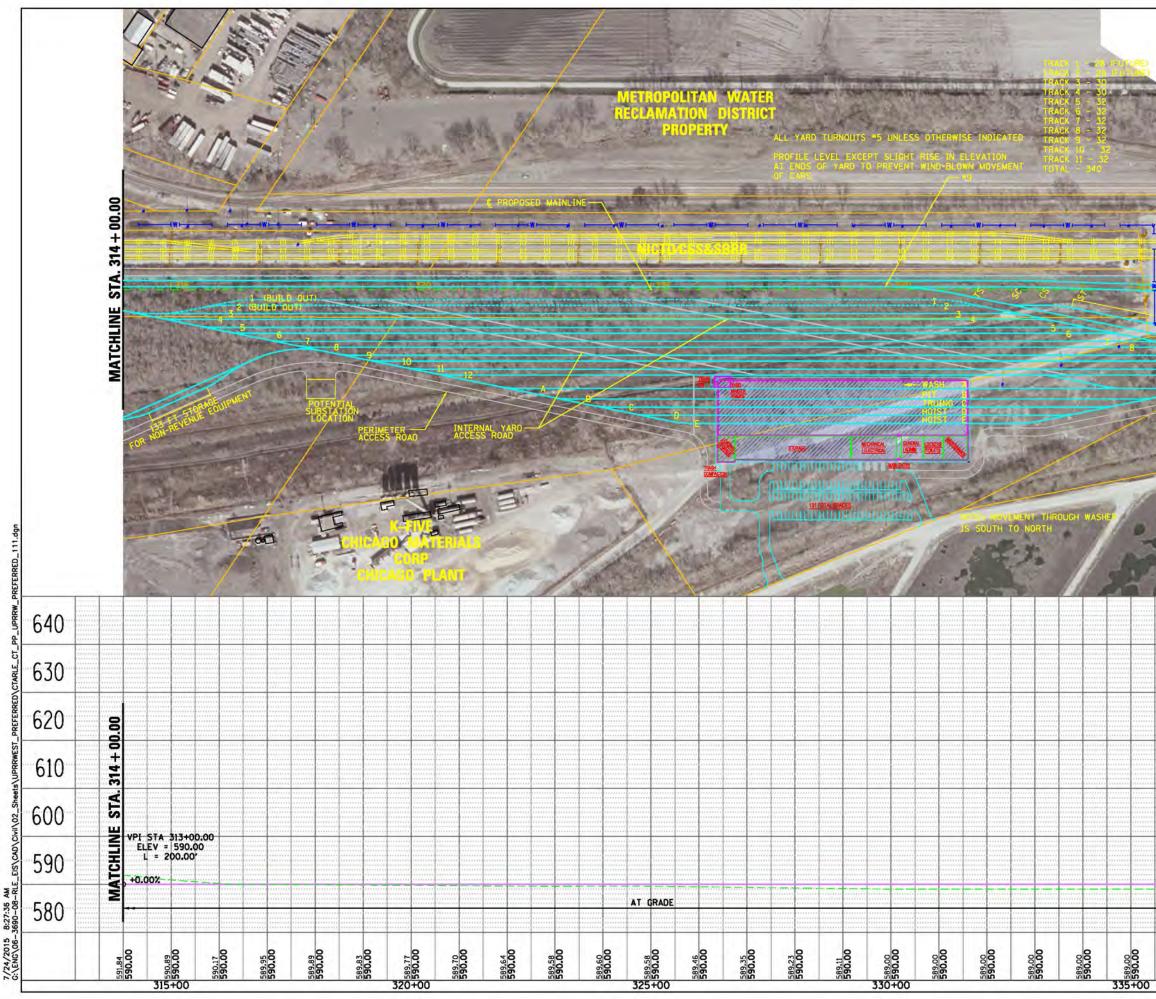




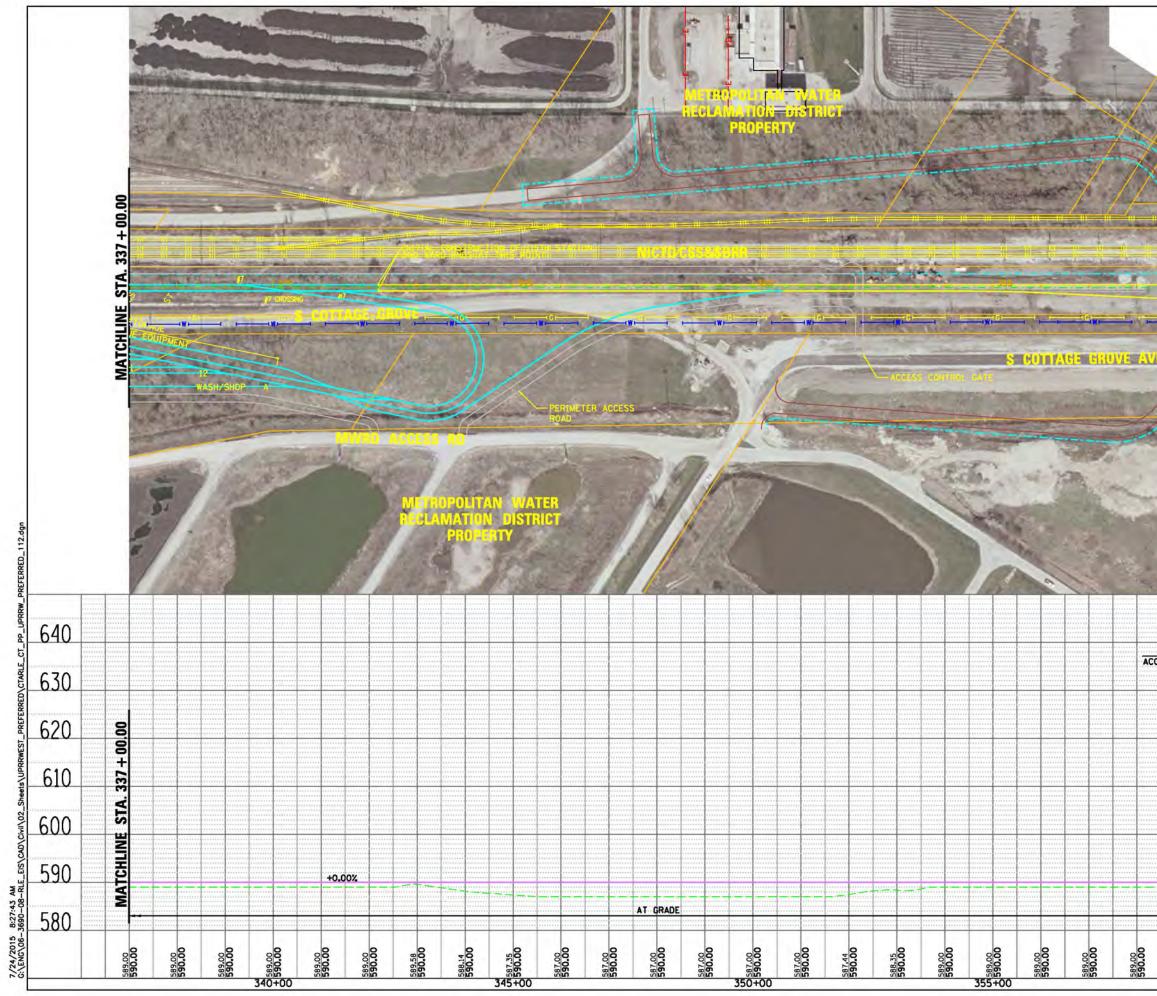
	MATCHLINE STA 204	00'00 + 167		LEGEND         PLAN         PR CENTERLINE         PR CENTERLINE         PR TRACK CENTERLINE         PR PRANNT ENVELOPE         PR ROADWAY/HARDSCAPE         PR STATION PLATFORM         PR BUILDING OUTLINE         PR PARKING         PR TRACK CENTERLINE         PR BUILDING OUTLINE         PR BUILDING OUTLINE         PR BUILDING OUTLINE         PR BUILDING OUTLINE         PR TRACK CENTERLINE         VITILITIES         EXISTING GAS LINE         EXISTING CAS LINE         EXIT ONLY         P
7.51 02.14"(RT) 47.64 66.53 = 55 MPH	<b>CURVE</b> PI ST DELTA T =179 L =35 E =6.2 FPC ST PT ST	<b>P</b> =8 100. 5.83 1.04 3 A. =	JPW-7 290+64,94 22' 49.78"(RT) 30 288+89.11 292+40.15 292+40.15 EED = 55 MPH	CHICAGO TRANSIT AUTHORITY SENSITIVE SECURITY INFORMATION
			650 640 630	WHENG THE RECORD COMMENTS SECTION ACCOUNTS IN THE IS CONTROLID. WHENG IN THE INFORM IS NOT SOLE IN PRIOR THE RECORD TO WHENG INFORM IS NOT SOLE. IN PRIOR THE RECORD TO THE WHENG INFORMATION OF THE RECORD IN THE RECORD TO WHENG INFORMATION OF THE RECORD IN THE RECORD TO WHENG INFORMATION OF THE RECORD IN THE RECORD TO WHENG INFORMATION OF THE RECORD INFORMATION OF THE RECORD INFORMATION OF THE RECORD INFORMATION OF THE RECORD INFORMATION OF THE RECORD INFORMATION OF THE RECORD INFOR
		STA. 291 + 00.00	620	ALTERNATIVE WEST OPTION 29 OF 51
23'-0" MIN.		MATCHLINE STA	610	APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE FILE NAME CTARLE_CT_PP_UPRRW_PREFERRED_10
		MA	600 590	100 0 100 200 SCALE H 1" = 100'-0" V 1" = 10'-0"
00+065 00+065	589.79	95.169		MARK DATE DESCRIPTION SHEET TITLE



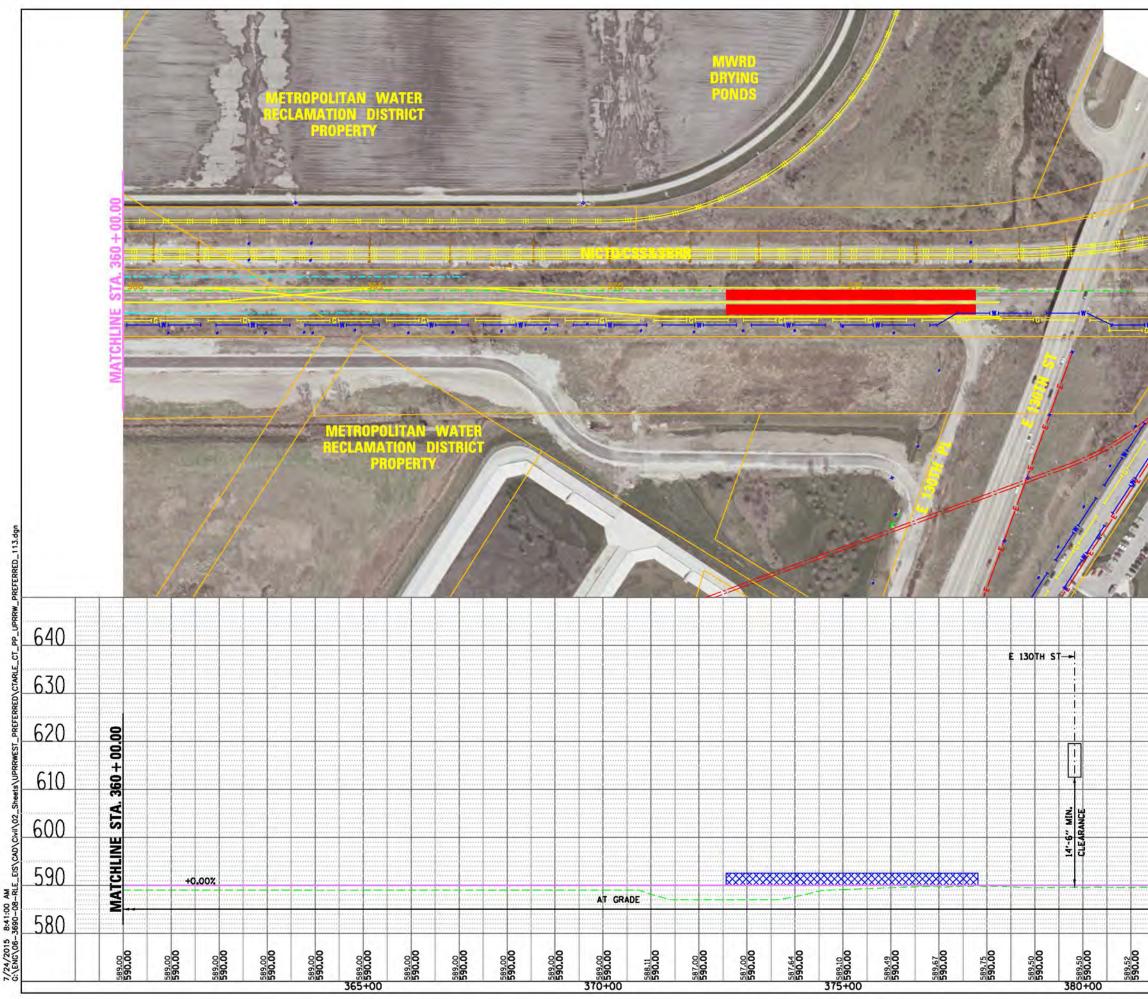
				LEGEND
	Ti line the	A A		LEGEND         PLAN         PR CENTERLINE         PR CENTERLINE         PR TRACK CENTERLINE         PR TRACK CENTERLINE         PR TRACK STRUCTURE         PR PERMANENT ENVELOPE         PR STATION PLATFORM         EX BUILDING OUTLINE         PR BUILDING OUTLINE         PR PARKING         PR PARKING         PR PARKING         PR PARKING OUTLINE         PR STATION PLATFORM         EXISTING OUTLINE         PR PARKING         PR PARKING         PR PARKING         PR PARKING         PR PARKING OUTLINE         PR EXISTING WATER LINES         EXISTING SEWER LINE         EXISTING GAS LINE         EXISTING ELEPHONE LINE         ENTRANCES         PRIMARY ENTRANCE         SECONDARY ENTRANCE         EXISTING TELEPHONE LINE         ENTRANCES         PROPOSED PARKING ENTRANCE         EXISTION PLATFORM LOCATION
		- 314+00.00		PRELIMINARY DRAFT
	E STA	5		Gy & Transit Group
	ATCHLIN			cta
4.94 19.78"(RT)	DELIA	50.00 .70	90.32 06.85"(RT)	CHICAGO TRANSIT AUTHORITY
89.11 40.15 55 MPH	E =20.	26 A. = 304 A. = 311+	+74.61 -02.56 = 55 MPH	SENSITIVE SECURITY INFORMATION WHENG: THE RECORE CONTANT SEVENTLE SECURITY INFORMATION WHENG: THE RECORE CONTANT SEVENTLE SECURITY INFORMATION THAT IS CONTROLLED INFORME WHENGLY A VECTO DIRECT VECTOR VECTOR VECTOR SECURITY CONTRIBUTION OF THE SECOND HOLE VECTOR VECTOR VECTOR CONTRIBUTION OF THE SECOND VECTOR VECTOR VECTOR VECTOR SECONDER & CONTRIBUTION OF UNAL CONTRIBUTION OF THE SECOND RECORDER & CONTRIBUTION OF UNAL CONTRIBUTION OF THE SECOND RECORDER & CONTRIBUTION OF UNAL CONTRIBUTION OF THE SECONDER OF THE RECORDER OF UNAL OF USE AND VECTOR VECTOR VECTOR
			640	DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING.
		<u> </u>	630	RED LINE EXTENSION
		A. 314 + 00.00	620	ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE WEST OPTION
	ekimen ö		610	30 OF 51
			600	IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY DRAWN BY
STA 313+0 EV = 590.	.00		EOO	PROJECT NO CTA-RLE FILE NAME CTARLE_CT_PP_UPRRW_PREFERRED_1
EV = 590. = 200.00	00		590	100 0 100 200
EV = 590. = 200.00			590	100 0 100 200 SCALE H 1" = 100'-0" V 1" = 10'-0"



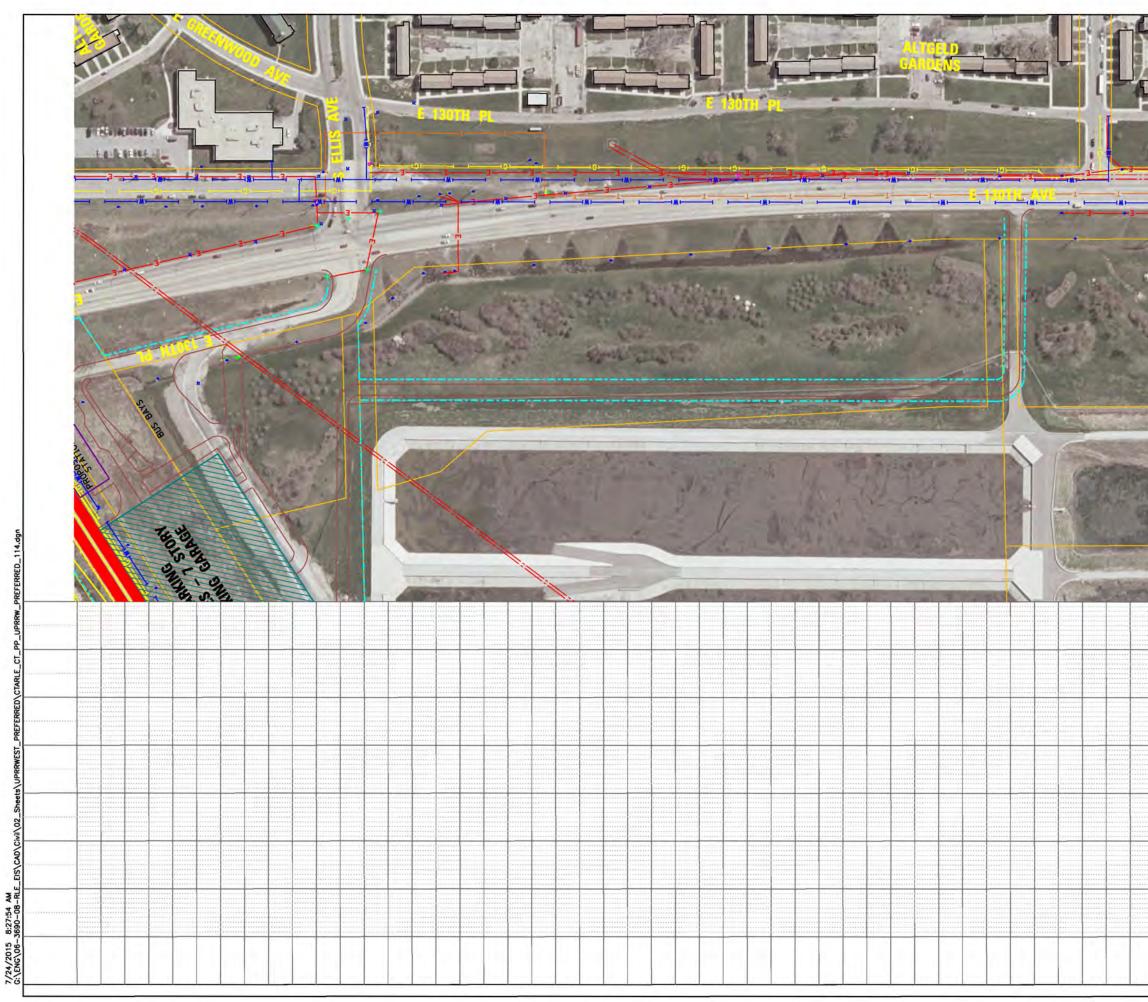
ATCHLINE STA. 337 +		EXISTING TELEPHONE LINE ENTRANCES PRIMARY ENTRANCE SECONDARY ENTRANCE EXIT ONLY PROPOSED PARKING ENTRANCE PROFILE PROFILE PROPOF RAIL PR ELEVATED STRUCTURE DEPTH PR ELEVATED STRUCTURE DEPTH PRELIMINARY DRAFT
		Chicago Transit Group CHICAGO TRANSIT AUTHORITY
	640	SENSITIVE SECURITY INFORMATION where here from conversion scheme and the recommender have a control to here a conversion of the second and the recommendation of the resona where it and to here it and the recommendation and the where the recommendation of the investment of the recommendations and the recommendation of the investment of the recommendations and the recommendation of the investment of the recommendations where the recommendation of the recommendation of the recommendations become the recommendation of the recommendation of the recommendations become the recommendation of the recommendation of the recommendation of the become the recommendation of the recommendation of the recommendation of the second of the recommendation of the recommendation of the recommendation of the second of the recommendation of the recommendation of the recommendation of the second of the recommendation of the recommendation of the recommendation of the second of the recommendation of the recommendation of the recommendation of the second of the recommendation of the recommendation of the recommendation of the second of the recommendation of the recommendation of the recommendation of the second of the recommendation of the recommendation of the recommendation of the recommendation of the second of the recommendation of t
	640 630	
		WHENG HE RECORD COMMENTS SECTION SECTION INFORMATION HAT IS CONTROLLED WHENG & CONTROL AND THE SECTION OF HERE OF IT IS TO CONTROLLED WHEN IS A WITCH ADMENDIA WHEN IS A WITCH ADMENDIA WHEN IS A WITCH ADMENDIA WHEN IS A WITCH ADMENDIA DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE
331 + 0000	630	WHEN THE RECORD COMMENTS SECTION SECTION INFORMATION THAT IS CONTROLLED, INFORM OF THE ADDRESS OF A PARKET IN OF A COMPANY OF A PARKET IN THE ADDRESS OF A PARKET IS AND THE PARKET ADDRESS OF A PARKET IS AND THE ADDRESS OF A PARKET IS AND THE PARKET ADDRESS OF A PARKET IS ADDRESS OF A PARKET IS AND THE PARKET ADDRESS OF A PAR
MATCHLINE STAL 337 + 00:00	630 620	WHENG HES RECEIP COMMENTS SERVICE SCOREY INTENDATION HAT IS CONTROLLED UNDER A CITY PARTS 15 AND 152C. AD PART OF HIS RECORD BY HE CONTROLLED THE DISC WITH A PARTED ON THE ADVECTMENT OF THE RECORDED TO HIS IN COMMENT ADVECTOR OF THE ADVECTMENT OF THE INFORMATION RECEIPTION ADVECTOR OF THE ADVECTMENT OF THE INFORMATION OF THE RECEIPTION OF THE SCORE OF THE ADVECTMENT OF THE INFORMATION RECEIPTION ADVECTOR OF THE ADVECTMENT OF THE INFORMATION RECEIPTION OF THE SCORE OF THE ADVECTMENT OF THE INFORMATION RECEIPTION OF THE SCORE OF THE SCORE OF THE INFORMATION RECEIPTION OF THE SCORE OF THE SCORE OF THE INFORMATION OF THE INFORMATION RECEIPTION OF THE SCORE OF THE SCORE OF THE SCORE OF THE RECEIPTION OF THE SCORE OF THE SCORE OF THE SCORE OF THE SCORE OF THE RECEIPTION OF THE SCORE OF THE SCORE OF THE SCORE OF THE SCORE OF THE RECEIPTION OF THE SCORE OF THE SCORE OF THE SCORE OF THE SCORE OF THE RECEIPTION OF THE SCORE OF THE SCORE OF THE SCORE OF THE SCORE OF THE RECEIPTION OF THE SCORE OF THE SCORE OF THE SCORE OF THE SCORE OF THE RECEIPTION OF THE SCORE OF THE SCORE OF THE SCORE OF THE SCORE OF THE RECEIPTION OF THE SCORE OF THE SCORE OF THE SCORE OF THE SCORE OF THE RECEIPTION OF THE SCORE OF THE SCORE OF THE SCORE OF THE SCORE OF THE RECEIPTION OF THE SCORE O



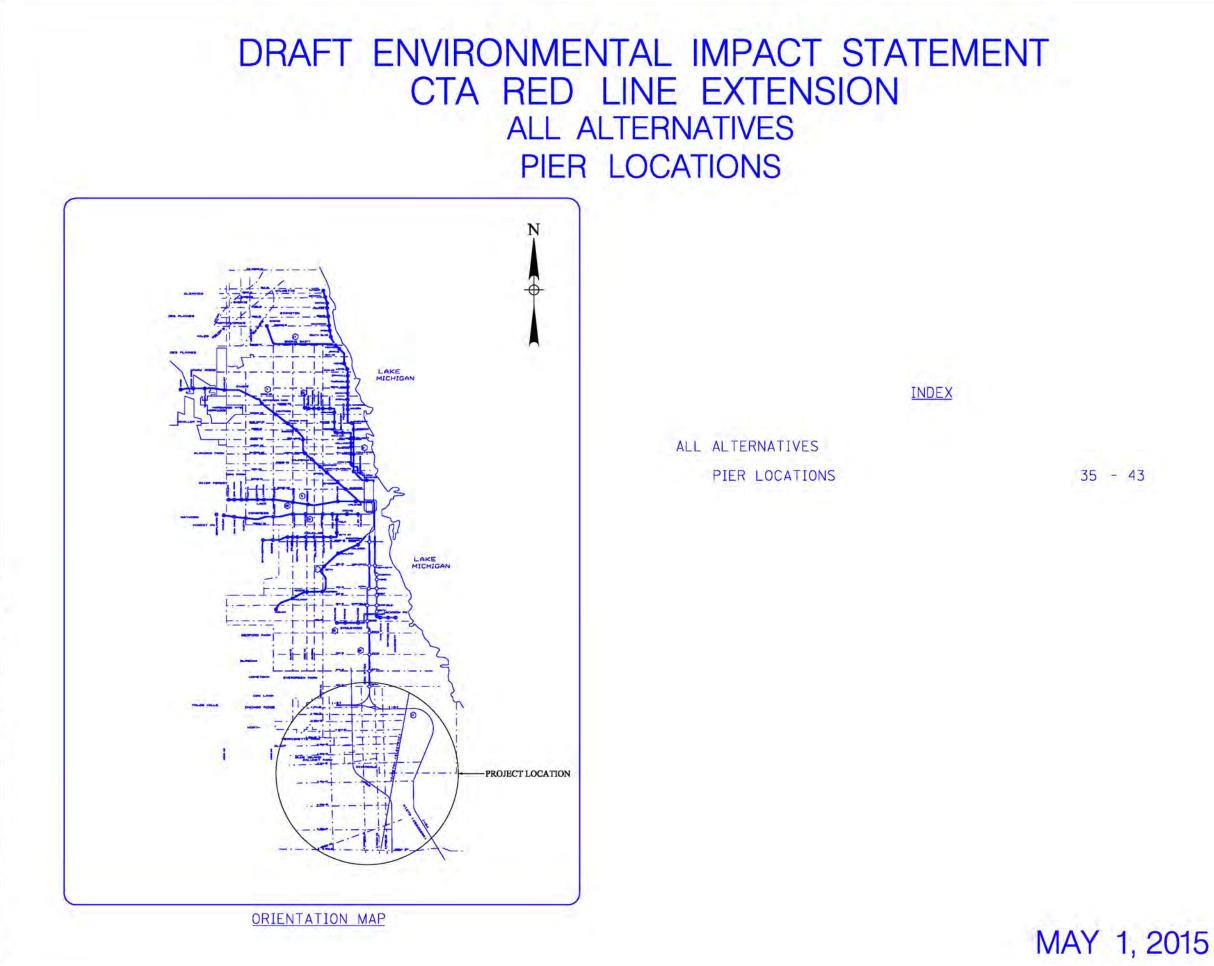
PR MWRD		LEGEND         PLAN         PR CENTERLINE         PR CENTERLINE         PR TRACK STRUCTURE         EX ROADWAY RIGHT-OF-WAY         EX ROADWAY RIGHT-OF-WAY         EX ROADWAY RIGHT-OF-WAY         EX ROADWAY HARDSCAPE         PR PERMANENT ENVELOPE         PR PERMANENT ENVELOPE         PR PARMANENT ENVELOPE         PR PERMANENT ENVELOPE         PR PARKING         PR TACK SUBSTATION         UTILITIES         EXISTING WATER LINES         EXISTING GAS LINE         EXISTING GAS LINE         EXISTING GAS LINE         EXISTING ELECTRIC LINE         EXISTING FELEPHONE LINE         ENTRANCES         PRIMARY ENTRANCE         SECONDARY ENTRANCE         EXISTING TELEPHONE LINE         ENTRANCES         PROPOSED PARKING ENTRANCE         EXISTING TELEPHONE LINE         EXISTING TELEPHONE         PROFILE         EX GROUND         PRELIMINARY DRAFT         CHICAGO TRANSIT      <
		EXISTING WATER LINES EXISTING SEWER LINE EXISTING GAS LINE EXISTING ELECTRIC LINE EXISTING TELEPHONE LINE EXISTING TELEPHONE LINE ENTRANCES PRIMARY ENTRANCE SECONDARY ENTRANCE EXIT ONLY PROPOSED PARKING ENTRANCE PROFILE EX GROUND PR TOP OF RAIL PR ELEVATED STRUCTURE DEPT EX GROUND PRELIMINARY DRAFT PRELIMINARY DRAFT CECCO CHICAGO TRANSIT AUTHORITY SENSITIVE SECURITY INFORMATION
		PRELIMINARY DRAFT PRELIMINARY DRAFT Greet Transit Group CHICAGO TRANSIT AUTHORITY SENSITIVE SECURITY INFORMATION
		CHICAGO TRANSIT AUTHORITY SENSITIVE SECURITY INFORMATION
SS ROAD		
SS ROAD		RESULT IN CALL PENALTY OR OTHER ACTION. FOR U.S. GOVERNMENT AGENCIES, PUBLIC DISCLOSURE IS GOVERNED BY 5 U.S.C. 552 AND 49 CFR PARTS 15 AND 1520.
	640	DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY,
	630	BY SCALING OF THIS DRAWING.
- 00	620	ENVIRONMENTAL IMPACT STUDY
	610	32 OF 51
23-0- MIN. CLEARANCE INE STA. 360 + 00.00	600	IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY
	590	PROJECT NO CTA-RLE FILE NAME CTARLE_CT_PP_UPRRW_PREFERRED_ 100 0 100 200
	580	SCALE H 1" = 100'-0"
00.00 589.00 589.00		



	LEGEND
	PLAN       EX CENTERLINE         PR       PR CENTERLINE         PR TRACK CENTERLINE       PR TRACK CENTERLINE         PR TRACK STRUCTURE       PR TRACK CENTERLINE         PR STATION PLATFORM       EX BUILDING OUTLINE         PR BUILDING OUTLINE       PR PARKING         PR TRACK CENTERLINE       PR UPRR TRACK CENTERLINE         UTILITIES       PR UPRR TRACK CENTERLINE         UTILITIES       EXISTING SEWER LINE         EXISTING SEWER LINE       EXISTING GAS LINE         EXISTING TELEPHONE LINE       EXISTING TELEPHONE LINE         PROPOSED PARKING ENTRANCE       SECONDARY ENTRANCE         PROPOSED PARKING ENTRANCE       EXIT ONLY         PR TOP OF RAIL       PR ELEVATED STRUCTURE DEPTH         PR ELEVATED STRUCTURE DEPTH       PR ELEVATED STRUCTURE DEPTH
· dit	PRELIMINARY DRAFT
In referre	Gy E Transit Group
	CHICAGO TRANSIT AUTHORITY
	AUTOMIT
	SENSITIVE SECURITY INFORMATION
640	
640 630 620	SENSITIVE SECURITY INFORMATION WHENC HE RECORD CONTARTS SECURITY INFORMATION WHENC HE RECORD CONTARTS SECURITY INFORMATION IN IT IS CONTIDUED INFO TO THE SECURITY OF THE SECURITY INFORMATION IN IT IS CONTIDUED WHEN THE WITH THE SECURITY OF THE SECURITY INFORMATION OF THE THEORY WHEN THE WITH THE SECURITY OF THEORY OF THE SECURITY IS AND USED WHEN THE WITH THE SECURITY OF THEORY OF THE SECURITY IS AND USED WHEN THE WITH THE SECURITY OF THEORY OF THE SECURITY IS AND USED WHEN THE WITH THE SECURITY OF THEORY OF THE SECURITY IS AND USED DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE
630	SENSITIVE SECURITY INFORMATION WEDGE DES RECORD COMMANS SERVICE SECURITY INFORMATION WEDGE DES RECORD COMMANS SERVICE SECURITY INFORMATION THAT IS CONTINUED WITH DES RECORD COMMANS SERVICE SECURITY INFORMATION THAT IS CONTINUED WITH DESTRUCTION OF THAT SECURITY OF THE RECORD WITH SECURITY OF WITH DESTRUCT ANNALOD OF THAT SECURITY OF THE RECORD OF THE THAT SECURITY OF WITH SECURITY OF THE SECURITY OF THE RECORD OF THE THAT SECURITY OF WITH SECURITY OF THE RECORD OF THE THAT SECURITY OF WITH SECURITY OF THE RECORD OF THE THAT SECURITY OF WITH SECURITY OF THAT SECURITY OF THE RECORD OF THE THAT SECURITY OF WITH SECURITY OF THE RECORD OF THE THAT SECURITY OF WITH SECURITY OF THE RECORD OF THE THAT SECURITY OF WITH SECURITY OF THE RECORD OF THE THAT SECURITY OF WITH SECURITY OF THE RECORD OF THE SECURITY OF
_630 _620	SENSITIVE SECURITY INFORMATION WHENC HE RECORD COMMAN SENSITY AND PROVIDENT AND ADDRESS SENSITY AND ADDRESS SENSITY AND ADDRESS SENSITY AND ADDRESS AND AND ADDRESS AD
630 620 610	SENSITIVE SECURITY INFORMATION WHENE HEINER COMPOSITION SUBJECT STUDY INFORMATION WHENE HEINER STORE COMMENTS SUBJECT STUDY INFORMATION WHENE IS A SUBJECT STUDY OF THE AVERAGE AND A DESTINATION OF THE INFORMATION OF INFORMATION OF THE INFORMATION OF THE INFORM
630 620 610 600	SENSITIVE SECURITY INFORMATION WHENCE THE MODEL OF CONTRACT SECURITY INFORMATION WHENCE THE MODEL OF CONTRACT SECURITY INFORMATION WHENCE THE MODEL OF THE SECURITY INFORMATION THE THE CONTROL OF THE THE SECURITY WHENCE WHENCE AND THE OFFICE OF THE SECURITY OF THE THE SECURITY SECURITY SECURITY SECURITY OF THE THE SECURITY OF THE SECURT OF THE SECURITY OF THE SECUL

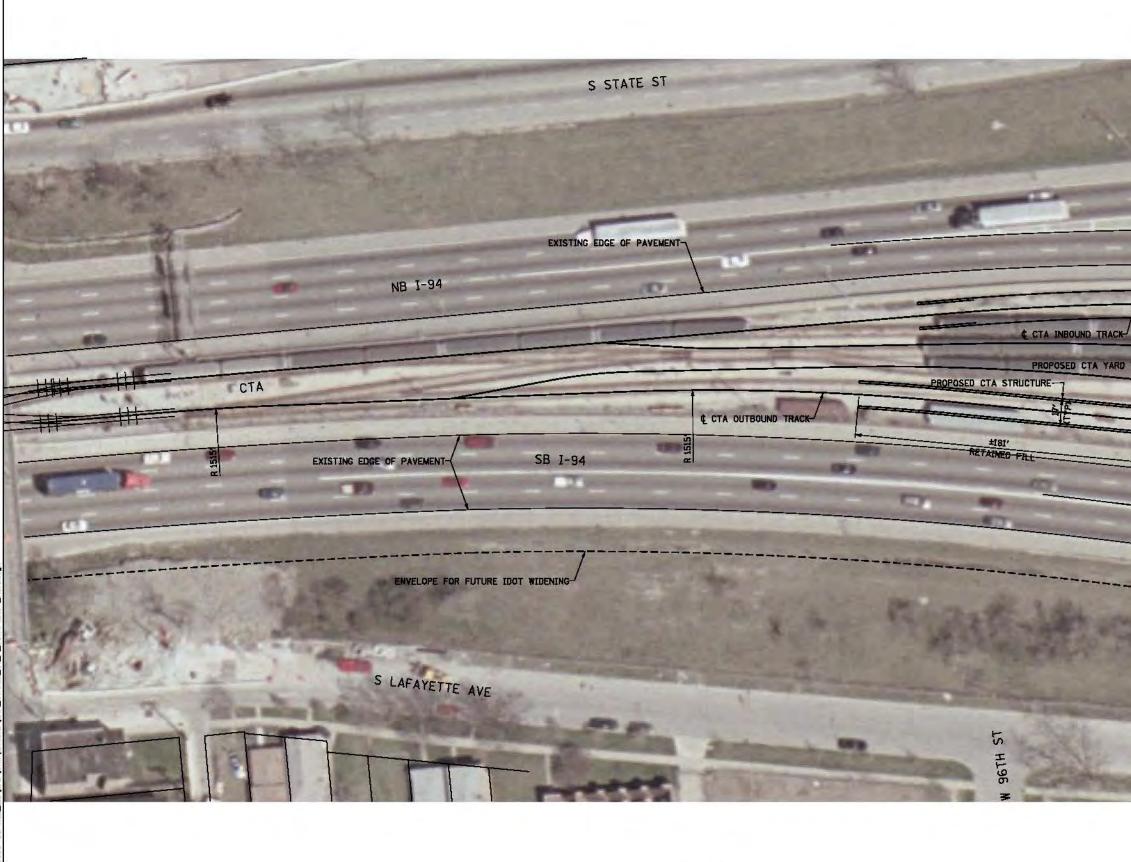


	[]
	LEGEND PLAN
19 W (19 19	EX TRACK CENTERLINE
	PR TRACK CENTERLINE PR TRACK STRUCTURE
	EX ROADWAY RIGHT-OF-WAY
	PR PERMANENT ENVELOPE
	PR ROADWAY/HARDSCAPE PR STATION PLATFORM
Ň	
	PR PARKING
	PR CTA SUBSTATION
- Contraction of the second	> EXISTING SEWER LINE
and the second second	EXISTING GAS LINE
- AND DE LAND	EXISTING TELEPHONE LINE
100 miles	A PRIMARY ENTRANCE
and an and the first the	SECONDARY ENTRANCE
and a set	PROPOSED PARKING ENTRANCE
ALL AND	EX GROUND
With the second second second	PR TOP OF RAIL PR ELEVATED STRUCTURE DEPTH
State State	PLATFORM LOCATION
and the second	PRELIMINARY DRAFT
	Gy C'Transit Group
1	CYVC Transit Group
and the state	cta
C. C	
14-17-1-1-1-1-1-1-1-1-1-1-1-1-1-1-1-1-1-	CHICAGO TRANSIT
Card and a second	AUTHORITY
	SENSITIVE SECURITY INFORMATION
5.0	WHENHOL THIS RECORD CONTAMS SCISTINE SECURITY INFORMATION THAT IS CONTROLLED
	UNCER 44 OTR PARTS 15 AND 1320, NO PART OF THIS RECORD MY BE DISCUSSED 10 PERSIGNS WINDUR TAYLED IN INDIA'S SECTION OF ATR PARTS 15 AND 1320, DECORT WITH THE WINTEN PORSISCIE OF THE ADMINISTRATION OF THE TRANSPORTATION SECURITY ADMINISTRATION OF THE SECTION OF TOTAL SECTION OF ADMINISTRATION SECURITY RESULT IN CAR. PRANET, OR TOMOR ACTION, LI JAUNGED RELOSE, MART RESULT IN CAR. PRANET, OR TOMOR ACTION, LI JAUNGED RELOSE, MART DISCUSSED AND ADMINISTRATION OF ANY ADMINISTRATION OF ADMINISTRATION DISCUSSED AND ADMINISTRATION OF ANY ADMINISTRATION OF ADMINISTRATION DISCUSSED ANY ADMINISTRATION OF ADMINISTRATION OF ADMINISTRATION DISCUSSED ANY ADMINISTRATION OF ADMINISTRATION OF ADMINISTRATION OF ADMINISTRATION DISCUSSED ADMINISTRATION OF ADMINISTRATION OF ADMINISTRATION OF ADMINISTRATION DISCUSSED ADMINISTRATION OF ADMINISTRATION OF ADMINISTRATION OF ADMINISTRATION OF ADMINISTRATION DISCUSSED ADMINISTRATION OF ADMINISTRATION OF ADMINISTRATION OF ADMINISTRATION OF ADMINISTRATION OF ADMINISTRATION DISCUSSED ADMINISTRATION OF ADMINISTRAT
	RESULT N OVE, RENELT OR DINER ACTON, FOR U.S. CONDINIED NODICES, PUBLIC DISCLOSURE IS CONTINUED BY 5 U.S.C. 552 MID 49 CTR PARTS 15 MID 1520.
	DRAWING SCALE IS NOT GUARANTEED.
	CTA ASSUMES NO RISK OF LIABILITY FOR
	ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING.
	RED LINE EXTENSION
	ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD
	ALTERNATIVE
	WEST OPTION
	34 OF 51
1	IN CHARGE
	APPROVED BY
	CHECKED BY DESIGNED BY
	DRAWN BY
	PROJECT NO CTA-RLE
	FILE NAME CTARLE_CT_PP_UPRRW_PREFERRED_114.dgr
	100 0 100 200
	SCALE H 1" = 100'-0" V 1" = 10'-0"
	4 I = 10-0
	MARK DATE DESCRIPTION
	SHEET TITLE



7/24/2015 8:27:58 AM G:\ENG\06-3690-08-RL





7/24/2015 9-03-55 AM G/EM/06/08-3880-08-RLE ES/CAV/CM/EMINIA/EXH\_CTARE ST\_P\_95THPER01

	1
z	
	PRELIMINARY DRAFT
IRACKS-	Cover Transit Group
ABUTMENT (TYP)	cta
HAMMERHEAD PIER-	CHICAGO TRANSIT AUTHORITY
	SENSITIVE SECURITY INFORMATION WHEN THE RECEIPTING ADDRESS ADD
	DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING.
	RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY
	ALL ALTERNATIVES PIER LOCATION SOUTH OF 95TH ST STATION
Children of the local division of the local	36 OF 51 IN CHARGE
and the second	APPROVED BY CHECKED BY
1 47 1	DESIGNED BY DRAWN BY
COLUMN STREET	PROJECT NO CTA-RLE FILE NAME EXH_CTARLE_ST_P_95THPIER01_North
	30 0 30 60 SCALE H 1" = 30'-0"
	MARK DATE DESCRIPTION
	SHEET TITLE

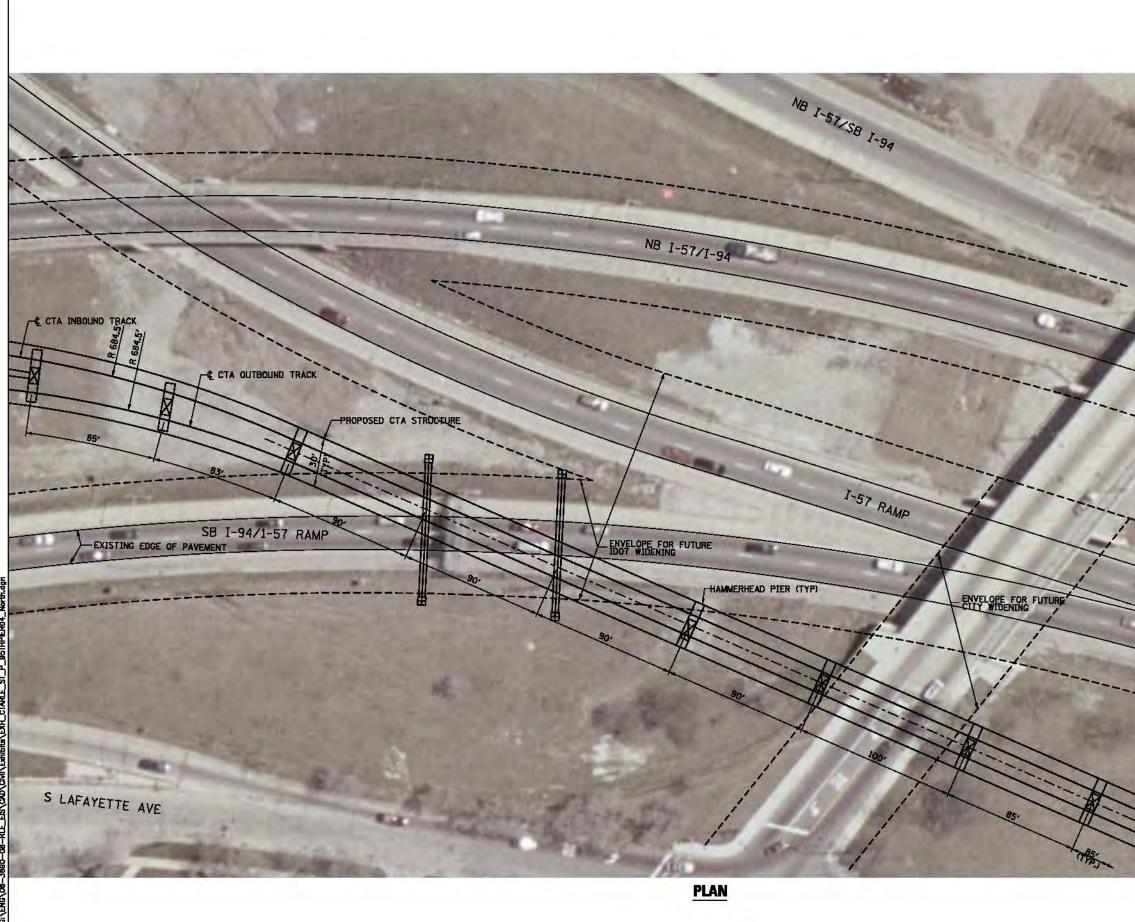


7/24/2015 9:03:58 AM G'LENG\OG-3880-08-RLE ESYCAD\CMI\Edminite\EXH\_CTARE\_ST\_P

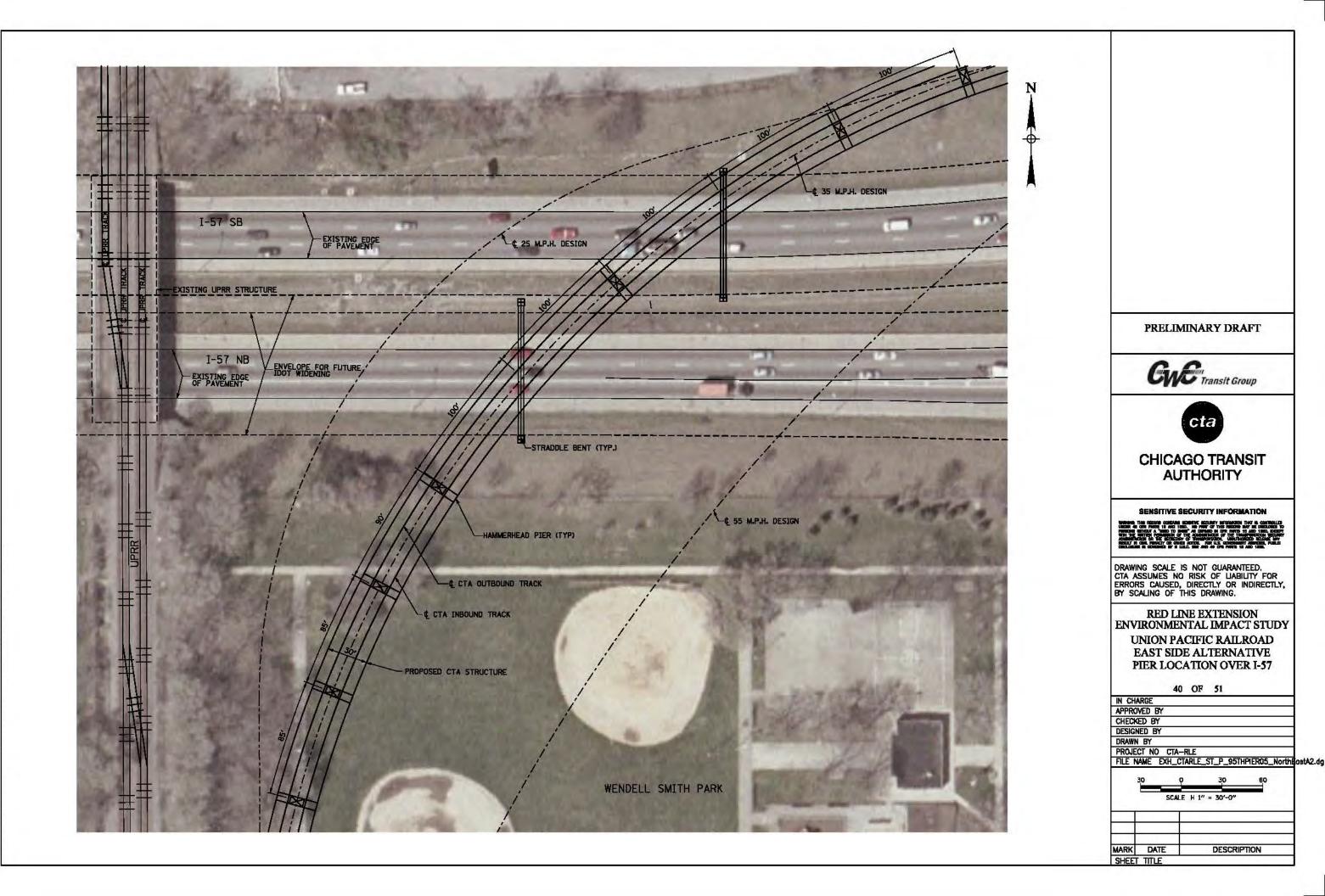
	1
2	
G SATH YARD TRACKS	
RE	PRELIMINARY DRAFT
	Gy Entransit Group
	cta
	CHICAGO TRANSIT AUTHORITY
	SENSITIVE SECURITY INFORMATION WHEN THE CONTRACT CONTRACT AND A CONTRACT WHEN THE CONTRACT AND A CONTRACT AND A CONTRACT WHEN THE CONTRACT AND A CONTRACT AND A CONTRACT A MARKET AND A CONTRACT AND A CONTRACT AND A CONTRACT AND A CONTRACT AND A CONTRACT AND A CONTRACT AND A MARKET AND A CONTRACT AND A CONTRACT AND A CONTRACT AND A MARKET AND A CONTRACT AND A CONTRACT AND A CONTRACT AND A MARKET AND A CONTRACT AND A CONTRACT AND A CONTRACT AND A MARKET AND A CONTRACT AND A CONTRACT AND A CONTRACT AND A MARKET AND A CONTRACT AND A CONTRACT AND A CONTRACT AND A MARKET AND A CONTRACT AND A CONTRACT AND A CONTRACT AND A MARKET AND A CONTRACT AND A CONTRACT AND A CONTRACT AND A MARKET AND A CONTRACT AND A CONTRACT AND A CONTRACT AND A MARKET AND A CONTRACT AND A CONTRACT AND A CONTRACT AND A CONTRACT AND A MARKET AND A CONTRACT AND A CONTRACT AND A CONTRACT AND A CONTRACT AND A MARKET AND A CONTRACT AND A CONTRACT AND A CONTRACT AND A CONTRACT AND A MARKET AND A CONTRACT AN
	DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING.
	RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY ALL ALTERNATIVES
	PIER LOCATION SOUTH OF 95TH ST STATION 37 OF 51 IN CHARGE
	IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE
	FILE NAME EXH_CTARLE_ST_P_95THPIER02_North
GEND	30 0 30 60 SCALE H 1" = 30'-0"
RE-DECK EXISTING TUNNEL SUPERSTRUCTURE	
	MARK DATE DESCRIPTION

ENVELOPE FOR FUTURE, ISTING SETH YARD IBACKS NB 1-57/1-94 RAMP -EXISTING EDGE OF PAVEMEN 85' 79 -HAMMERHEAD PIER (TYP) E CTA INBOUND TRACK CTA OUTBOUND TRACK 1723 74' 74' PROPOSED CTA STRUCTURE EXISTING EDGE OF PAVEMENT SB 1-94/1-57 RAMP ENVELOPE FOR FUTURE S LAFAYETTE AVE 7/24/2015 9-04:00 AM PLAN

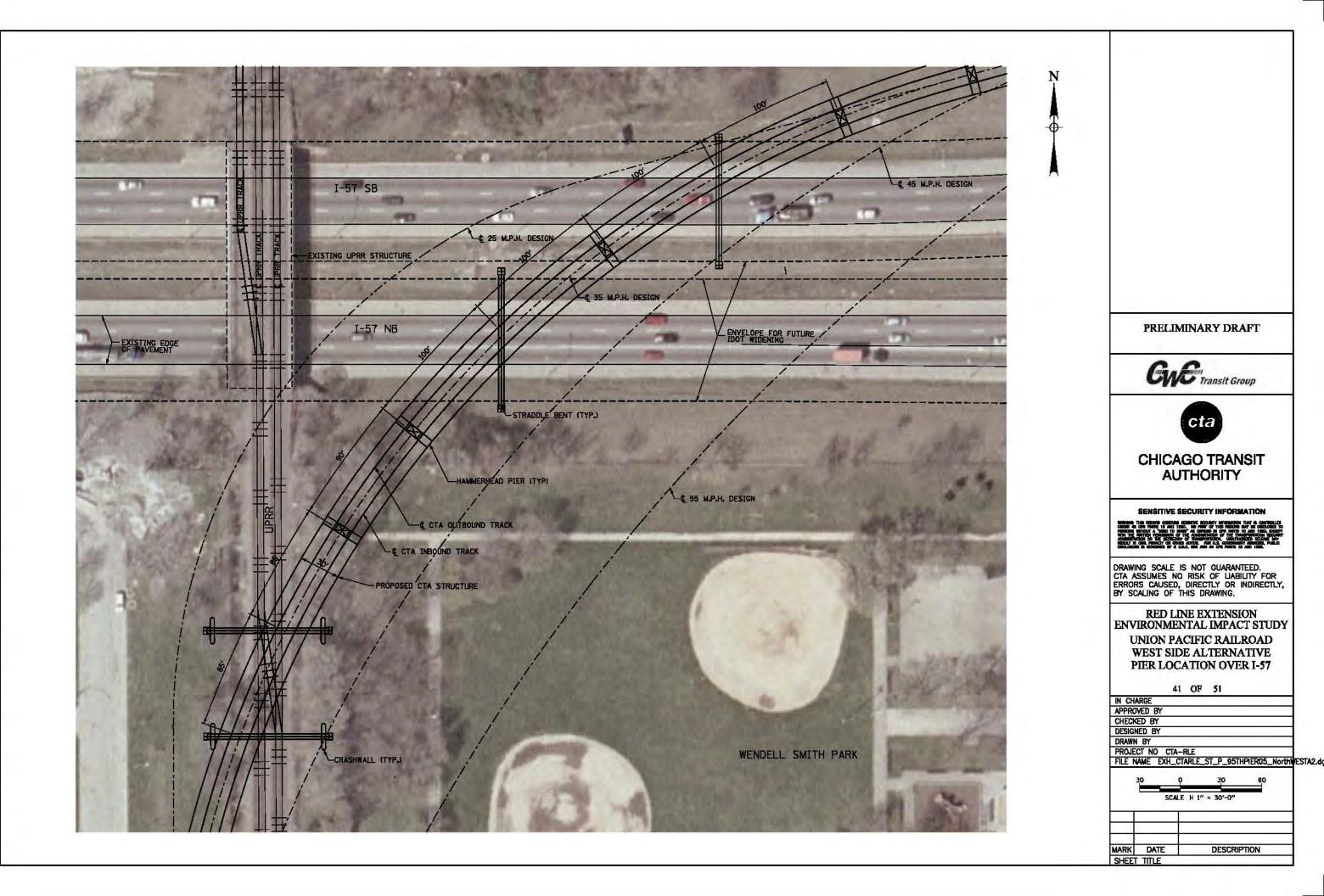
*	
14	PRELIMINARY DRAFT
	Gy E Transit Group
	cta
Z i	CHICAGO TRANSIT AUTHORITY
	SENSITIVE SECURITY INFORMATION
CT/	AWING SCALE IS NOT GUARANTEED. A ASSUMES NO RISK OF LIABILITY FOR RORS CAUSED, DIRECTLY OR INDIRECTLY, SCALING OF THIS DRAWING.
E	RED LINE EXTENSION WIRONMENTAL IMPACT STUDY
	ALL ALTERNATIVES PIER LOCATION SOUTH OF 95TH ST STATION
IN	38 OF 51 CHARGE
AP CH	PROVED BY ECKED BY
DR. PR	SIGNED BY AWN BY OJECT NO CTA-RLE
	E NAME EXH_CTARLE_ST_P_95THPIER03_North. 30 0 30 60 SCALE H 1" = 30'-0"
E	
MAR	
	RK DATE DESCRIPTION EET TITLE



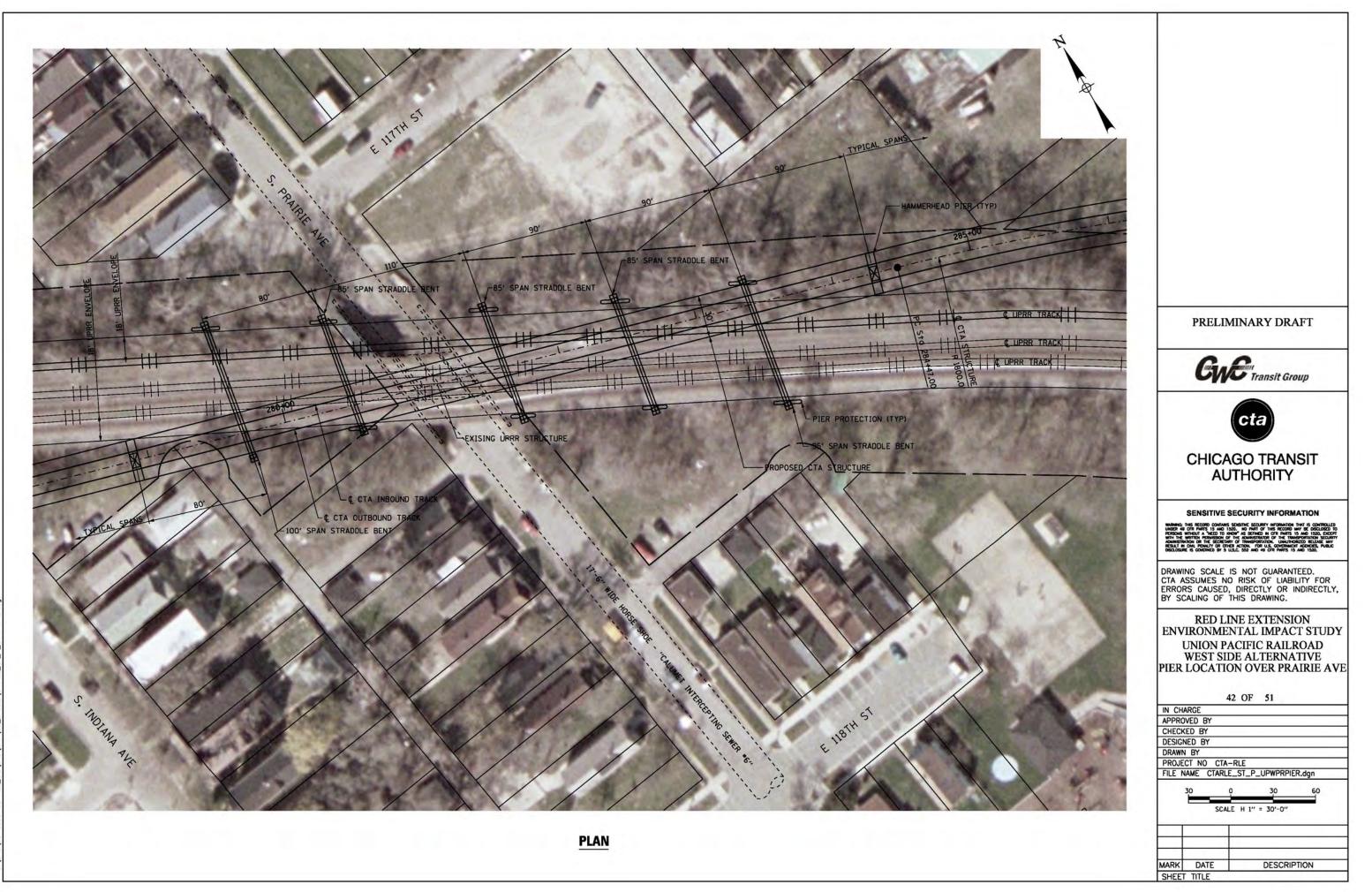
1 K	
S WETTWORTH AND	PRELIMINARY DRAFT
	Gwert Transit Group
	Cta
	CHICAGO TRANSIT AUTHORITY
	SENSITIVE SECURITY INFORMATION WHEN IN ADDA OF THE SENTE EXAMPLE TO A CONTRAL UNDER A OF FAMILY IN ADDITION IN THE CONTRAL OF WHEN A OF FAMILY ADDITION OF THE OF THE SENTE ADDITION THE ADDITION OF THE OFFICE ADDITION OF THE OFFICE ADDITION THE ADDITION OF THE OFFICE ADDITION OF THE OFFICE ADDITION THE ADDITION OF THE OFFICE ADDITION OF THE OFFICE ADDITION THE ADDITION OF THE OFFICE ADDITION OF THE OFFICE ADDITION THE ADDITION OF THE OFFICE ADDITION OF THE OFFICE ADDITION THE ADDITION OF THE OFFICE ADDITION OF THE OFFICE ADDITION OF THE DECLARE & CONSIDER OF THE OFFICE ADDITION OF THE OFFICE ADDITION THE OFFICE ADDITION OF THE OFFICE ADDITION OF THE OFFICE ADDITION THE OFFICE ADDITION OF THE OFFICE ADDITION OF THE OFFICE ADDITION THE OFFICE ADDITION OF THE OFFICE ADDITION OF THE OFFICE ADDITION THE OFFICE ADDITION OF THE OFFICE ADDITION OF THE OFFICE ADDITION THE OFFICE ADDITION OF THE OFFICE ADDITION OF THE OFFICE ADDITION THE OFFICE ADDITION OF THE OFFICE ADDITION OF THE OFFICE ADDITION THE OFFICE ADDITION OFFICE ADDITION OF THE OFFICE ADDITION OFFICE ADDITION THE OFFICE ADDITION OF THE OFFICE ADDITION OFFICE ADDITION OFFICE ADDITION THE OFFICE ADDITION OFFICE AD
	DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING.
	RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY
	ALL ALTERNATIVES PIER LOCATION SOUTH OF 95TH ST STATION
	39 OF 51 IN CHARGE
	APPROVED BY CHECKED BY DESIGNED BY
and the second second	DRAWN BY PROJECT NO CTA-RLE
	FILE NAME EXH_CTARLE_ST_P_95THPIER04_North
Antical	
	MARK DATE DESCRIPTION SHEET TITLE



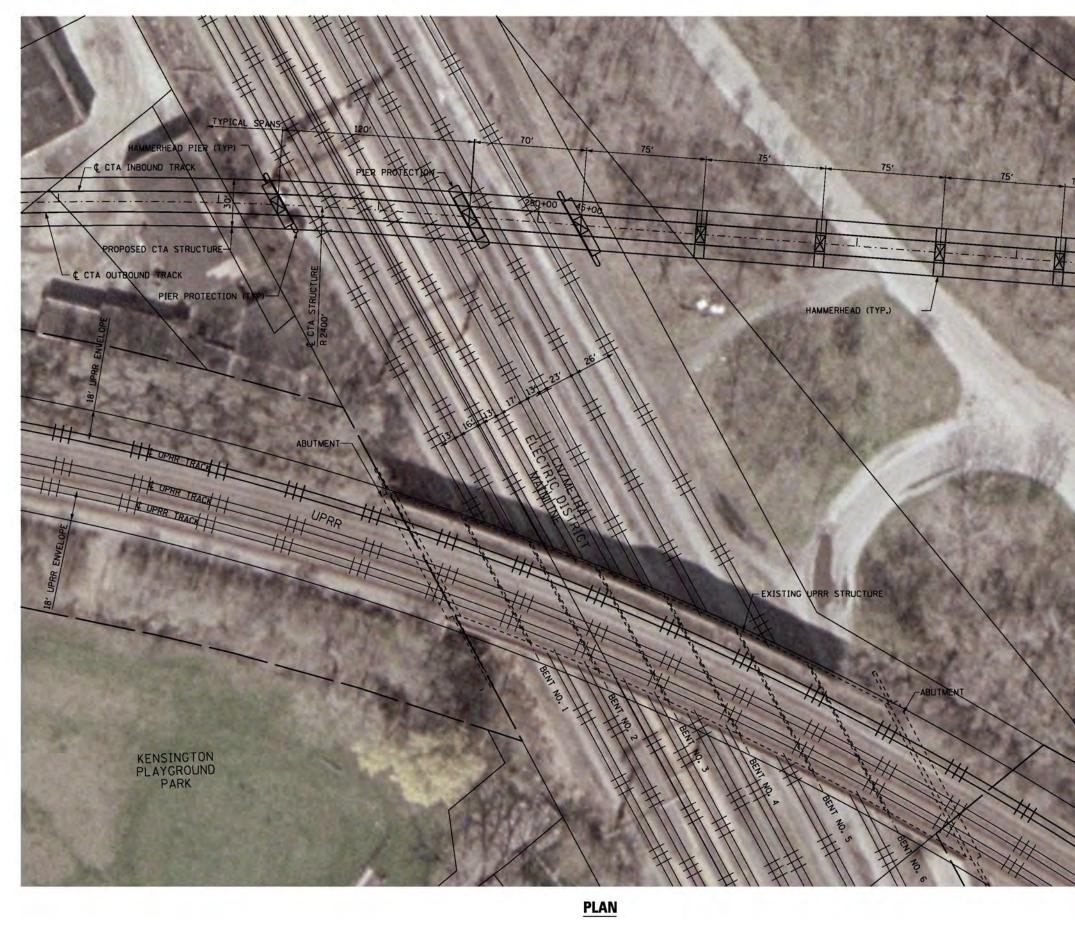
7/24/2015 9:04:08 AM G\ENG\O@-3890-08-RLE\_ES\CAU\CM\\Exhibits\EXH\_CTARE\_ST\_P\_95THPIER05\_NorthEomt/2.di



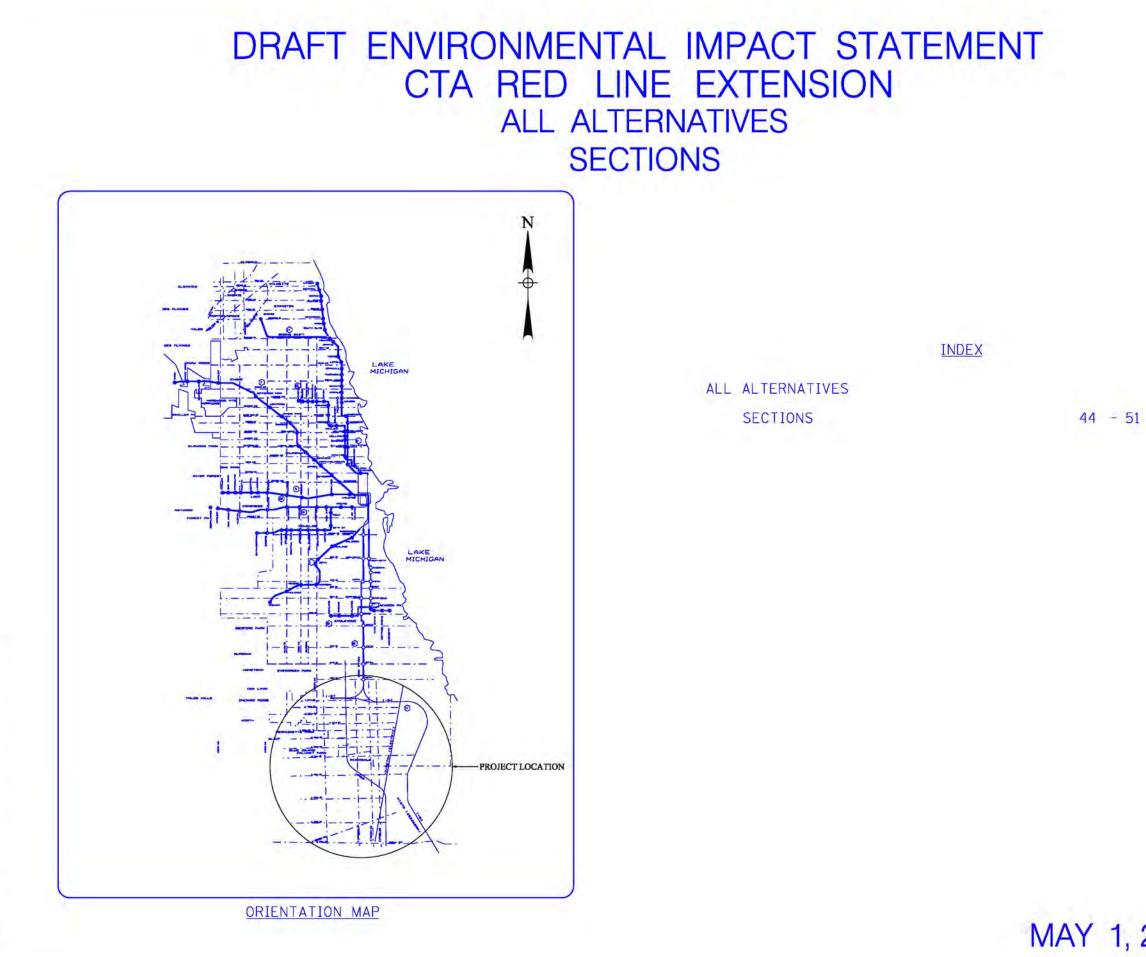
7/24/2015 9:04:08 AM Gi/ENG/06-3890-08-RLE\_ES\CAU\CMI\EMID#&\EXH\_CTARE\_ST\_\_95THPER05\_NortHWESTA2.4



7/24/2015 9:23:40 AM G:\ENC\06-3690-08-RLE\_EIS\CAD\Civil\02\_Sheets\CTARLE\_ST\_P\_UPWPF



PRELIMINARY DRAFT
0.0
Gy E"Transit Group
cta
CHICAGO TRANSIT AUTHORITY
SENSITIVE SECURITY INFORMATION
WRINE, his score occases science science wromethor has a compute income of or write 1 and 136. In or write of the record wire of occase to response wrong rates to 100 and 200 and 100 and 100 and 100 and 100 and write wrong response of the awardstate of the response science and the wrong response of the awardstate of the response science response and the response of the response and the response response of the response of the response of the response response of the response of the response of the response of the response of the response of the response of the response of the response of the response of the response of the response of the response of the response of the response of the response of the response of the response of the response of the respons
DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING.
RED LINE EXTENSION
ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD
EAST & WEST SIDE ALTERNATIVE PIER LOCATION OVER CN/METRA
43 OF 51
IN CHARGE APPROVED BY
CHECKED BY DESIGNED BY
DRAWN BY PROJECT NO CTA-RLE
FILE NAME CTARLE_ST_P_UPEMETPIER.dgn
FILE NAME CTARLE_ST_P_UPEMETPIER.dgn 30 0 30 60
FILE NAME CTARLE_ST_P_UPEMETPIER.dgn 30 0 30 60



7/24/2015 9:30:15 AM G:\ENG\06-3690-08-RL

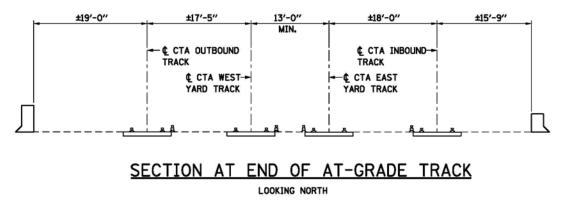


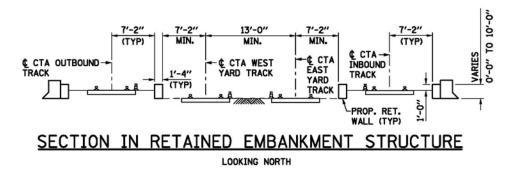
é	00	HE
Γ.	20	)15
.,		

	PRELIM	IINARY DRAFT
	Gw	Grant Group
		cta
0	HICA	GO TRANSIT
		THORITY
s	ENSITIVE S	ECURITY INFORMATION
ADMINISTRATIO	R PARTS 15 AND 154 R PARTS 15 AND 154 OUT A "NEED TO KNI TIEN PERNISSION OF N OR THE SECRETARY N. PENALTY OR OTHE S CONSIMED BY 5 LU	Szestre Szchety modsandom that is commodized on "A so part of this records may be declared to of" As defined in CTR Parts 13 Ano. 1520, decept the Administrative of the transformation siccularity (of transformatice of the transformation siccularity (of transformatice of the transformation siccularity (of transformatice of the size), usual transformation siccularity (of transformatice of the size), usual transformation siccularity (of transformatice of the size), usual transformation size (the size), (of the size), usual transformation of the size of th
CTA ASS	SUMES NO	S NOT GUARANTEED. RISK OF LIABILITY FOR DIRECTLY OR INDIRECTLY, HIS DRAWING.
BY SCA	1.201.5	HIS DRAWING. NE EXTENSION
ENVI	ALL A	NTAL IMPACT STUDY LTERNATIVES ECTIONS
	4	4 OF 51
IN CHAR		
CHECKED		
DRAWN		RLE
FILE NAM	ME CTARLE	_G_COVER_Sections.dgn
MARK	DATE	DESCRIPTION
	TITLE	

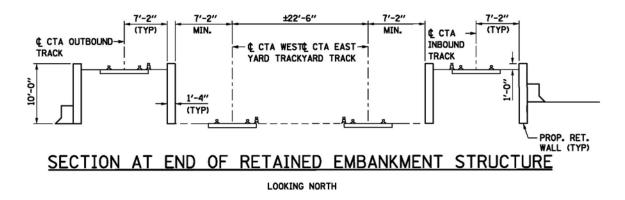


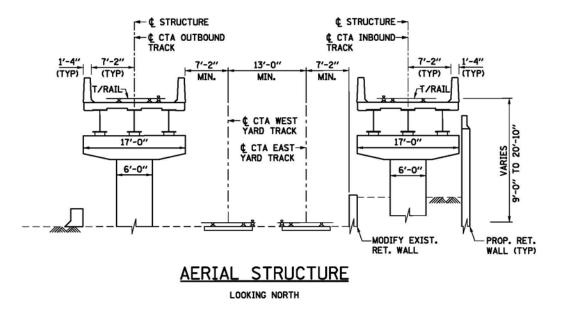
LOOKING NORTH



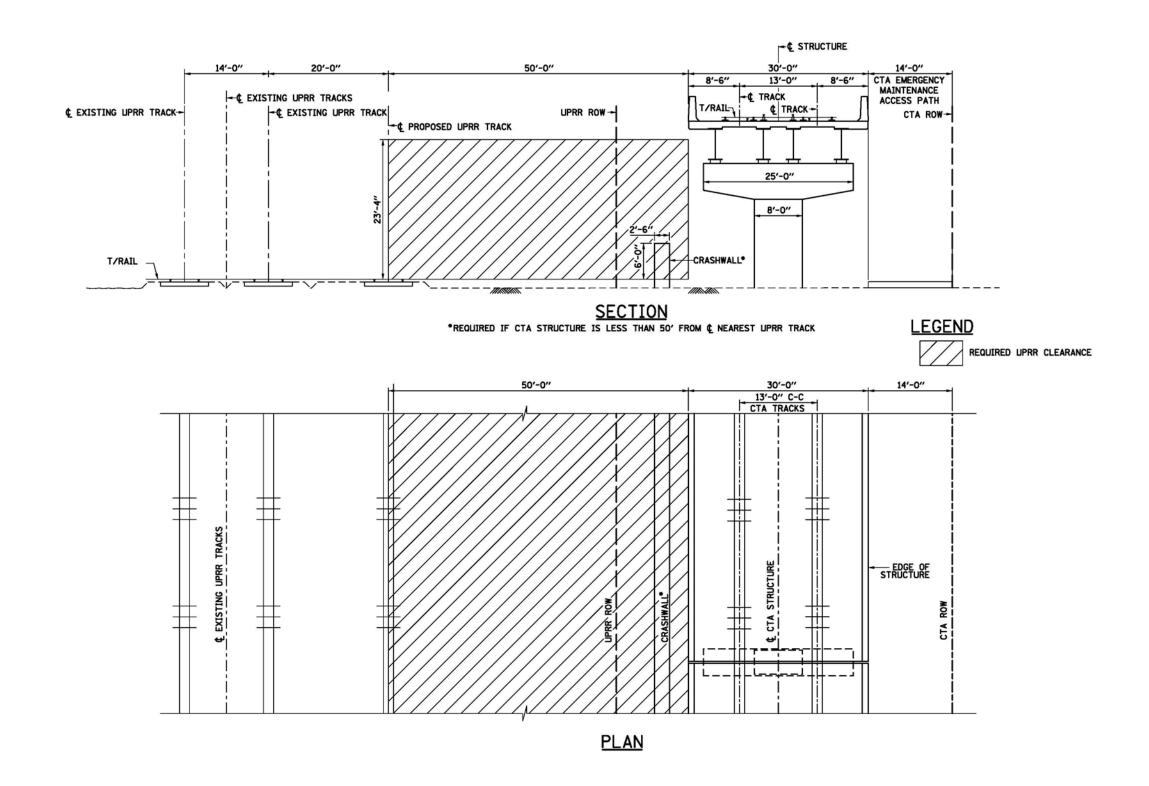


PRELIN	MINARY DRAFT
Си	<b>E</b> Transit Group
	cta
CHICA	GO TRANSIT
	THORITY
SENSITIVE	SECURITY INFORMATION
PERSONS WITHOUT A "NEED TO I WITH THE WRITTEN PERMISSION O ADMINISTRATION OR THE SECRETA RESULT IN CML PENALTY OR OT DISCLOSURE IS GOVERNED BY 5	NOW" AS DEFINED IN G'R PARTS 15 AND 1530, EDCEPT IF THE ADMINISTRATOR OF THE TRANSPORTATION SECURITY RY OF TRANSPORTATION. UNAUTHORIZZD RELEASE MAY HER ACTION. FOR U.S. GOVERNMERT ADMINES, PUBLIC U.S.C. 552 AND 48 G'R PARTS 15 AND 1520.
DRAWING SCALE	IS NOT GUARANTEED.
CTA ASSUMES N ERRORS CAUSED BY SCALING OF	O RISK OF LIABILITY FOR , DIRECTLY OR INDIRECTLY,
	INE EXTENSION INTAL IMPACT STUDY
	ALTERNATIVES SOUTH OF 95TH ST.
	50011101 9511151.
SECTIONS	
SECTIONS 4 IN CHARGE	5 OF 51
SECTIONS 4 IN CHARGE APPROVED BY CHECKED BY	
SECTIONS 4 IN CHARGE APPROVED BY	
4 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA	5 OF 51
4 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA	-RLE
4 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA	-RLE
4 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA	-RLE
A SECTIONS IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA	-RLE



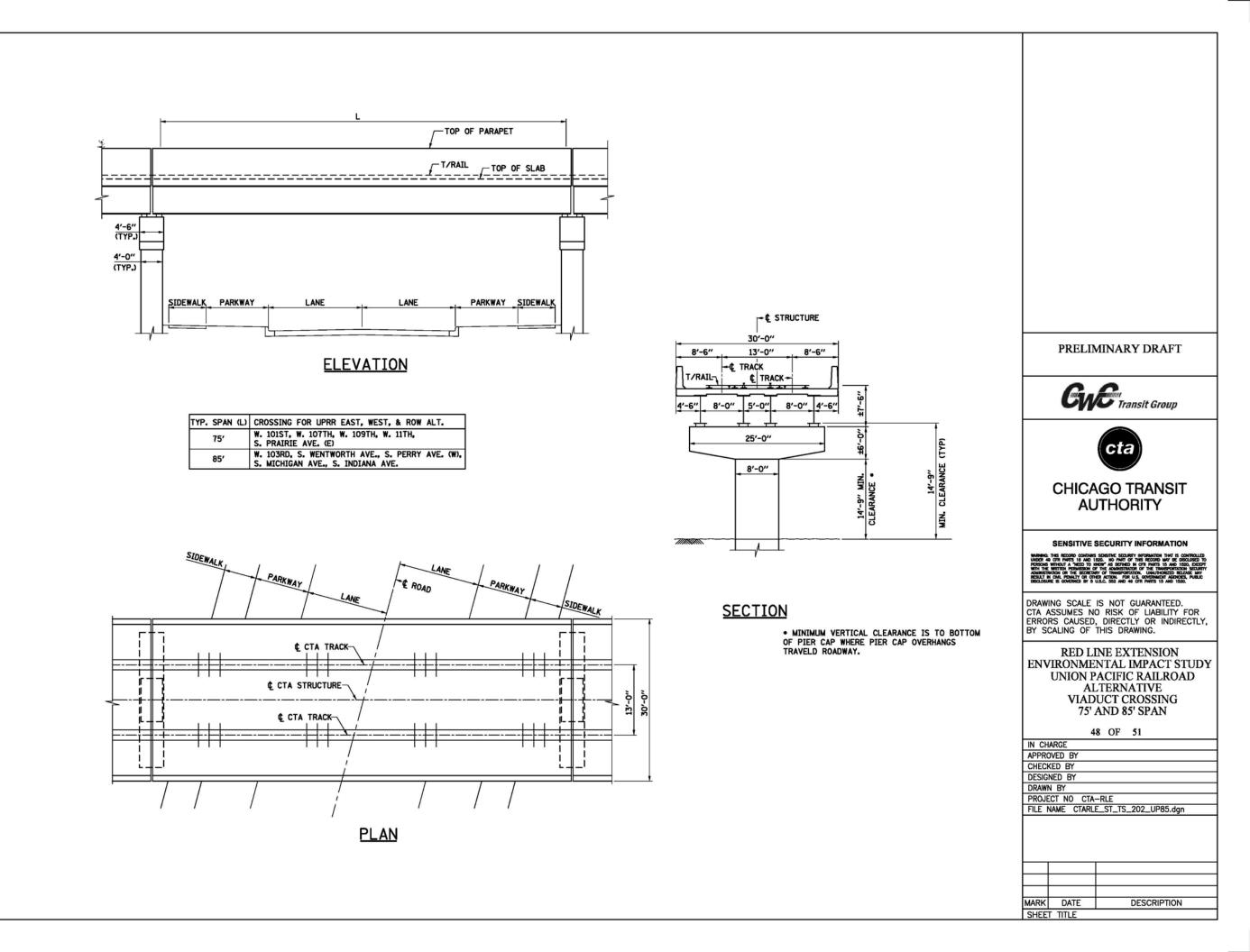


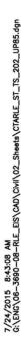
PRELIN	AINARY DRAFT
C.	<b>C</b>
CW	Transit Group
	cta
	GO TRANSIT
SENSITIVE	
WARNING: THIS RECORD CONTAINS UNDER 49 CFR PARTS 13 AND 12 PERSONS WITHOUT A "NEED TO K WITH THE WRITTEN PERMISSION OF	SENSITIVE SECURITY INFORMATION THAT IS CONTROLLED 20. NO PART OF THIS RECORD MAY BE DISCLOSED TO NOW AS DEFINED IN OFF PARTS 15 AND 1520, EXCEPT THE ADMINISTRATOR OF THE TRANSPORTATION SECURITY TO AS TRANSPORTATION.
RESULT IN CML PENALTY OR OTH DISCLOSURE IS GOVERNED BY 5 (	7 THE ADMINISTRATOR OF THE TRANSPORTATION SECURITY FOR FTANSPORTATION, UNUMPOREZED RELASE MAY EN ACTION, FOR U.S. GOVERNMENT ADENCES, PUBLIC LS.C. 352 AND 40 GFR PARTS 15 AND 1520.
DRAWING SCALE	IS NOT GUARANTEED. D RISK OF LIABILITY FOR
DRAWING SCALE CTA ASSUMES NO ERRORS CAUSED, BY SCALING OF	D RISK OF LIABILITY FOR DIRECTLY OR INDIRECTLY,
CTA ASSUMES NO ERRORS CAUSED, BY SCALING OF RED LI	N RISK OF LIABILITY FOR DIRECTLY OR INDIRECTLY, THIS DRAWING.
CTA ASSUMES NO ERRORS CAUSED BY SCALING OF RED LI ENVIRONME	NE EXTENSION NTAL IMPACT STUDY
CTA ASSUMES NO ERRORS CAUSED BY SCALING OF RED LI ENVIRONME ALL A	N RISK OF LIABILITY FOR DIRECTLY OR INDIRECTLY, THIS DRAWING.
CTA ASSUMES NO ERRORS CAUSED, BY SCALING OF RED LI ENVIRONME ALL A	D RISK OF LIABILITY FOR DIRECTLY OR INDIRECTLY, THIS DRAWING. NE EXTENSION NTAL IMPACT STUDY LTERNATIVES
CTA ASSUMES NO ERRORS CAUSED, BY SCALING OF RED LI ENVIRONME ALL A SECTIONS 46	D RISK OF LIABILITY FOR DIRECTLY OR INDIRECTLY, THIS DRAWING. NE EXTENSION NTAL IMPACT STUDY LTERNATIVES
CTA ASSUMES NO ERRORS CAUSED, BY SCALING OF RED LI ENVIRONME ALL A SECTIONS 46 IN CHARGE APPROVED BY	N RISK OF LIABILITY FOR DIRECTLY OR INDIRECTLY, THIS DRAWING. NE EXTENSION NTAL IMPACT STUDY LTERNATIVES SOUTH OF 95TH ST.
CTA ASSUMES NO ERRORS CAUSED, BY SCALING OF RED LI ENVIRONME ALL A SECTIONS 46 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY	N RISK OF LIABILITY FOR DIRECTLY OR INDIRECTLY, THIS DRAWING. NE EXTENSION NTAL IMPACT STUDY LTERNATIVES SOUTH OF 95TH ST.
CTA ASSUMES NO ERRORS CAUSED, BY SCALING OF RED LI ENVIRONME ALL A SECTIONS 46 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-	ORISK OF LIABILITY FOR DIRECTLY OR INDIRECTLY, THIS DRAWING. NE EXTENSION NTAL IMPACT STUDY LTERNATIVES SOUTH OF 95TH ST. OF 51
CTA ASSUMES NO ERRORS CAUSED, BY SCALING OF RED LI ENVIRONME ALL A SECTIONS 46 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-	ORISK OF LIABILITY FOR DIRECTLY OR INDIRECTLY, THIS DRAWING. NE EXTENSION NTAL IMPACT STUDY LTERNATIVES SOUTH OF 95TH ST. OF 51
CTA ASSUMES NO ERRORS CAUSED, BY SCALING OF RED LI ENVIRONME ALL A SECTIONS 46 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-	ORISK OF LIABILITY FOR DIRECTLY OR INDIRECTLY, THIS DRAWING. NE EXTENSION NTAL IMPACT STUDY LTERNATIVES SOUTH OF 95TH ST. OF 51
CTA ASSUMES NO ERRORS CAUSED, BY SCALING OF RED LI ENVIRONME ALL A SECTIONS 46 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-	ORISK OF LIABILITY FOR DIRECTLY OR INDIRECTLY, THIS DRAWING. NE EXTENSION NTAL IMPACT STUDY LTERNATIVES SOUTH OF 95TH ST. OF 51
CTA ASSUMES NO ERRORS CAUSED, BY SCALING OF RED LI ENVIRONME ALL A SECTIONS 46 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-	ORISK OF LIABILITY FOR DIRECTLY OR INDIRECTLY, THIS DRAWING. NE EXTENSION NTAL IMPACT STUDY LTERNATIVES SOUTH OF 95TH ST. OF 51
CTA ASSUMES NO ERRORS CAUSED, BY SCALING OF RED LI ENVIRONME ALL A SECTIONS 46 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-	ORISK OF LIABILITY FOR DIRECTLY OR INDIRECTLY, THIS DRAWING. NE EXTENSION NTAL IMPACT STUDY LTERNATIVES SOUTH OF 95TH ST. OF 51

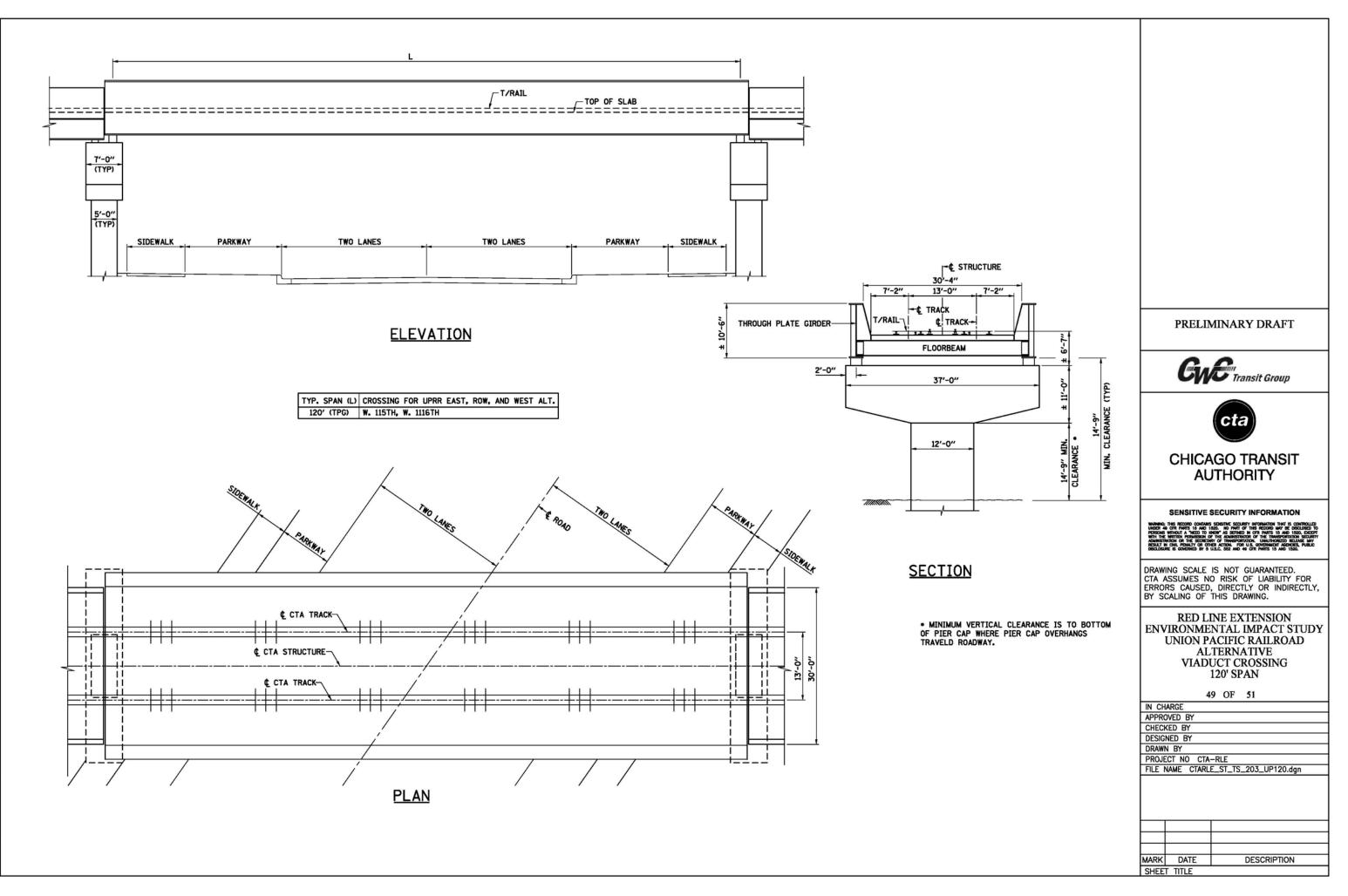


7/24/2015 8:43:07 AM G:\ENG\06-3690-08-RLE\_EIS\CAD\Civi\02\_Sheets\CTARLE\_ST\_TS\_201\_UPCLEAR.

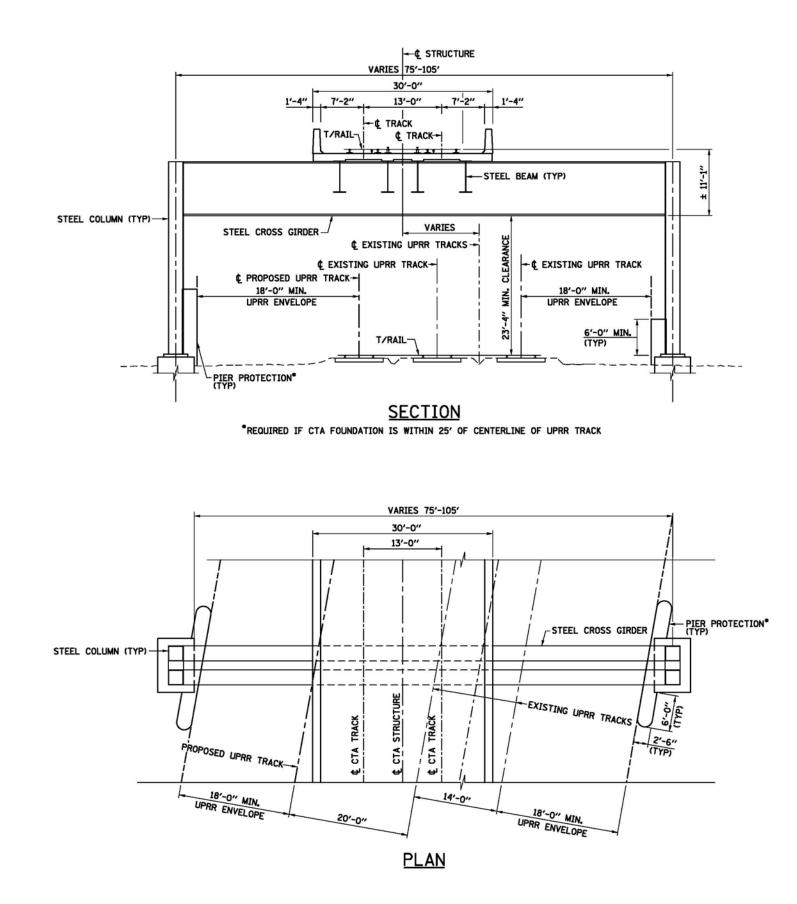
PRELI	MINARY DRAFT
0	0
GA	Firansit Group
	- mansh Group
	(Cta)
	•
CHIC	AGO TRANSIT
	JTHORITY
SENSITIVE	SECURITY INFORMATION
WARNING: THIS RECORD CONTAIN	S SEGURITY INFORMATION THAT IS CONTROLLED
UNDER 49 CFR PARTS 15 AND PERSONS WITHOUT A "NEED TO	1320. HO PART OF THIS RECORD MAY BE DISCLOSED TO INNOW" AS DEFINED IN CRR PARTS 15 AND 1220, DICEPT
ADMINISTRATION OR THE SECRET RESULT IN CML PENALTY OR OT	OF THE ADMINISTRATOR OF THE TRANSPORTATION SECURITY NEY OF TRANSPORTATION, UNAUTHORIZED RELEASE MAY HER ACTION, FOR U.S. GOVERNMONT ADDICES, FUBLIC U.S.C. 552 AND 40 CFR PARTS 15 AND 1520.
DESCLUSIVE IS GOVERNED BY 5	USLE 302 MD 40 CHR PHRIS 15 MD 1520.
DRAWING SCALE	IS NOT GUARANTEED.
CTA ASSUMES N	O RISK OF LIABILITY FOR D, DIRECTLY OR INDIRECTLY,
ERRORS CAUSED BY SCALING OF	D, DIRECTLY OR INDIRECTLY,
BT SCALING UP	THIS DRAWING.
REDL	INE EXTENSION
ENVIRONME	ENTAL IMPACT STUDY
UNION P	ACIFIC RAILROAD
	TERNATIVE
C	LEARANCE
	17 OF 51
IN CHARGE APPROVED BY	
CHECKED BY	
DESIGNED BY	
DRAWN BY	
PROJECT NO CTA	
FILE NAME CTARL	.E_ST_TS_201_UPCLEAR.dgn
	DESCRIPTION
MARK DATE SHEET TITLE	DESCRIPTION





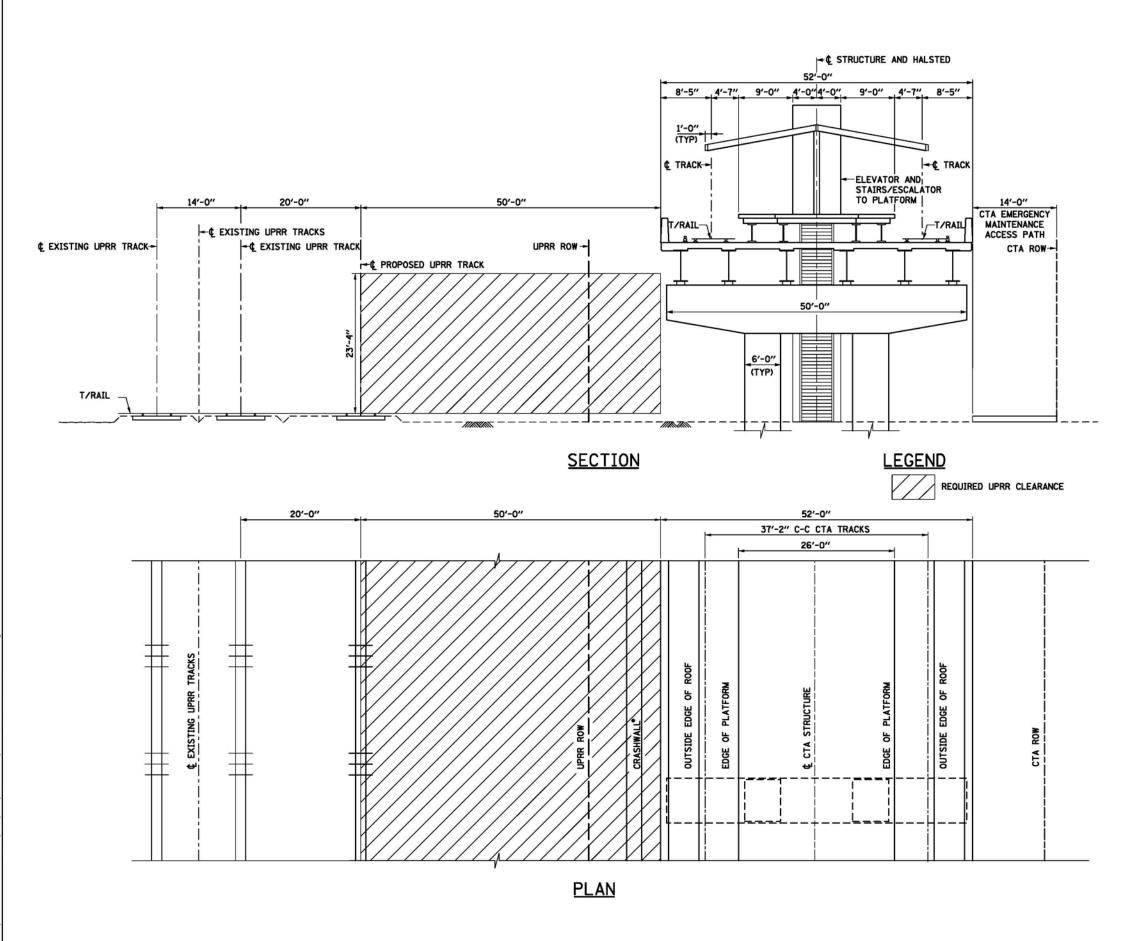


7/24/2015 8:43:10 AM 6:\ENG\06-3690-08-RLE\_EIS\CAD\Civil\02\_Sheets\CTARLE\_ST\_TS\_203\_UP





PRELI	MINARY DRAFT
C.	0
CN	<b>G</b> Transit Group
	cta
СНІСА	GO TRANSIT
	JTHORITY
SENSITIVE	SECURITY INFORMATION
WARNING: THIS RECORD CONTAINS UNDER 49 CFR PARTS 15 AND PERSONS WITHOUT A "NEED TO	S SDISITIVE SECURITY INFORMATION THAT IS CONTROLLED 1320. NO PART OF THIS RECORD MAY BE DISCLOSED TO 000M AS DEDINED IN GFT PARTS 15 AND 1320. EXCEPT
WARNING: THIS RECORD CONTAINS UNDER 49 CFR PARTS 15 AND PERSONS WITHOUT A "NEED TO	SECURITY INFORMATION S DEALING SCORPT WINGHAM THAT IS COMPOLID SCOL NO AND THIS RECORD WITH CONCIDENT NOW AS DEMAD IN OWNERS IS AND ISSO. CREEP WITH ACTON. UNKNOWNERS RELING WAY HER ACTON. FOR ULS OWNERS DELING WAY HER ACTON. FOR ULS OWNERS THE DISSO.
WARNING, THE RECORD CONTAIN UNDER 49 CPR PARTS 12 AND PERSONS WITHOUT A VIEZD TO WITH THE WRITEDN PERNISSION C ADMINISTRATION OR THE SECRET RESULT IN CALL POWALTY OR OT DESCLOSURE IS GOVERNED BY 5	S SDETINE SECRETY INTORNATION THAT IS CONTROLLD SSE. NO NATT OF THE RECORD MAY BE DESCRETE TO THE ADMENDIATION OF THE INTERPORTENTIAL SECRET IF OF THREFOREMENTS, LUMIPORED RELACE MAY WE ACTIVE TO A US OUTPOINTION TO ADDRESS, PUBLIC LISE, SSE AND 40 GR THETH 15 AND 1555.
WARNER THE RECORD CONTAM UNDER 49 CFR WHITE 19 AND PERSONE WHITEN MEDIATION WHITTHE WHITEN MEDIATION WHITTHE WHITEN MEDIATION RESULT AND AND AND AND AND DECLOSURE IS GOVERNED BY S DRAWING SCALE CTA ASSUMPES N	
WARNE THE RECORD CONTAM UNDER 49 GTR MIRTS 15 AND PERSONS WITHOUT A NEED TO WITH THE WITTEN PERSONS WITH THE WITTEN PERSONS WITHOUT AND THE PERSON PERSONS OF THE PERSONS DECLOSURE IS GOVERNED BY S DRAWING SCALE CTA ASSUMES N	S SPENTIKE SECRATY INFORMATION THAT IS CONTROLLED SEC. TO ANY OF THIS RECORD MAY BE DISCLORED TO MAY AS DEALED IN OTH NATES IS NOT TAKE SECONT WY OF THIS RECORD WITH AN ISSUE WY OF THIS PROFESSION OF THE AND ISSUE USLC SEE AND 44 OFF PARTS IS AND ISSUE ISS NOT GUARANTEED. O RISK OF LIABILITY FOR , DIRECTLY OR INDIRECTLY,
WHENCE THE RECORD CONTAIN UNCOME & OFFINITION OF THE STORED THE THE WHEN A STORED TO AND THE THE WHEN A STORED TO AN ADMINISTRATION OF THE STORET RECLIMENT OF THE STORET RECLIMENT OF THE STORET DRAWING SCALE CTA ASSUMES N ERRORS CAUSED BY SCALING OF RED L	S SHORING SECRETY INFORMATION THAT IS CONTROLLED AND ANY OF THE RECORD BAY BE CONTROLLED THE ANALYSIC SECRET ANY INFORMATION RELATED THE ANALYSIC SECRET ANY INFORMATION RELATION AND ANY OF THE INFORMATION RELATION AND ANY OF THE INFORMATION RELATION IS NOT GUARANTEED. O RISK OF LIABILITY FOR , DIRECTLY OR INDIRECTLY, THIS DRAWING. INE EXTENSION
DRAWING SCALE CTA ASSUMES NA CONTACT AND A CONTACT AND A CONTACT AND A CONTACT AND A CONTACT AND A CONTACT AND A CONTACT AND A CONTACT AND A CONTACT AND A CONTACT AND A CONTACT AND A CONTACT AND A CONTACT AND A CONTACT AND A CONTACT AND A CONTACT AND A CONTACT AND A C	S SHORING SECRETY INFORMATION THAT IS CONTROLLED BASE. NO PART OF THE RECORD BAY IS CONTROLLED THAN AN ANY OF THE RECORD BAY IS CONTROLLED THAN ANY OF THE INFORMATION RELATION IN A CONTROL OF THE INFORMATION RELATION IN A CONTROL OF THE INFORMATION RELATION IN A CONTROL OF THE INFORMATION RELATION IS NOT GUARANTEED. O RISK OF LIABILITY FOR , DIRECTLY OR INDIRECTLY, THIS DRAWING. INE EXTENSION CNTAL IMPACT STUDY
UNDER THE RECORD CONTACT UNDER A CRIMENT A TAXAGE OF THE TAXAGE A TAXAGE A CRIMENT A CRIMENT A CRIMENT A CRIME	S DEFINE SECRETY INTRAUENT THAT IS CONTROLLED BASE. NO PART OF THE RECORD WAY BE CONTROLLED BASE. NO PART OF THE RECORD WAY BE CONTROLLED THE ADDAMENT OF THE INFORMATION RELATION WAY A DEPARTMENT OF THE INFORMATION RELATION INTECTLY OR INDIRECTLY, THIS DRAWING. INTELEXTENSION INTAL IMPACT STUDY ACIFIC RAILROAD TERNATIVE
UNDER THE RECORD CONTACT UNDER A CRIMENT A TAXAGE OF THE TAXAGE A TAXAGE A CRIMENT A CRIMENT A CRIMENT A CRIME	S DEFINE SCARY INFOMUMENT THAT IS CONTROLLED BASE. NO PART OF THE RECORD BAY IS CONTROLLED THAN ALLED THE ACCOUNT AND A CONTROL TO THE ACCOUNT OF THAN ALLED THE ACCOUNT OF THE INFORMATION RELATION AND ALLED THAN AND A CONTROL TO RELATE AND ALLED THAN AND A CONTROL TO RELATE AND ALLED THAN AND A CONTROL TO RELATE AND ALLED THAN AND A CONTROL TO RELATE IS NOT GUARANTEED. O RISK OF LIABILITY FOR , DIRECTLY OR INDIRECTLY, THIS DRAWING. INE EXTENSION CNTAL IMPACT STUDY ACIFIC RAILROAD
UNDER THE RECORD CONTACT UNDER 40 CFI PARTS 10 AND PREVENT WITCH A TAKED OF A CARACTERISTICS A TAKED OF A CARACTERISTICS A TAKED OF REALT IN CARACTERISTICS A CARACTERISTIC DRAWING SCALE CTA ASSUMES N ERRORS CAUSED BY SCALING OF RED L ENVIRONME UNION P AL THRU	A SOUTH A SCARY WITH THE CONTROL D ASS. AN OWNER OF THE RECORD WAY IS CONTROLLED ASS. AN OWNER OF THE RECORD WAY IS CONTROL TO ANY AS DEMAND IN ON MATTIN IS AND TAKE. THE ACTURY ON INFORMATING AND THE CONTROL WAY BY ACTURY ON IS ANY AND AND THE CONTROL WAY BY ACTURY ON IS ANY AND AND THE CONTROL WAY BY ACTURY ON IN A OWNER TO AND THE CONTROL IN CONTROL OWNER OF THE AND THE CONTROL O RISK OF LIABILITY FOR , DIRECTLY OR INDIRECTLY, THIS DRAWING. INE EXTENSION INTAL IMPACT STUDY ACIFIC RAILROAD TERNATIVE BENT SECTION
UNDER THE RECORD CONTACT UNDER 40 GPH PARES 10 AND PREVENT HILL IS	S DEFINE SECRETY INTRAUENT THAT IS CONTROLLED BASE. NO PART OF THE RECORD WAY BE CONTROLLED BASE. NO PART OF THE RECORD WAY BE CONTROLLED THE ADDAMENT OF THE INFORMATION RELATION WAY A DEPARTMENT OF THE INFORMATION RELATION INTECTLY OR INDIRECTLY, THIS DRAWING. INTELEXTENSION INTAL IMPACT STUDY ACIFIC RAILROAD TERNATIVE
UNDER THE RECORD CONTACT UNDER 40 CFI PARTS 10 AND PREVENT WITH A THE 10 AND PREVENT WITH A THE 10 AND PREVENT WITH A THE INFORMATION RECORDER 5 A CONTACT A THE INFORMATION OF A CONTACT A THE INFORMATION A THE INFORMATION RECORDER 5 A CONTACT A THE INFORMATION A THE BROWN A CONTACT A THE INFORMATION A THE INFORMATION A THE BROWN A CONTACT A THE INFORMATION A THE INFORMATION A THE BROWN A CONTACT A THE INFORMATION A THE INFORMATION A THE INFORMATION A THE BROWN A CONTACT A THE INFORMATION A THE INFOR	A SOUTH A SCARY WITH THE CONTROL D ASS. AN OWNER OF THE RECORD WAY IS CONTROLLED ASS. AN OWNER OF THE RECORD WAY IS CONTROL TO ANY AS DEMAND IN ON MATTIN IS AND TAKE. THE ACTURY ON INFORMATING AND THE CONTROL WAY BY ACTURY ON IS ANY AND AND THE CONTROL WAY BY ACTURY ON IS ANY AND AND THE CONTROL WAY BY ACTURY ON IN A OWNER TO AND THE CONTROL IN CONTROL OWNER OF THE AND THE CONTROL O RISK OF LIABILITY FOR , DIRECTLY OR INDIRECTLY, THIS DRAWING. INE EXTENSION INTAL IMPACT STUDY ACIFIC RAILROAD TERNATIVE BENT SECTION
DRAWING SCALE CTA ASSUMES NO CONTACT DRAWING SCALE CTA ASSUMES N ERRORS CAUSED BY SCALING OF RED LI ENVIRONME UNION P ALL THRU S IN CHARGE APPROVED BY CHECKED BY DESIGNED BY	A SOUTH A SCARY WITH THE CONTROL D ASS. AN OWNER OF THE RECORD WAY IS CONTROLLED ASS. AN OWNER OF THE RECORD WAY IS CONTROL TO ANY AS DEMAND IN ON MATTIN IS AND TAKE. THE ACTURY ON INFORMATING AND THE CONTROL WAY BY ACTURY ON IS ANY AND AND THE CONTROL WAY BY ACTURY ON IS ANY AND AND THE CONTROL WAY BY ACTURY ON IN A OWNER TO AND THE CONTROL IN CONTROL OWNER OF THE AND THE CONTROL O RISK OF LIABILITY FOR , DIRECTLY OR INDIRECTLY, THIS DRAWING. INE EXTENSION INTAL IMPACT STUDY ACIFIC RAILROAD TERNATIVE BENT SECTION
UNDERSE THE RECORD CONTACT UNDERSE THE RECORD CONTACT UNDERSE THE RECORD CONTACT THE RECORD OF THE RECORD RECORDER OF THE RECORD ORAWING SCALE CTA ASSUMES N ERRORS CAUSED BY SCALING OF RED L ENVIRONME UNION P AL THRU S IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DESIGNED BY PROJECT NO CTA	S DEFINE SECRETY INTRAJUENT THAT IS CONTROLLED BASE. NO PART OF THE RECORD WAY BE CONTROLLED BASE. NO PART OF THE RECORD WAY BE CONTROLLED WAY AS DEPENDENT OF THE THE AND THE CONTROL THE WAY AS DEPENDENT OF THE THE AND THE CONTROL WAY WAY AND AND AND AND AND THE CONTROL WAY WAY AND AND AND AND AND AND THE AND THE IS NOT GUARANTEED. O RISK OF LIABILITY FOR , DIRECTLY OR INDIRECTLY, THIS DRAWING. INE EXTENSION CONTAL IMPACT STUDY ACIFIC RAILROAD TERNATIVE BENT SECTION 50 OF 51 -RLE
UNDERSE THE RECORD CONTACT UNDERSE THE RECORD CONTACT UNDERSE THE RECORD CONTACT THE RECORD OF THE RECORD RECORDER OF THE RECORD ORAWING SCALE CTA ASSUMES N ERRORS CAUSED BY SCALING OF RED L ENVIRONME UNION P AL THRU S IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DESIGNED BY PROJECT NO CTA	S DEFINE SECRETY INTRAJUENT THAT IS CONTROLLED BASE. NO PART OF THE RECORD WAY BE CONTROLLED WAY A CONTROL OF THE INFORMATION INTRADUCED TO THE ACTION OF THE INFORMATION INTRADE WAY WAY A CONTROL OF THE INFORMATION INTRADE WAY INFORMATION OF THE INFORMATION OF THE INFORMATION OF INFORMATION OF THE INFORMATION OF THE INFORMATION INFORMATION OF THE INFORMATION OF THE INFORMATION OF THE INFORMATION INFORMATION OF THE INFORMATION OF THE INFORMATION OF THE INFORMATION INFORMATION OF THE INFORMATION OF THE INFORMATION OF THE INFORMATION INFORMATION OF THE INFORMATION OF THE INFORMATI
UNDERSE THE RECORD CONTACT UNDERSE THE RECORD CONTACT UNDERSE THE RECORD CONTACT THE RECORD OF THE RECORD RECORDER OF THE RECORD ORAWING SCALE CTA ASSUMES N ERRORS CAUSED BY SCALING OF RED L ENVIRONME UNION P AL THRU S IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DESIGNED BY PROJECT NO CTA	S DEFINE SECRETY INTRAJUENT THAT IS CONTROLLED BASE. NO PART OF THE RECORD WAY BE CONTROLLED BASE. NO PART OF THE RECORD WAY BE CONTROLLED WAY AS DEPENDENT OF THE THE AND THE CONTROL THE WAY AS DEPENDENT OF THE THE AND THE CONTROL WAY WAY AND AND AND AND AND THE CONTROL WAY WAY AND AND AND AND AND AND THE AND THE IS NOT GUARANTEED. O RISK OF LIABILITY FOR , DIRECTLY OR INDIRECTLY, THIS DRAWING. INE EXTENSION CONTAL IMPACT STUDY ACIFIC RAILROAD TERNATIVE BENT SECTION 50 OF 51 -RLE
UNDERSE THE RECORD CONTACT UNDERSE THE RECORD CONTACT UNDERSE THE RECORD CONTACT THE RECORD OF THE RECORD RECORDER OF THE RECORD ORAWING SCALE CTA ASSUMES N ERRORS CAUSED BY SCALING OF RED L ENVIRONME UNION P AL THRU S IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DESIGNED BY PROJECT NO CTA	S DEFINE SECRETY INTRAJUENT THAT IS CONTROLLED BASE. NO PART OF THE RECORD WAY BE CONTROLLED BASE. NO PART OF THE RECORD WAY BE CONTROLLED WAY AS DEPENDENT OF THE THE AND THE CONTROL THE WAY AS DEPENDENT OF THE THE AND THE CONTROL WAY WAY THE CONTROL WAY AND THE CONTROL WAY WAY AND AND AND AND AND THE CONTROL WAY WAY AND AND AND AND AND AND THE AND THE IS NOT GUARANTEED. O RISK OF LIABILITY FOR , DIRECTLY OR INDIRECTLY, THIS DRAWING. INE EXTENSION CONTAL IMPACT STUDY ACIFIC RAILROAD TERNATIVE BENT SECTION 50 OF 51 -RLE
UNDERSE THE RECORD CONTACT UNDERSE THE RECORD CONTACT UNDERSE THE RECORD CONTACT THE RECORD OF THE RECORD RECORDER OF THE RECORD ORAWING SCALE CTA ASSUMES N ERRORS CAUSED BY SCALING OF RED L ENVIRONME UNION P AL THRU S IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DESIGNED BY PROJECT NO CTA	S DEFINE SECRETY INTRAJUENT THAT IS CONTROLLED BASE. NO PART OF THE RECORD WAY BE CONTROLLED BASE. NO PART OF THE RECORD WAY BE CONTROLLED WAY AS DEPENDENT OF THE THE AND THE CONTROL THE WAY AS DEPENDENT OF THE THE AND THE CONTROL WAY WAY THE CONTROL WAY AND THE CONTROL WAY WAY AND AND AND AND AND THE CONTROL WAY WAY AND AND AND AND AND AND THE AND THE IS NOT GUARANTEED. O RISK OF LIABILITY FOR , DIRECTLY OR INDIRECTLY, THIS DRAWING. INE EXTENSION CONTAL IMPACT STUDY ACIFIC RAILROAD TERNATIVE BENT SECTION 50 OF 51 -RLE
DRAWING SCALE CTA ASSUMES NO BECLARE & CONTACT AND A DRAWING SCALE CTA ASSUMES N ERRORS CAUSED BY SCALING OF RED L' ENVIRONME UNION P ALL THRU 5 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DESIGNED BY DRAWN BY PROJECT NO CTA FILE NAME CTARL	Section Storey information that is controling and the part of the income were concluded to the Advertised of the induction were concluded to the Advertised of the induction were and its not guaranteed. IS NOT GUARANTEED. O RISK OF LIABILITY FOR , DIRECTLY OR INDIRECTLY, THIS DRAWING. INE EXTENSION CNTAL IMPACT STUDY ACIFIC RAILROAD TERNATIVE BENT SECTION 50 OF 51
UNIONS THE RECORD CONTACT UNIONS AND THE RECORD CONTACT UNIONS AND THE RECORD THE ADDRESS AND THE RECORD THE ADDRESS AND THE RECORD RECORD A DECORD A DECORD RECORD A DECORD A DECORD BY SCALING OF RED L' ENVIRONME UNION P ALL THRU SCALING OF ALL THRU SCALING OF CHECKED BY CHECKED BY DESIGNED BY PROJECT NO CTA	S DEFINE SECRETY INTRAJUENT THAT IS CONTROLLED BASE. NO PART OF THE RECORD WAY BE CONTROLLED BASE. NO PART OF THE RECORD WAY BE CONTROLLED WAY AS DEPENDENT OF THE THE AND THE CONTROL THE WAY AS DEPENDENT OF THE THE AND THE CONTROL WAY WAY THE CONTROL WAY AND THE CONTROL WAY WAY AND AND AND AND AND THE CONTROL WAY WAY AND AND AND AND AND AND THE AND THE IS NOT GUARANTEED. O RISK OF LIABILITY FOR , DIRECTLY OR INDIRECTLY, THIS DRAWING. INE EXTENSION CONTAL IMPACT STUDY ACIFIC RAILROAD TERNATIVE BENT SECTION 50 OF 51 -RLE



7/24/2015 8:43:12 AM 6:\ENC\06-3690-08-RLE\_EIS\CAD\Civil\02\_Sheets\CTARLE\_ST\_TS\_205\_UPSTCU

DDELDA	NARY DRAFT
FRELIVIT	NAKI DRAFI
0.0	2
CWZ	Transit Group
	cta
CHICAG	O TRANSIT
A 1 17	
AUI	HORITY
	HORITY
SENSITIVE SEC WAREA THE RECORD CONTAME SOM WEEKS WITH THE AND THE OF PERSON WITHOUT A YEEK TO RECORD	
SENSITIVE SEC WARKS THE RECORD GALLARE SOL UNDER STOLEN AND A SECTION OF THE WITH THE WITH THE WITH THE ADMINISTRATION OF THE RECHERNING OF BECIDENCE & CONSIDER 9 SULLA	HORITY SURITY INFORMATION THE SCORPY INFORMATION HAT IS CONTROLLED IN ANT OF THE SCORE WAY BE CONTROLLED IN ADDITIONATION HAT IS AND ISOL CREET MANIFERRITOR IN THE INFORMATION SECURITY ADDITIONAL TO U.S. CONTROLLED HEAD AND TOOL FOR U.S. CONTROLLED HEAD AND ISOL
SENSITIVE SEC www.exc.ms.accomp.contrast som under au orn news to two tack min the minute Analyzing of the Advancementation on the screectory of second to the screectory of the advancementation of the screectory of second to the screectory of the advancementation of the screectory of second to the screectory of the second to the screectory of the second to the screectory of the second to the screectory of the DRAWING SCALE IS	HORITY CURITY INFORMATION MAY ADDRESS AND AND AND AND AND AND AND MAY ADDRESS AND AND AND AND AND AND MAY AND
SENSITIVE SEC www.exc.ms.rccom.com/ans.som under au crim www.site.wo.rccom. methods.wite.com/ans.som com/ans.som/ans.ans.som com/ans.som/ans.som sectors.com/ans.som beccome is owned in 5 July DRAWING SCALE IS	HORITY CURITY INFORMATION INF
SENSITIVE SEC WARDEN THE RECORD CONTANT SET UNDER STATES RECORD CONTANT SET UNDER STATES RESERVANCE WITH THE WITH RESERVANCE OF THE ADMINISTRATIC CONTANT RESERVANCE DESCRIPTION OF THE SECONDARY OF DESCRIPTION OF THE SECONDARY DESCRIPTION OF THE SECONDARY DESCRIPTION OF THE SECONDARY DESCRIPTION OF THE SECONDARY OF THE SECONDARY OF THE SECONDARY OF THE SECONDARY OF THE SECONDARY OF THE SECONDARY OF THE SECONDARY OF THE SECONDARY OF THE SECONDARY OF THE	HORITY CURITY INFORMATION INFO ADDRESS MORE AND ADDRESS MORE ADDRESS
SENSITIVE SEC WAREN THE RECORD CONTANT SCH UNDER STRUCTURE THE RECORD OF THE RECORD STRUCTURE ADDISON THE THE WEIGHT OF THE RECORD OF THE ADDISON THE RECORD OF THE SCHWART OF BECCARE & CONTROL OF THE RECORD THE RECORD OF THE RECORD OF THE ERRORS CAUSED, D BY SCALING OF THE RECORD LINI ENVIRONMENT UNION PAC	HORITY CURITY INFORMATION THE STOREY INFORMATION TH
SENSITIVE SEC WARKS THE RECORD CONTANT SOM UNCOME OF THE RECORD CONTANT SOM UNCOME OF THE RECORD CONTANT ADMINISTRATION OF THE SCREWAY OF RECORD AND ADDINATION OF THE SCREWAY RECORD AND ADDINATION OF THE SCREWAY DRAWING SCALE IS CTA ASSUMES NO F ERRORS CAUSED, D BY SCALING OF THI RED LINI ENVIRONMENT UNION PAC ALTE	HORITY CURITY INFORMATION THE STOREY INFORMATION THE STOREY INFORMATION THE STOREY INFORMATION THE STOREY INFORMATION THE STOREY INFORMATION THE STOREY INFORMATION STOREY INFORMATION STOREY ST
SENSITIVE SEC WARKS THE RECORD CONTANT SOM UNCOME OF THE RECORD CONTANT SOM UNCOME OF THE RECORD CONTANT ADMINISTRATION OF THE SCREWAY OF RECORD AND ADDINATION OF THE SCREWAY RECORD AND ADDINATION OF THE SCREWAY DRAWING SCALE IS CTA ASSUMES NO F ERRORS CAUSED, D BY SCALING OF THI RED LINI ENVIRONMENT UNION PAC ALTE	HORITY CURITY INFORMATION THE STOREY INFORMATION THE STOREY INFORMATION THAT IS CONTROLLED TO ANY OF THE STORE OF ANY OF THE STORE AND TO A SOMED A ONE OF THE STORE AND ADDR THE STOREY INFORMATION OF ANY OF THE STORE STORE AND A STOREY AND ADDR STORE AND A STOREY AND ADDR STOREY AND A STOREY AND ADDR STOREY AND A STOREY AND ADDR STOREY AND A STOREY STOREY AND A STOREY ST
SENSITIVE SEC WARKS THE RECORD CONTANT SOM UNCOME & OFF HAVE DI AND TACT THE TRE WARKS THE STOREGAT AND THE STOREGAT STOREGATION OF THE STOREGATION OF THE STOR	HORITY CURITY INFORMATION THE STOREY INFORMATION THE STOREY INFORMATION THAT IS CONTROLLED TO ANY OF THE STORE OF ANY OF THE STORE AND TO A SOMED A ONE OF THE STORE AND ADDR THE STOREY INFORMATION OF ANY OF THE STORE STORE AND A STOREY AND ADDR STORE AND A STOREY AND ADDR STOREY AND A STOREY AND ADDR STOREY AND A STOREY AND ADDR STOREY AND A STOREY STOREY AND A STOREY ST
SENSITIVE SEC WHERE AN ADDRESS WHEAT A ADDRESS MERCINE WHEAT A ADDRESS MERCINE WHEAT A ADDRESS MERCINE WEAT A ADDRESS DRAWING SCALE IS CTA ASSUMES NO F ERRORS CAUSED, D BY SCALING OF THI RED LINI ENVIRONMENT UNION PAC ALTE TYPICAL ST 51 IN CHARGE APPROVED BY	HORITY CURITY INFORMATION THE SCHEDY INFORMATION THE SCHEDY INFORMATION THAT IS CONTROLLED THE SCHEDY INFORMATION THAT IS CONTROLLED THE SCHEDY INFORMATION OF THE SCHEDY THE SCHEDY INFORMATION OF THE SCHEDY THE SCHEDY INFORMATION OF THE SCHEDY SCHEDY INFORMATION OF THE SCHEDY SCHEDY INFORMATION OF THE SCHEDY INFORMATION SCHEDY INFORMATION TAL IMPACT STUDY INFIC RAILROAD RNATIVE ATION SECTION
SENSITIVE SEC WHEN IT IS INCOME ON ANY SEC VERSION WHICH A VERSION OF THE ANY THE WITH A SECTION OF THE ANY THE WITH A SECTION OF THE ANY THE ANY ANY ANY ANY ANY ANY ANY ANY ANY DECLEMENT OF ANY ANY ANY ANY ANY ANY ANY ANY DECLEMENT OF ANY ANY ANY ANY ANY ANY ANY ANY ANY DECLEMENT OF ANY	HORITY CURITY INFORMATION THE SCHEDY INFORMATION THE SCHEDY INFORMATION THAT IS CONTROLLED THE SCHEDY INFORMATION THAT IS CONTROLLED THE SCHEDY INFORMATION OF THE SCHEDY THE SCHEDY INFORMATION OF THE SCHEDY THE SCHEDY INFORMATION OF THE SCHEDY SCHEDY INFORMATION OF THE SCHEDY SCHEDY INFORMATION OF THE SCHEDY INFORMATION SCHEDY INFORMATION TAL IMPACT STUDY INFIC RAILROAD RNATIVE ATION SECTION
SENSITIVE SEC WARKEN THE RECORD CONTANT SOM UNDER STRUCTURE THE RECORD OF THE RECORD WARKEN AND THE RECORD OF THE RECORD OF THE RECORD OF THE ADMENTIME OF THE RECORD OF THE STRUCTURE THE THE WARKEN ADMENTION OF THE RECORD OF THE STRUCTURE DRAWING SCALE IS CTA ASSUMES NO F ERRORS CAUSED, D BY SCALING OF THI RED LINI ENVIRONMENTION PAC ALTE TYPICAL ST 51 IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RE	HORITY UNITY INFORMATION THE STOREY INFORMATION THE STOREY INFORMATION THE STOREY INFORMATION THAT IS CONTROLLED TO ALL CONTROL TO ALL ADDRESS BELAGE MAY THE TO AND ADDRESS BELAGE MAY THE STOREY AND ADDRESS BELAGE MAY THE STOREY AND ADDRESS BELAGE MAY THE TO ADDRESS B
SENSITIVE SEC WARKEN THE RECORD CONTANT SOM UNDER STRUCTURE THE RECORD OF THE RECORD WARKEN AND THE RECORD OF THE RECORD OF THE ADMENTION OF THE RECORD OF THE SOURCE OF BECILIER & CONTROL OF THE RECORD OF THE ADMENTION OF THE RECORD OF THE ADMENTION OF THE RECORD OF THE RECORD OF TH	HORITY UNITY INFORMATION UNITY
SENSITIVE SEC WARKEN THE RECORD CONTANT SOM UNDER STRUCTURE THE RECORD OF THE RECORD WARKEN AND THE RECORD OF THE RECORD OF THE ADMENTION OF THE RECORD OF THE SOURCE OF BECILIER & CONTROL OF THE RECORD OF THE ADMENTION OF THE RECORD OF THE ADMENTION OF THE RECORD OF THE RECORD OF TH	HORITY UNITY INFORMATION THE STOREY INFORMATION THE STOREY INFORMATION THE STOREY INFORMATION THAT IS CONTROLLED TO ALL CONTROL TO ALL ADDRESS BELAGE MAY THE TO AND ADDRESS BELAGE MAY THE STOREY AND ADDRESS BELAGE MAY THE STOREY AND ADDRESS BELAGE MAY THE TO ADDRESS B
SENSITIVE SEC WARKEN THE RECORD CONTANT SOM UNDER STRUCTURE THE RECORD OF THE RECORD WARKEN AND THE RECORD OF THE RECORD OF THE ADMENTION OF THE RECORD OF THE SOURCE OF BECILIER & CONTROL OF THE RECORD OF THE ADMENTION OF THE RECORD OF THE ADMENTION OF THE RECORD OF THE RECORD OF TH	HORITY UNITY INFORMATION THE STOREY INFORMATION THE STOREY INFORMATION THE STOREY INFORMATION THAT IS CONTROLLED TO ALL CONTROL TO ALL ADDRESS BELAGE MAY THE TO AND ADDRESS BELAGE MAY THE STOREY AND ADDRESS BELAGE MAY THE STOREY AND ADDRESS BELAGE MAY THE TO ADDRESS B
SENSITIVE SEC WARKEN THE RECORD CONTANT SOM UNDER STRUCTURE THE RECORD OF THE RECORD WARKEN AND THE RECORD OF THE RECORD OF THE ADMENTION OF THE RECORD OF THE SOURCE OF BECILIER & CONTROL OF THE RECORD OF THE ADMENTION OF THE RECORD OF THE ADMENTION OF THE RECORD OF THE RECORD OF TH	HORITY UNITY INFORMATION THE STOREY INFORMATION THE STOREY INFORMATION THE STOREY INFORMATION THAT IS CONTROLLED TO ALL CONTROL TO ALL ADDRESS BELAGE MAY THE TO AND ADDRESS BELAGE MAY THE STOREY AND ADDRESS BELAGE MAY THE STOREY AND ADDRESS BELAGE MAY THE TO ADDRESS B
SENSITIVE SEC WARKEN THE RECORD CONTANT SOM UNDER STRUCTURE THE RECORD OF THE RECORD WARKEN AND THE RECORD OF THE RECORD OF THE ADMENTION OF THE RECORD OF THE SOURCE OF BECILIER & CONTROL OF THE RECORD OF THE ADMENTION OF THE RECORD OF THE ADMENTION OF THE RECORD OF THE RECORD OF TH	HORITY UNITY INFORMATION THE STOREY INFORMATION THE STOREY INFORMATION THE STOREY INFORMATION THAT IS CONTROLLED TO ALL CONTROL TO ALL ADDRESS BELAGE MAY THE TO AND ADDRESS BELAGE MAY THE STOREY AND ADDRESS BELAGE MAY THE STOREY AND ADDRESS BELAGE MAY THE TO ADDRESS B



## Memorandum

To:	CTA, Sonali Tandon
From:	CWC - Wight & Company
Date:	January 6, 2015, revised March 16, 2015
Subject:	Evaluation of Conceptual Red Line Extension Alignment

# Purpose

The purpose of this memo is to verify that the conceptual Red Line Extension (RLE) alignment meets certain minimum design criteria and to analyze the potential benefits or drawbacks of different variations of the two Union Pacific Railroad (UPRR) Alignments. Both the East and West Options have been checked for horizontal clearance and curves.

### Verification #1: UPRR Horizontal Clearance

We checked the proposed alignment to verify that it would provide a minimum fifty foot horizontal clearance from the centerline of UPRR's future third track on east side of the UPRR corridor.

The minimum clearance distance is met along the entire East Alignment, with the exception of the following two locations:

- The proposed alignment would shift west to avoid an existing pump station at 104th Street (proposed alignment stationing 166+00 through 182+25). The centerline of UPRR's potential third track and the centerline of the proposed RLE would have a minimum separation of 27.6 feet. If the UPRR East Option is selected, the minimum separation distance would not be met through this segment of the extension. Relocating the existing pump station so the alignment could be adjusted is not feasible. A crash wall could be provided between the UPRR tracks and the proposed extension.
- Farther south from 104th Street, the proposed alignment again shifts west towards the UPRR tracks north of the proposed Michigan Avenue station (near 116th Street, from approximate station 257+00 to 278+00). The centerline of UPRR's potential third track and the centerline of the proposed RLE have a minimum separation of 46.8 feet. More detailed design could likely adjust the proposed alignment through this section to provide the minimum required separation. Note that the proposed alignment in this area is entirely outside the existing UPRR right-of-way.

The minimum clearance distance would be met along the entire West Option except where the CTA facility would cross the UPRR at I-57 and near the Metra Electric District tracks.

## Verification #2: Design Speed at Wendell Smith Park

The proposed alignment from April 2013 had a 15 mph (335-foot radius) curve connecting the East-West portion of the extension (which runs along I-57) to the North-South portion of the extension (which would run along the east edge of the UPRR right-of-way.) The small radius was intended to minimize the impacts the proposed improvement would have on Wendell Smith Park. A curve speed of 15 mph would be a severe constraint on a new rail alignment and has been dropped from further consideration. We checked the possibility of increasing the design speed through this turn to improve future operations. (See the February 27, 2015 memorandum *Documentation of Red Line Extension Curve Speed Discussion* for curve speed criteria.) The attached Exhibit 1 depicts the various design speeds that were analyzed and their corresponding impacts to the park.

Generally, as the design speed increases the impacts on the park would become more severe. A 25 mph (388.80-foot radius) curve could provide a faster operation speed for the CTA with minimal additional impacts on the park compared to the proposed 15 mph curve. Increasing to a 35 mph (730.13-foot radius) curve for the East Option would cause severe impacts to the park, specifically eliminating the ability to use the existing baseball fields. Curves of 45 mph and 55 mph are not recommended with either alignment due to impacts at the park.

# Verification #3: Location of Proposed 103rd St Station

The proposed 103rd Street station would be located one block north of the historic Roseland Pump Station, located at 351 West 104th Street. The Roseland Pump Station provides water to approximately 750,000 residents of the City of Chicago and south suburbs. The minimum separation between the edge of the proposed track structure and the face of the pump station building is 8.8 feet. We checked to see if adjusting the location of the proposed 103rd Street station could increase this spacing. Shifting the station to the north would have no impact on the spacing between the proposed CTA structure and the pump station. The minimum spacing occurs south of the point of tangency of the southernmost curve associated with the 103rd Street station, as shown in the attached Exhibit 2; therefore, shifting the station north has no effect on this minimum spacing. Shifting the station south would decrease this minimum spacing.

The station could be shifted approximately 265 feet north to avoid conflict with the existing communications tower located just west of the intersection of Harvard Avenue and 103rd Place. This shift would require two additional properties to be acquired where the northernmost station curves tie back into the alignment. The distance pedestrians would walk from both the surface lot (at the southwest corner of the UPRR and 103rd Street) and the bus turnaround lot (at the northeast corner to the station platform) would be increased by this

shift. The shift of the 103rd Street station to the north to avoid the tower is not included on the plan sheets as additional residential displacements would be required.

### Verification #4: Illinois Department of Transportation Comments

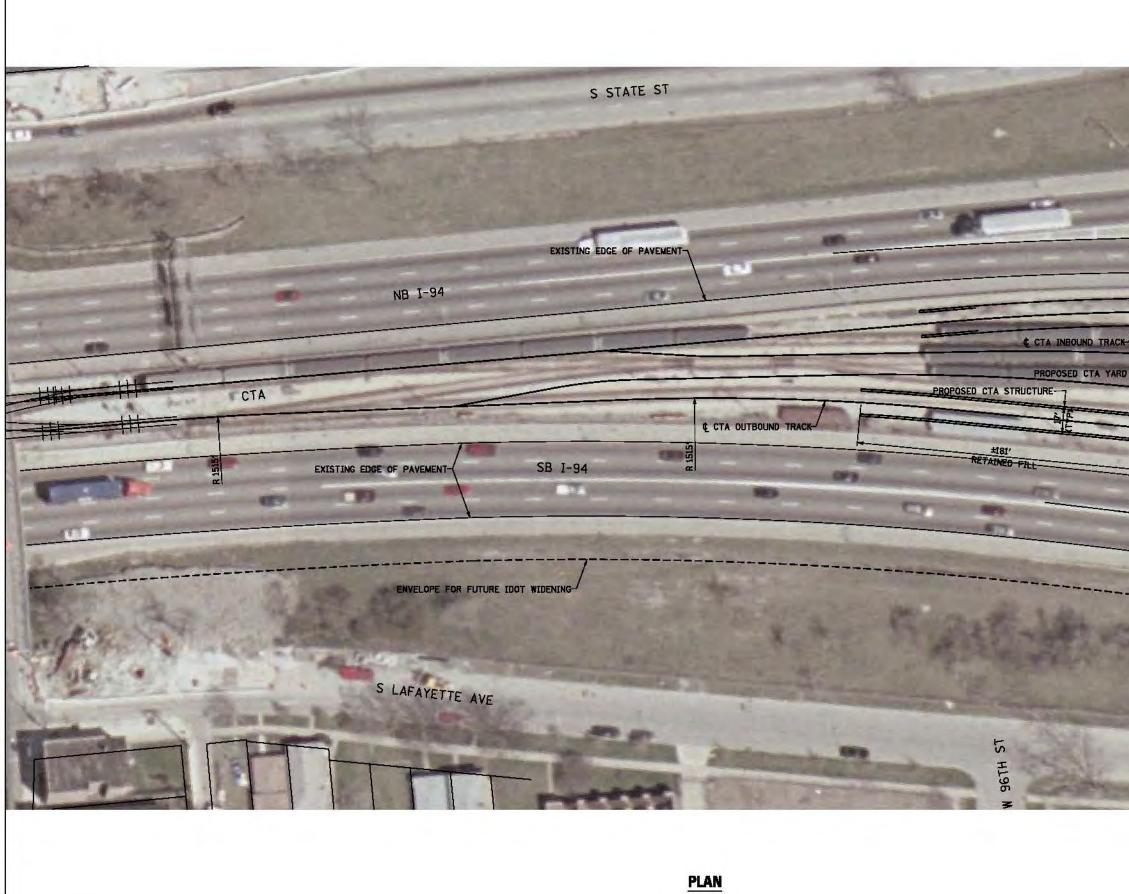
The Illinois Department of Transportation (IDOT) provided several comments via e-mail (attached Exhibit 3) after reviewing the proposed RLE horizontal alignment in the area of I-90/94, I-57, and 95th Street.

IDOT commented that the piers for the proposed RLE structure "should be placed such that the Department has the flexibility to widen our roadways either towards the roadway's median or outward with standard cross-section elements. As such any pier(s) placed in our medians should be placed in the middle of our medians, not immediately behind any existing or proposed barrier walls. Also, any piers proposed on the outside of our roadways should be placed at least 27feet from the existing outside edges of pavement to account for any additional lanes, shoulders, and barrier walls that are needed to protect the pier from being struck by a vehicle." Exhibit 1 depicts the conceptual pier locations for the proposed alignment from the existing 95th street station south to Wendell Smith Park. CWC adjusted the pier locations to fall outside the limits of IDOT's future widening.

IDOT pointed out that any structure crossing a roadway should provide a minimum of 15'-0" of vertical clearance. The RLE would cross the following IDOT facilities: 95th Street, southbound I-94, I-57 ramp, and I-57 mainline. The extension would not modify the existing 15'-10" clearance at 95th Street. The proposed extension would cross the existing southbound I-94 tunnel. This clearance would not be affected by the RLE. The profile would rise from the tunnel, cross the I-57 ramp, and eventually cross over Wentworth Avenue. A 16-foot clearance is provided over Wentworth Avenue, which would lead to an approximately 31-foot clearance over the I-57 ramp. The alignment would then cross the I-57 mainline with an approximately 39-foot clearance. Excessive clearance is provided over the IDOT roadways because of the height the profile reaches to go over Wentworth Avenue.

IDOT also commented on the need for barrier warrant analysis, permitting, and air rights agreements. These tasks will be addressed in the future when the project moves from conceptual to preliminary design.

cc: file



\$TIME\$

Z	
atras -	
TRACKS	Gy & Transit Group
ABUTMENT (TYP)	cta
HAMMERHEAD PIER-	CHICAGO TRANSIT AUTHORITY
	SENSITIVE SECURITY INFORMATION When the mode conversion agents advery regention has a contact of when a on mart is not been and the mode of the mode of the second when a contact of the second of the mode of the second of the second of the second of the second of the second when a contact of the second of the second of the second when a second of the second of the second of the second when a second of the second of the second of the second when a second of the second of the second of the second when a second of the second of the second of the second of the second of the second of the second of the second of the second of the second of the second of the
	DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING.
	RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY
	ALL ALTERNATIVES PIER LOCATION SOUTH OF 95TH ST STATION
	IN CHARGE APPROVED BY
	CHECKED BY DESIGNED BY
No. of Concession, Name	DRAWN BY PROJECT NO CTA-RLE FILE NAME \$FILES\$
	30 0 30 60 SCALE H 1" = 30'-0"
	MARK DATE DESCRIPTION SHEET TITLE

EXHIBIT 1 - CURVES AT WENDALL SMITH AND PIER LOCATIONS (1/6)



\$TIME\$

EXHIBIT 1 - CURVES AT WENDALL SMITH AND PIER LOCATIONS (2/6)

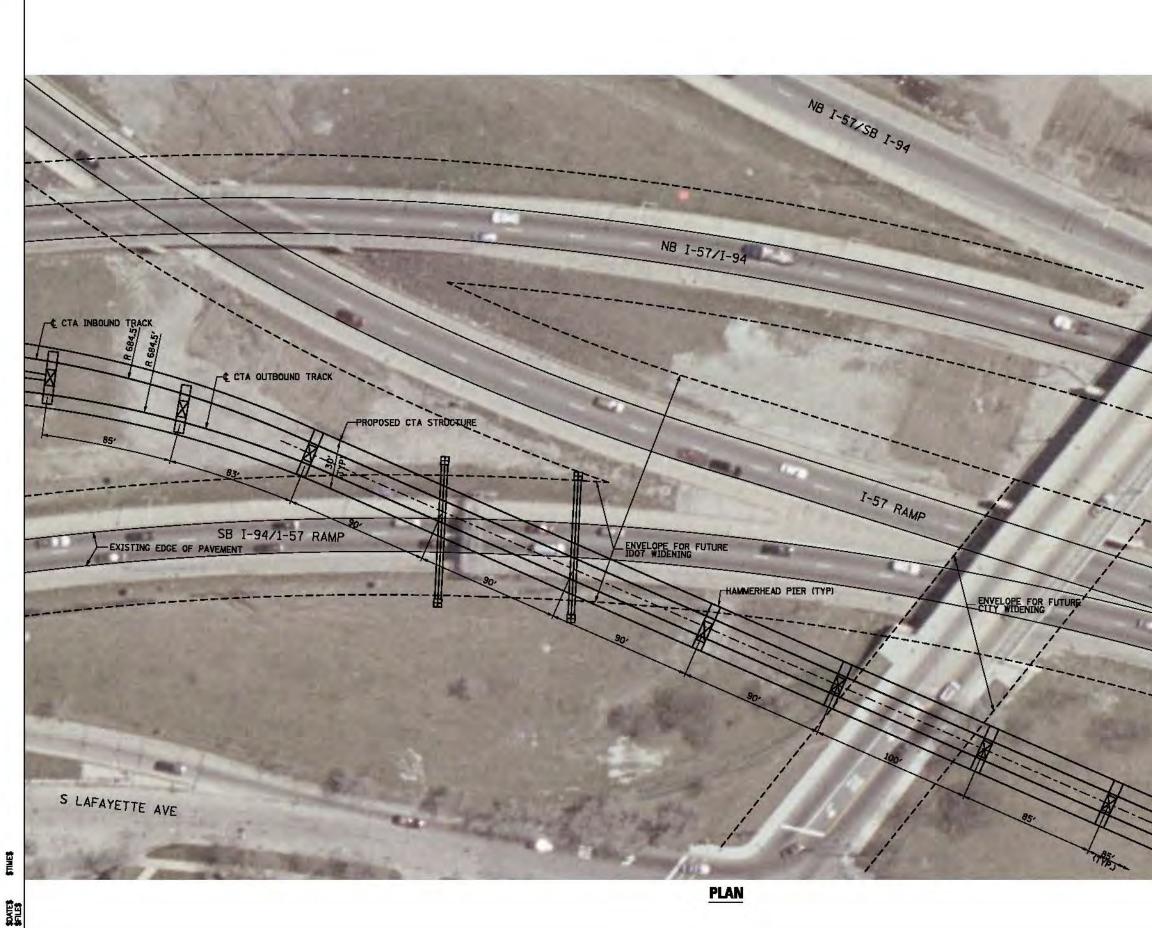
Z	
ING 98TH YARD TRACKS	
	Gy C"Transit Group
	cta
	CHICAGO TRANSIT AUTHORITY
	SENSITIVE SECURITY INFORMATION Where the receive convert entry information the scattering where the receive convertige with the second second to the receiver of the second secon
	DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING.
	RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY ALL ALTERNATIVES PIER LOCATION SOUTH OF 95TH ST STATION
	IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE FILE NAME \$FILES\$
EGEND	30 0 30 60 SCALE H 1" = 30'-0"
RE-DECK EXISTING TUNNEL SUPERSTRUCTURE	MARK DATE DESCRIPTION SHEET TITLE



STINES

T	
SI PANO	
· · · · · · · · · · · · · · · · · · ·	<b>Gyve</b> "Transit Group
85:	cta
	CHICAGO TRANSIT AUTHORITY
	SENSITIVE SECURITY INFORMATION WITH THE NEW CONTRACTOR DAY & SHOULD BE THE SENSE OF THE SENSE OF
	DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING.
	RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY ALL ALTERNATIVES PIER LOCATION SOUTH OF 95TH ST STATION
	IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE FILE NAME \$FILES\$
	30 0 30 60 SCALE H 1" = 30'-0"
	MARK DATE DESCRIPTION SHEET TITLE

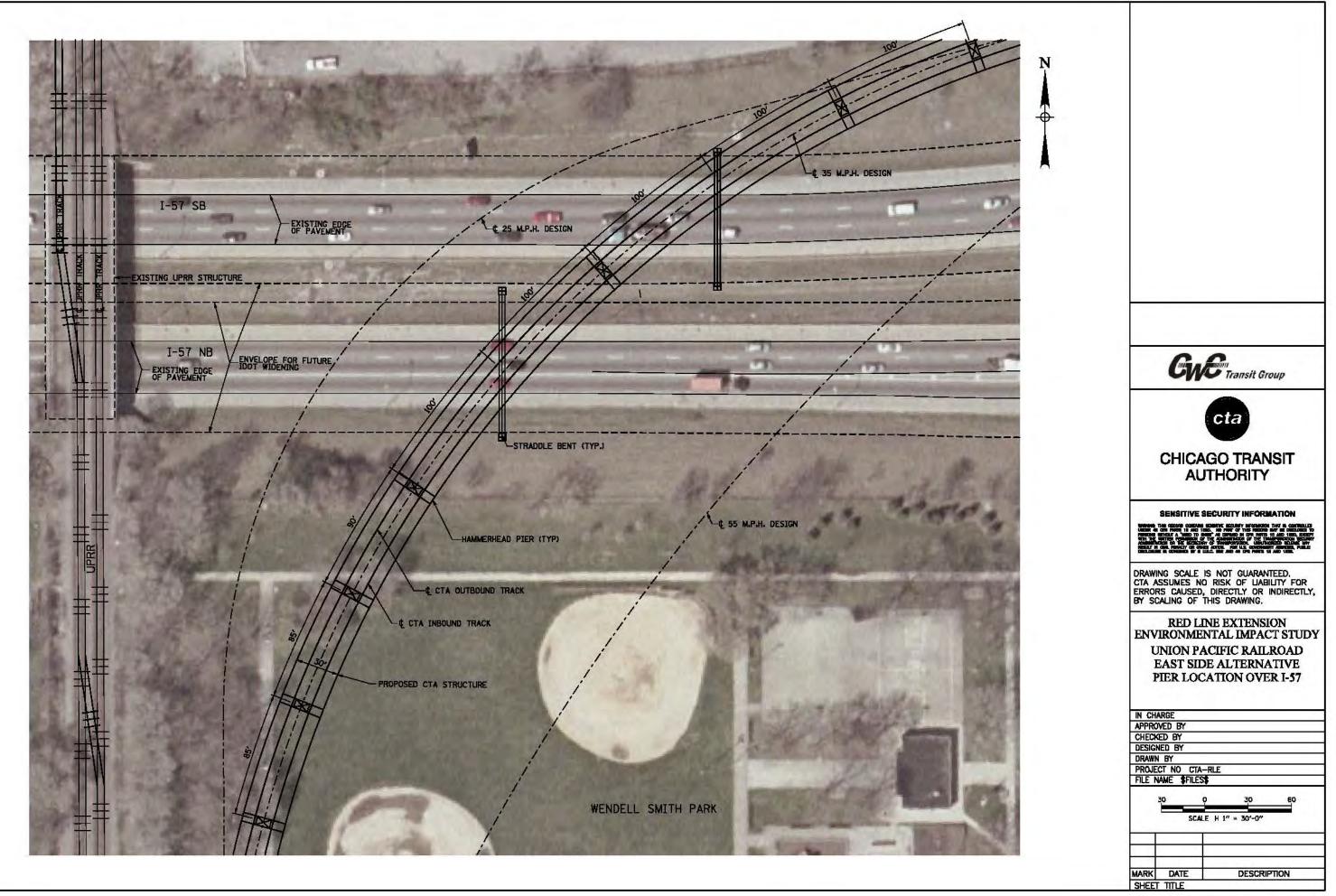
EXHIBIT 1 - CURVES AT WENDALL SMITH AND PIER LOCATIONS (3/6)



STIMES

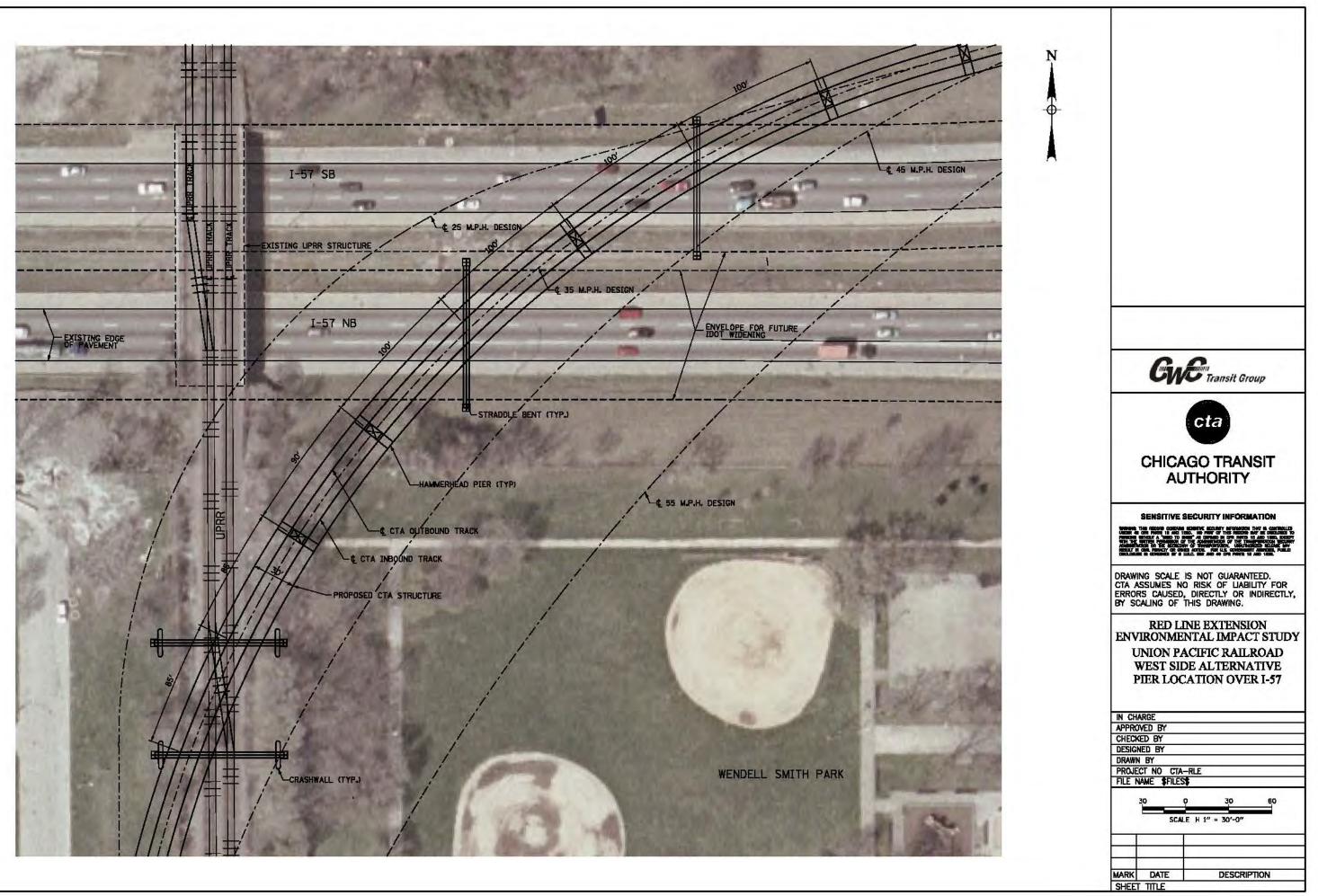
1. Mark	
S HEATHORTH AVE	
	<b>Cype</b> Transit Group
	Cta CHICAGO TRANSIT AUTHORITY
	SENSITIVE SECURITY INFORMATION WHEN THE CHEMIC CHEMIC BOARD HOT INFORMATION WHEN I AND THE SECURITY INFORMATION WHEN I AND THE SECURITY INFORMATION WHEN I AND THE SECURITY I AND THE SECURITY WHEN I AND THE SECURITY I AND THE SECURITY WHEN I AND THE SECURITY I AND THE SECURITY WHEN I AND THE SECURITY I AND THE SECURITY I AND THE SECURITY WHEN I AND THE SECURITY I AND THE SECURITY I AND THE SECURITY WHEN I AND THE SECURITY I AND THE SECURITY I AND THE SECURITY WHEN I AND THE SECURITY I AND THE SECURITY I AND THE SECURITY WHEN I AND THE SECURITY I AN
	DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING.
	RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY ALL ALTERNATIVES PIER LOCATION SOUTH OF 95TH ST STATION
	IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE FILE NAME \$FILES\$
	30 0 30 60 SCALE H 1" = 30"-0"
	MARK DATE DESCRIPTION SHEET TITLE

**EXHIBIT 1 - CURVES AT WENDALL SMITH AND PIER LOCATIONS (4/6)** 



1

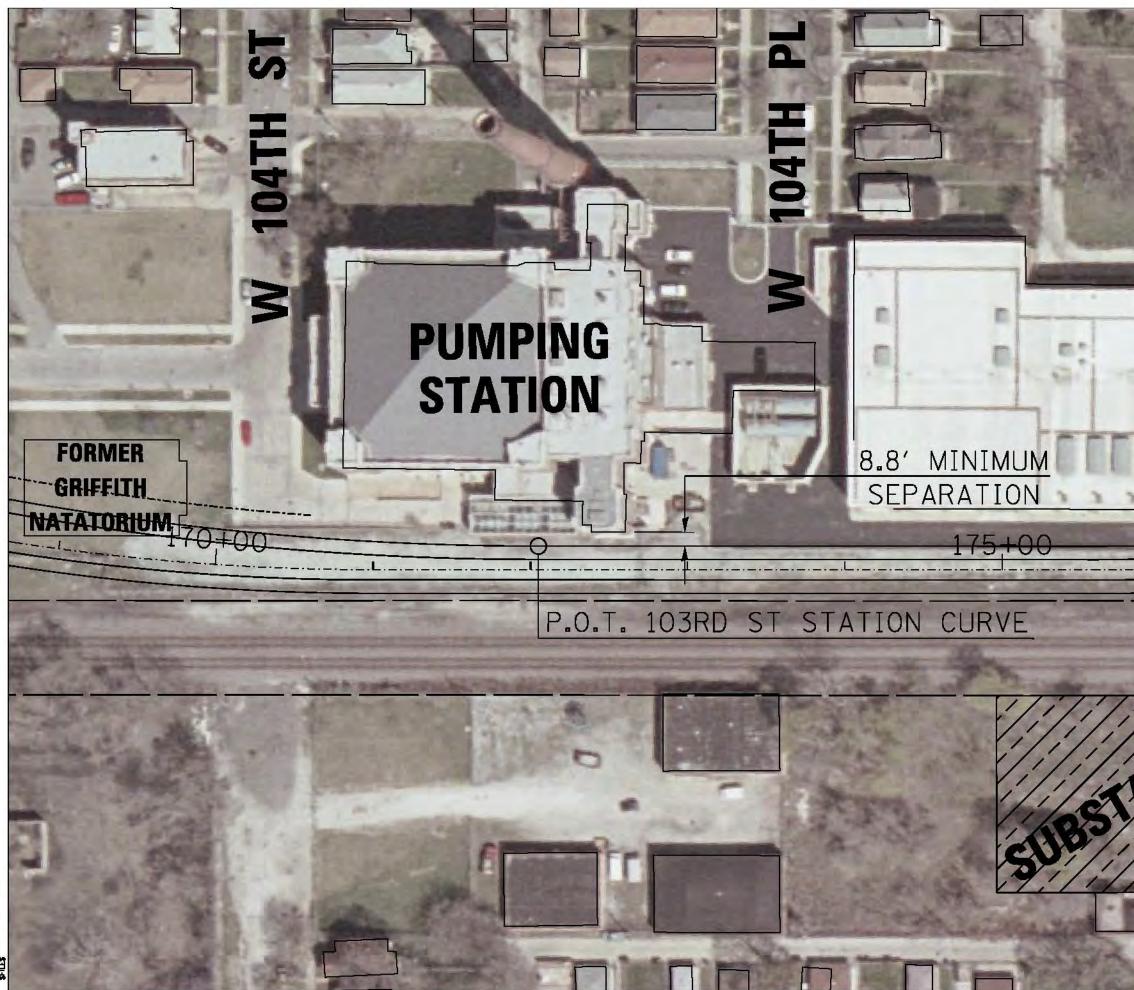
**EXHIBIT 1 - CURVES AT WENDALL SMITH AND PIER LOCATIONS (5/6)** 



Sannas Sannas

1

**EXHIBIT 1 - CURVES AT WENDALL SMITH AND PIER LOCATIONS (6/6)** 



	LEGEND
	PLAN PLAN PLAN PLAN PLAN PLAN PLAN PLAN
	UTILITIES EXISTING WATER LINES EXISTING SEWER LINE ENTRANCES PROMARY ENTRANCE EXIT ONLY PROPOSED PARKING ENTRANCE PROFILE PROFILE PROFILE PR TOP OF RAIL PR ELEVATED STRUCTURE DEPTH EXXXXXXX PLATFORM LOCATION
	Chicago Transit Group CHICAGO TRANSIT AUTHORITY
	SENSITIVE SECURITY INFORMATION WHEN A COMPACT SET OF A COMPACT AND A CO
TION	IN CHARGE ALTERNATIVE BAST SIDE OPTION IN CHARGE APPROVED BY CHECKED BY DESIGNED BY PROJECT NO CTA-RLE FILE NAME EXH_CTARLE_CT_LAY_UPRREAST_A3_103rdS 100 0 100 200 SCALE H 1" = 100'-0" V 1" = 10'-0"
	MARK DATE DESCRIPTION SHEET TITLE

EXHIBIT 2 - 103RD ST STATION ANALYSIS (1/1)

From: Salley, Jason R [mailto:Jason.Salley@illinois.gov]
Sent: Tuesday, December 16, 2014 8:39 AM
To: Tandon, Sonali; Baczek, John A
Subject: RE: CTA Red Line Extension

Sonali, Good morning.

We have looked at your proposed alignment of the CTA's Red Line Extension near I-90/94 at I-57 & 95<sup>th</sup> St and we offer the following comments:

- 1. The piers for the structures carrying the Red-Line over any of the Department's roadways should be placed such that the Department has the flexibility to widen our roadways either towards the roadway's median or outward with standard cross-section elements. As such any pier(s) placed in our medians should be placed in the middle of our medians, not immediately behind any existing or proposed barrier walls. Also, any piers proposed on the outside of our roadways should be placed at least 27-ft from the existing outside edges of pavement to account for any additional lanes, shoulders and barrier walls that are needed to protect the pier from being struck by a vehicle.
- 2. Any structures carrying the Red-Line over our roadways should provide at least 15'-0" of vertical clearance over our roadways.
- 3. The existing barrier walls along I-57 may have to be extended and/or replaced in order to adequately protect any proposed piers along I-57 from being struck by vehicles. The length of the extensions will be subject to barrier warrant and length of need analyses to be done during Contract Plan preparation should this project proceed towards construction. New barrier walls or guardrail may also be required if piers are proposed along I-57 where there are no existing barrier walls to extend. Lastly, existing barrier that are to be extended to protect proposed CTA piers may need to be rehabilitated or replaced depending on their condition at the time of the construction of this project. The cost sharing of this work is to be determined at the time of the preparation of the Contract Plans.
- Should the extension of the Red-Line move towards Contract Plan preparation, the CTA should ascertain a Permit from the Department for the construction of the extension within our Right-of-Way.
- 5. The extension of the Red-Line as depicted will require an Air Rights Agreement with the Federal Highway Administration as well as the Department.
- 6. Wentworth Ave is under the Jurisdiction and Maintenance of the City of Chicago. Please coordinate this extension with them in order to ascertain any requirements or concerns that they might have. Please share their comments with the Department so that all concerns are shared with all of the interested parties along the corridor.

Attached you will find a copy of the geometrics for I-57 south of its interchange with I-90/94 as per the Department's Phase I report for I-90/94 from 31<sup>st</sup> Street to I-57.

Please contact me or John Baczek if you have any questions or need any clarifications.

Sincerely, Jason Salley, P.E. Geometrics Engineer IDOT - District 1 P: 847.705.4085 F: 847.705.4159



### Memorandum

To:	CTA, Sonali Tandon
From:	CWC - Wight & Company
Date:	December 2, 2014, revised March 16, 2015
Subject:	Evaluation of Grade Separation of Red Line Extension

# Purpose

The purpose of this memo is to compare the potential benefits or drawbacks of constructing the proposed Red Line Extension (RLE) as an elevated track structure with the benefits or drawbacks of constructing the RLE as an at-grade track with roadway overpasses or underpasses.

The proposed extension must be elevated from 95th Street until south of Interstate 57 (I-57) (approximately station 135+00) to provide the required clearance over the expressway. South of I-57, the proposed track structure could potentially be lowered to match existing grade. Using the maximum allowable grade, this match point would occur 950 feet south of the interstate (approximately station 144+50.) The profile of this option is shown in the attached Exhibit A. The proposed track can potentially remain at grade for 2.57 miles (approximately station 280+00), at which point the track must be elevated to clear the CN/Metra Electric District mainline tracks.

Costs presented in this memorandum are macro-level estimates based on 2012 dollars. The costs presented are for comparison of the construction costs and do not include soft costs and unallocated contingencies. These costs do not represent the full cost of implementing the RLE.

### Option #1: Aerial Track Structure

The initial planning phases, including the Alternatives Analysis study that was completed in 2009, has the RLE on aerial track structure.

Approximate construction cost = \$100 to \$120 million per mile of double track

Estimated Total Construction Cost ~ \$120 M \* 2.57 miles = \$308.4 Million

Evaluation of Grade Separation of Red Line Extension Revised March 16, 2015 Page 2

#### **Benefits:**

- Minimizes effects on the neighborhood
- Minimizes noise levels as compared to at-grade trains
- Built-in security by being above grade
- Maintains existing roadway access

#### Drawbacks:

- Cost of aerial structure
- Public perception of visual impacts due to height of the structure

### Option #2: At-grade track, roads overpass track

Approximate construction cost = \$10 million per mile of double track.

Track construction cost ~ \$10 M \* 2.57 miles = \$25.7 Million

Roadway overpasses would be constructed at 101st Street, 103rd Street, 107th Street, 109th Street, 111th Street, Wentworth Avenue, 115th Street, and State Street (8 Crossings).

Overpass construction cost ~ \$25 million / each \* 8 = \$200 Million

Estimated Total Construction Cost ~ \$25.7 Million + \$200 Million = \$225.7 Million

#### **Benefits:**

- Potential \$82.7M construction savings versus Option #1: Aerial Track Option (Note: Potential construction savings would not outweigh the additional right-of-way acquisition covered under "Drawbacks" below.)
- CTA at-grade, vehicular traffic is grade separated from the CTA and Union Pacific Railroad (UPRR)

### Drawbacks:

- An analysis of the potential 107th Street overpass revealed approximately 220 residents would be adversely affected by the construction of the overpass (reduced access, increased travel distance and time, perceived visual impacts of overpasses, additional displacements, neighborhood interruption during construction, etc.) Extrapolating to the entire project, approximately 1,500 homes would have some adverse impact. Refer to Exhibit B.
- Additional right-of-way acquisition. Refer to Exhibit B.

- Closure of some cross streets where they intersect with the embankment for bridge approaches.
- Relocation of utilities along the roadways and cross streets.
- Community impacts due to neighborhood isolation.
- Potential visual impacts for homes near overpasses (abutting an approximately 30-foot tall embankment).
- Additional security may be required to protect the train operations and third rail as compared to aerial track structure.
- Potential increase in noise levels affecting adjoining residents.

### Option #3: At-grade track, cross-roads underpass track

Approximate construction cost = \$10 million per mile of double track.

Track construction cost ~ \$10 M \* 2.57 miles = \$25.7 Million

Roadway underpasses would be constructed at 101st Street, 103rd Street, 107th Street, 109th Street, 111th Street, Wentworth Avenue, 115th Street, and State Street (8 Crossings).

Underpass construction cost ~ \$60 million / each \* 8 = \$480 Million

Estimated Total Construction Cost ~ \$25.7 Million + \$480 Million = \$505.7 Million

#### **Benefits:**

• CTA at-grade, vehicular traffic is grade separated from the CTA and UPRR

#### Drawbacks:

- Costs of structure is significantly higher than Options #1 and #2.
- Property impacts similar to Option #2; additional property impacts would occur to accommodate the underpass structure (reduced access due to grade difference and needed retaining wall for underpass structure).
- Additional right-of-way acquisition.
- Closure of some cross streets required where the streets intersect or would require lowing cross streets to intersect at grade. Lowering of cross streets would require additional rightof-way and additional property impacts due to access.
- Community impacts due to neighborhood isolation.

- Additional costs associated with a pumping station for drainage (flood prevention).
- Relocation of utilities along the major roadways and cross streets.
- Additional security may be required to protect the train operations and third rail as compared to aerial track structure.
- No additional benefits over the Options #1 or #2

### Option #4: At-grade track between crossroads, maximum allowable grade

Another option considered to take advantage of the significantly lower cost of constructing atgrade track is to lower the profile of the proposed extension between crossroads to run atgrade as much as possible while avoiding the prohibitive adverse effects of constructing roadway overpasses or underpasses. Using the maximum allowable grades provided in the *CTA Infrastructure Design Criteria Manual*, and the minimum vertical curves recommended by AREMA, Exhibit C was created to show what the proposed profile would look like if the track were lowered to grade between crossroads. Note that Exhibit C does not show a realistic construction alternative; its purpose is solely to show the maximum possible length of atgrade construction between crossroads.

The combination of the tight existing crossroad spacing and the relatively shallow maximum allowable profile grade prevents the profile from dropping down to grade in all but two locations: 950 feet between 103rd Street/ 107th Street, and 1000 feet between 111th Street/ Wentworth Avenue.

### **Benefits:**

Potential construction savings versus Option #1: Aerial Track Option =

1950 ft/5280 ft \* (\$120 Million - \$10 Million) = \$40.6 Million

#### Drawbacks:

- Potential customer discomfort caused by constantly varying profile (a "rollercoaster" feel).
- Increased train spacing to provide safe braking distance which would lead to increased running time and need for additional cars.
- Increased operational risks related to traction.
- Additional energy consumption, vehicle wear and tear, and wheel/track maintenance.

### **Option #5: Track on retained embankment**

After analyzing the option of varying the vertical alignment at the maximum allowable grade, we considered varying the vertical alignment at a gentler grade which would avoid the

"rollercoaster" feel of the maximum grade while taking advantage of shorter structure heights. In conjunction with the gentler grade, we evaluated where it would be feasible to install the tracks on embankment with retaining walls rather than aerial track structure.

A significant portion of the alignment could potentially be built on embankment with retaining walls. Below are the approximate station ranges of the UPRR East Option which could be built on retained embankment:

Start Station	End Station	Retaining Wall Length
137+50	150+00	1,250 ft
150+75	159+85	910 ft
165+80	189+60	2,380 ft
190+30	203+08	1,278 ft
203+83	209+85	602 ft
217+13	241+34	2,421 ft
242+04	252+14	1,010 ft
252+84	257+55	471 ft
258+25	264+27	602 ft
270+40	280+00	960 ft
	Tota	l 11,884 ft

#### **Benefits:**

- Track structure on retained embankment can be constructed for approximately \$50 Million / mile. Potential construction savings for this option versus Option #1: Aerial Track Option = 11884 ft/5280 ft \* (\$120 Million \$50 Million) = \$157.5 Million (Note that the "planning level" costs per mile are intended to show the order of magnitude of cost savings in order to determine whether an alternative has merit for further study in the Preliminary Engineering phase. If additional crossings are required, the order of magnitude cost savings would be similar.)
- Potential for reduced noise impacts based on use of ballasted track system on the embankment.
- Potential lower maintenance for retaining walls versus aerial track structure.

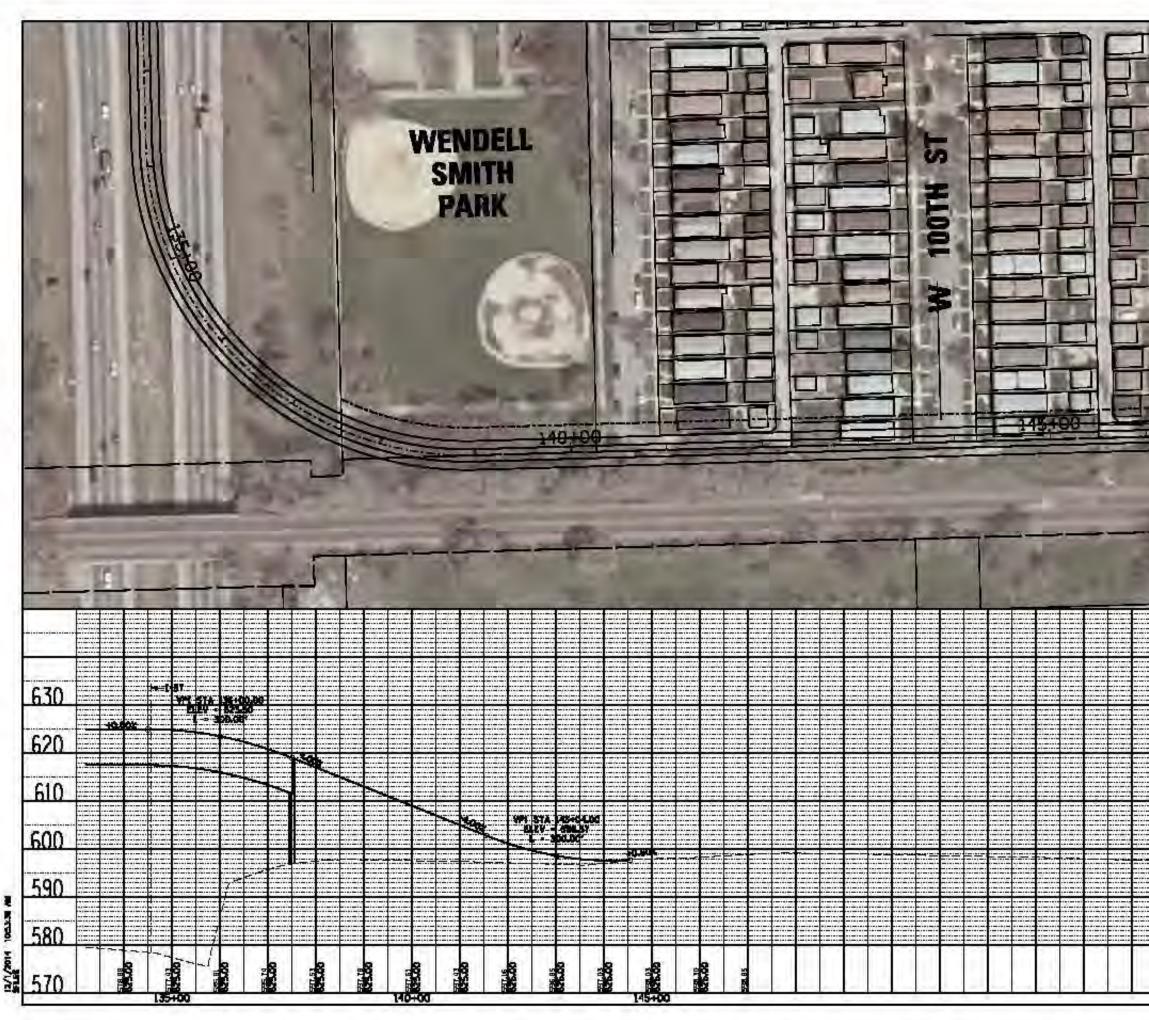
#### Drawbacks:

- Visual impacts (and accompanying community impacts) from retaining walls (approximate 23' retained height above grade).
- Visual impacts from potential graffiti (and accompanying increased maintenance cost).

# **Conclusions:**

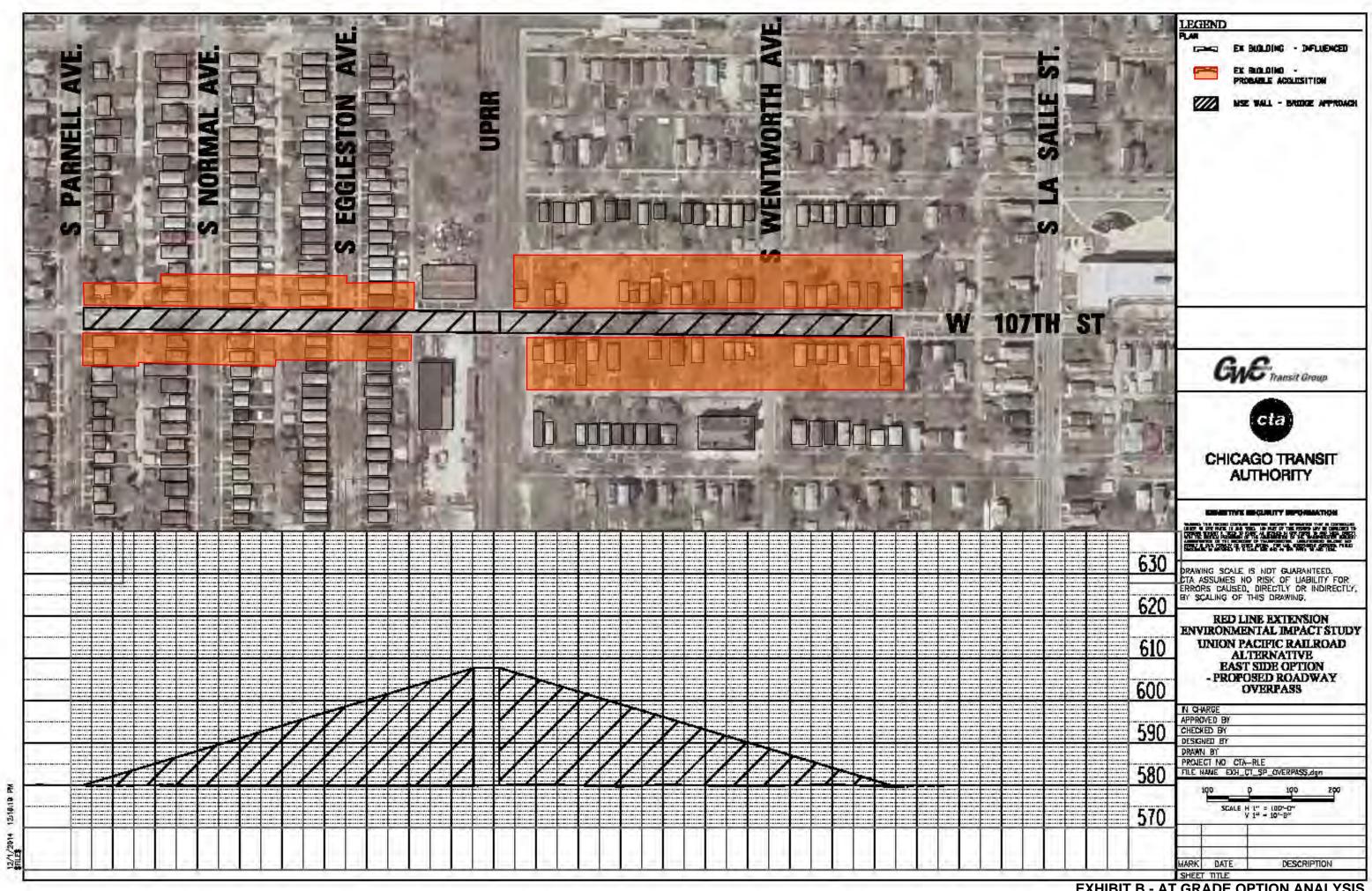
- Constructing the proposed extension at-grade, with roadway underpasses is the most expensive option, and offers no unique benefits.
- Constructing the proposed extension at-grade, with roadway overpasses offers potential construction savings on the track, but could potentially adversely affect more homes than Option #1: Aerial Track Structure. Although a detailed analysis of the additional right-of-way acquisition was not performed, the potential adverse impacts on such a large number of homes likely outweigh the potential \$100 million in savings.
- The small cost benefits of Option #4 (which shows only a short section of track has the potential to be constructed at-grade) would not outweigh the operational drawbacks of the inclines.
- Options #1 and #5: Aerial Track Structure and Track on Retained Embankment would extend transit service with the best balance of cost and neighborhood impacts over Options #2, #3, and#4. Option #5 has the potential for a sizable construction cost savings (over \$100 million) if the potential community impacts (including visual impacts) are acceptable or can be mitigated.

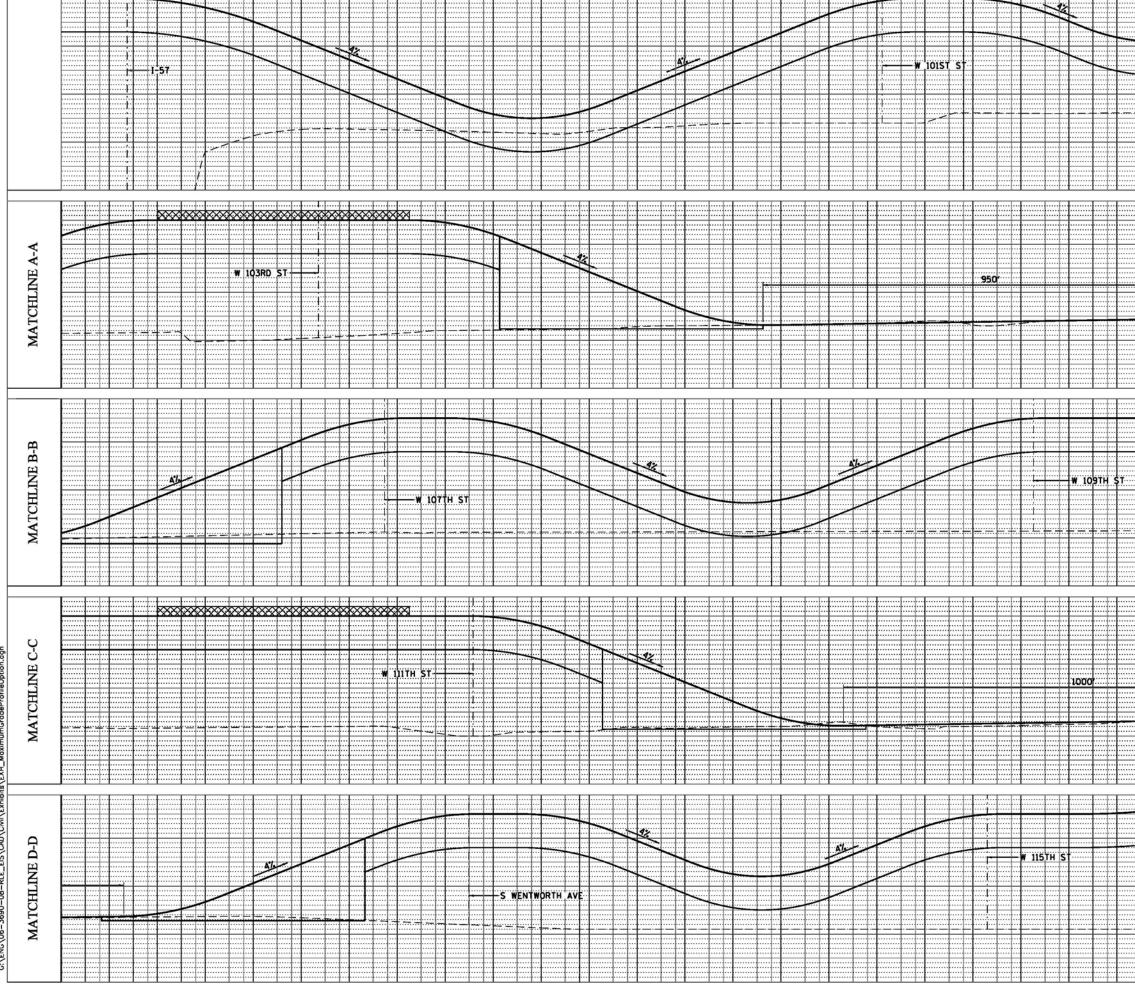
cc: file



1	-	30	10	T	LEGEND	
			W 100TH PL	アーモーモーモーモー	EX CONTERLINE     PR CENTERLINE     PR CENTERLINE     PR TRACK CENTERLINE     PR PARAMENT DIVELU     PR PARAMENT DIVELU     PR PARAMENT     PR CTA SUBSTATION     PR PARAMENT     PR PARAME	-WAY F-BAY ME FE TRUME S
THUT						
	-	-	_		CHICAGO TRANS AUTHORITY	п
					CHICAGO TRANS	
						HQM ED. Y FOR
				630	CHICAGO TRANS AUTHORITY	ED. Y FOR ARECTLY.
				630 620	DRAMMA SCALE IS NOT GUARANTEE CITA ASSUMES NO RISK OF LABILIT ERRORS CAUSED, DIRECTLY OR IND BY SCALING OF THIS DRAMMAG. RED LINE EXTENSION ENVIRONMENTAL IMPACT UNION PACIFIC RAILER ALTERNATIVE EAST SIDE OPTION	TOR Y FOR ARECTLY, STUDY OAD
					CHICAGO TRANS AUTHORITY COMMENDAL STATES CALLED STATES AND STATES CITA ASSUMES NO RESK OF LABILIT ERRORS CAUSED, DIRECTLY OR IND BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT UNION PACIFIC RAILER ALTERNATIVE EAST SIDE OFTION - AT GRADE PROFIL	TOR Y FOR ARECTLY, STUDY OAD
				620	CHICAGO TRANS AUTHORITY CONTINUE CONTINUES CONTINUES IN OF CLARANTER CTA ASSUMES NO RESK OF LABILIT ERRORS CAUSED, DIRECTLY OR INC BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT UNION PACIFIC RAILER ALTERNATIVE EAST SIDE OFTION - AT GRADE PROFIL	TOR Y FOR ARECTLY, STUDY OAD
				620 610	CHICAGO TRANS AUTHORITY CHICAGO TRANS AUTHORITY CHICAGO TRANS CALINE HOUSE CONTENT CHICAGO CONTENT CHICAGO CONTENT CHICAGO CONTENT CONTENT CHICAGO CONTENT CHICAGO CONTENT CHICAGO CONTENT CON	TO TO TO TO TO TO TO TO TO TO TO TO TO T
				620 610 600	CHICAGO TRANS AUTHORITY CHICAGO TRANS AUTHORITY CHICAGO TRANS CALLER CALLET AND AUTHORITY CHICAGO CONTRACT CITA ASSUMES NO RISK OF LABILIT ENTROPE CAUSED, DIRECTLY OR INC BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT UNION PACIFIC RAILER ALTERNATIVE EAST SIDE OFTION - AT GRADE PROFILE IN CHICAGE ATTROVED BY CHECKED BY CHECKED BY DRAWN BY PROJECT NO CIA-RE FILE WARE EDILANGYOGPYCTHICOLIGN.A	ED. Y FOR MRECTLY, STUDY DAD

**EXHIBIT A - AT GRADE OPTION ANALYSIS** 





12/12/2014 12:31:45 PM G:\ENG\06-3690-08-RLE\_EIS\CAD\Civi\Ex

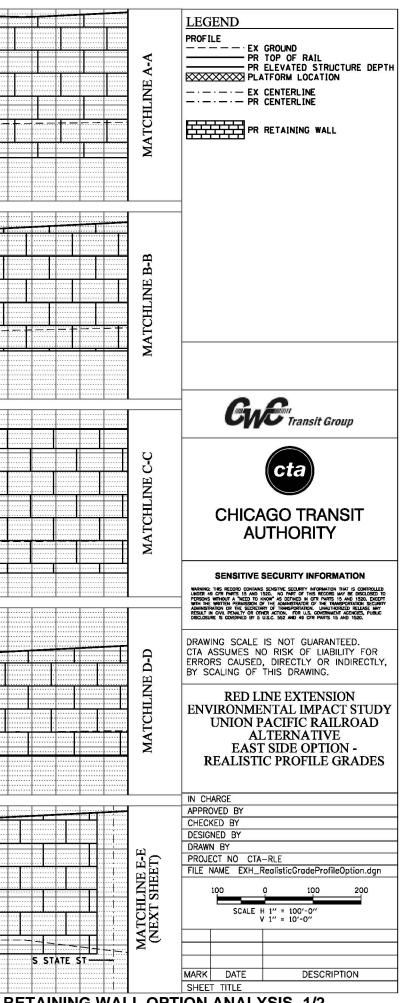
	 							LEGEND
	 			1		F		PROFILE
	 		-					EX GROUND
	 						A.	PR TOP OF RAIL
	 					_	MATCHLINE A-A	PR ELEVATED STRUCTURE DEPTH
	 			2			[7]	EXXXXXXX PLATFORM LOCATION
	 						Ë	EX CENTERLINE
	 						A,	
	 						H	
Т	 1						2	
	 						<b>a</b>	
	 						V	
	 						4	
	 1							
	 ••••							
•••	 				•••••			
	 						в	
	 						7	
	 ••••						H	
	 						E	
	 				H		4	
	 						Ц	
	 1		<b>.</b>				H	
	 		·		· ·····		2	
	 				-		I	
	 						MATCHLINE B-B	
	 İ			<b>.</b>			2	
	 1							
	_			_				Gy F Transit Group
	 							Transit Group
	 						(7	
	 						MATCHLINE C-C	
	 						0	
	 						н	
	 		·····				Z	
	 						Ξ	CHICAGO TRANSIT
	 						Ð	
	 						E	AUTHORITY
	 				<u></u> .	····	A	AUTHORITT
_	 						Σ	
	 						· · ·	
	 							SENSITIVE SECURITY INFORMATION
	 							WARNING: THIS RECORD CONTAINS SENSITIVE SECURITY INFORMATION THAT IS CONTROLLED
_								WARNING: THE RECORD CONTAINS SENSITING SECURITY INFORMATION THAT IS CONTROLLED UNDER 40 CFR PARTS 15 AND 1320, NO PART OF THIS RECORD MAY BE DESCLOSED TO PERSONS WITHOUT A "NEED TO KNOW" AS DEFINED IN O'R PARTS 15 AND 1320, EXCEPT
								WITH THE WHITEN PERMISSION OF THE ADMINISTRATOR OF THE TRANSPORTATION SECURITY Administration or the Societary of thisportation, unautinorgen relates why result in CML pennly or other action. For U.S. Government Acencies, public desclosure is governmed by 5 U.S.C. Soz and 9 gover parts 15 and 1520.
	 							DISCLOSURE IS GONDRINED BY 5 U.S.C. 552 AND 49 CFR PARTS 15 AND 1520.
								DRAWING SCALE IS NOT GUARANTEED.
							0	DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR
							Ģ	CTA ASSUMES NO RISK OF LIABILITY FOR
							D-D	CTA ASSUMES NO RISK OF LIABILITY FOR
							E D-D	
							NE D-D	CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING.
							LINE D-D	CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION
							HLINE D-D	CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION
							CHLINE D-D	CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY
							TCHLINE D-D	CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD
							ATCHLINE D-D	CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE
							MATCHLINE D-D	CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST SIDE OPTION -
							MATCHLINE D-D	CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST SIDE OPTION -
							MATCHLINE D-D	CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE
							MATCHLINE D-D	CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST SIDE OPTION -
							MATCHLINE D-D	CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING.
							MATCHLINE D-D	CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST SIDE OPTION -
							MATCHLINE D-D	CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING.
							MATCHLINE D-D	CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING.
							MATCHLINE D-D	CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING.
							MATCHLINE D-D	CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST SIDE OPTION - MAXIMUM PROFILE GRADES IN CHARGE APPROVED BY CHECKED BY DESIGNED BY
								CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST SIDE OPTION - MAXIMUM PROFILE GRADES IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY
								CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST SIDE OPTION - MAXIMUM PROFILE GRADES IN CHARGE APPROVED BY CHECKED BY DESIGNED BY
								CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST SIDE OPTION - MAXIMUM PROFILE GRADES IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DESIGNED BY PROJECT NO CTA-RLE
								CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST SIDE OPTION - MAXIMUM PROFILE GRADES IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY
								CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING.
								CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING.
								CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST SIDE OPTION - MAXIMUM PROFILE GRADES IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DESIGNED BY PROJECT NO CTA-RLE FILE NAME EXH_MaximumGradeProfileOption.dgn
								CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING.
		s st						CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST SIDE OPTION - MAXIMUM PROFILE GRADES IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE FILE NAME EXH_MaximumGradeProfileOption.dgn 100 0 100 200 SCALE H 1" = 100'-0"
		5 ST	ATE					CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST SIDE OPTION - MAXIMUM PROFILE GRADES IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE FILE NAME EXH_MaximumGradeProfileOption.dgn 100 0 100 200 SCALE H 1" = 100'-0"
		5 ST					MATCHLINE E-E MATCHLINE D-D (NEXT SHEET)	CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST SIDE OPTION - MAXIMUM PROFILE GRADES IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE FILE NAME EXH_MaximumGradeProfileOption.dgn 100 0 100 200 SCALE H 1" = 100'-0"
			ATE					CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST SIDE OPTION - MAXIMUM PROFILE GRADES IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE FILE NAME EXH_MaximumGradeProfileOption.dgn 100 0 100 200 SCALE H 1" = 100'-0"
		s st						CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST SIDE OPTION - MAXIMUM PROFILE GRADES IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE FILE NAME EXH_MaximumGradeProfileOption.dgn 100 0 100 200 SCALE H 1" = 100'-0" V 1" = 10'-0"
		5 ST						CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST SIDE OPTION - MAXIMUM PROFILE GRADES IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE FILE NAME EXH_MaximumGradeProfileOption.dgn 100 0 100 200 SCALE H 1" = 100'-0"
		5 ST						CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST SIDE OPTION - MAXIMUM PROFILE GRADES IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DESIGNED BY DRAWN BY PROJECT NO CTA-RLE FILE NAME EXH_MaximumGradeProfileOption.dgn 100 0 100 200 SCALE H 1" = 100'-0" V 1" = 10'-0"

**EXHIBIT C - MAXIMUM GRADE PROFILE OPTION ANALYSIS -1/2** 

	G:\ENC\06-3690-08-RLE_EIS\CAD\CWi\Exhibits\	EXH_Maxir			MATCHLINE E-E
	MATCHLINE I-I	HLINE H-H	MATCHLINE G-G	MATCHLINE F-F	(FROM PREVIOUS SHEET)
					××××
					AN A
					YE
					~~~
					-w 116
					6TH 3
					ST
					IDIAN
					IA AY
					E
R. MYRD /					RAIR
RD A					IE A
					/E

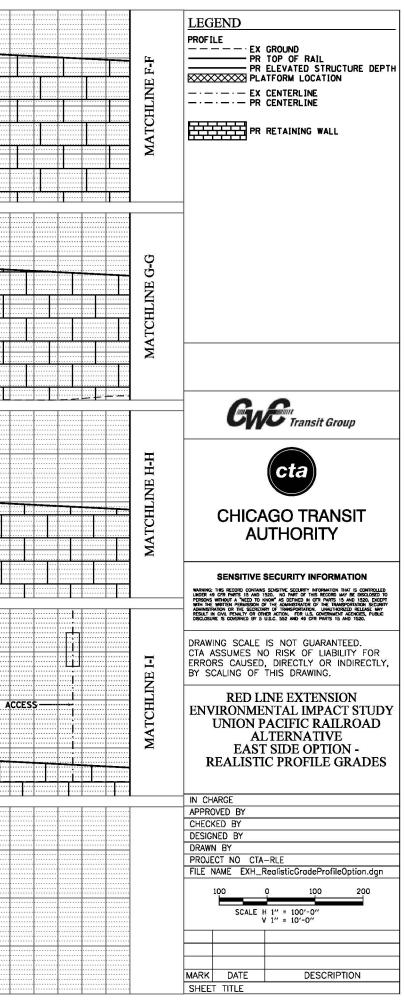
		LEGEND
	1	PROFILE
		EX GROUND
	 Ľ.	PR TOP OF RAIL
	 MATCHLINE F-F	PR ELEVATED STRUCTURE DEPTH
	 ш	EXXXXXXX PLATFORM LOCATION
	 Z	EX CENTERLINE
	 9	PR CENTERLINE
	 E	
	 ΗĔ	
	 A	
	 X	
	 1	
	 1	
	 5	
	 5	
	 <u> </u>	
	 IZ	
	 EI I	
	 H	
	 2	
	 MATCHLINE G-G	
	 Z	
	1	
	 1	
	 1	0.0
		Cyce Transit Group
		Transit Group
	 1 🕀	
	 Ĥ	(Cta)
	 Ξ	
	 1 3	
		CHICAGO TRANSIT
	 ΗĔ	
	 IATC	AUTHORITY
	MATCHLINE H-H	
	MATC	AUTHORITY
	MATC	
	MATC	AUTHORITY SENSITIVE SECURITY INFORMATION
	MATC	AUTHORITY SENSITIVE SECURITY INFORMATION UNDER A STATE SCHEME SCHEMEY INFORMATION THAT IS CONTROLLED UNDER A OF NAMES SCHEME SCHEMEY INFORMATION THAT IS CONTROLLED UNDER A OF NAMES SCHEME SCHEMEY INFORMATION OF THE SCHEME WITH DE WITTEN PERMASSION OF THE ADMINISTRY OF THE TRANSPORTATION SCHEMEY ADMINISTRY OF THE SCHEME AND A OF THE TRANSPORTATION SCHEMEY RESULT IN CALE PENALTY OF OHER ACTION, TORI U.S. GOVERNMENT ACTIONS RELAGE WAY RESULT IN CALE PENALTY OF OHER ACTION, TOR U.S. GOVERNMENT ACTIONS RELAGE DECISIONE IS GOVERNOL BY S U.S.C. SSF AND 44 OR PARTS 15 AND 1500.
		AUTHORITY SENSITIVE SECURITY INFORMATION UNDER A DRI NUT DI SC. NO THE OTHER TORMATION THAT IS CONTROLLED UNDER AS DRI NUT DI SC. NO THE OTHER TORMATION THAT IS CONTROLLED UNDER AS DRI NUT DI SC. NO THE OTHER TORMATION THAT IS CONTROLLED UNDER AS DRI NUT DI SC. NO THE OTHER THE OTHER THE OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER THE OTHER THE OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER THE OTHER THE OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER THE OTHER THE OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER THE OTHER THE OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER OTHER THE OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER OTHER THE OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER OTHER THE OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER OTHER THE OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER OTHER THE OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER OTHER THE OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER OTHER THE OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER OTHER OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER OTHER OTHER ADMENTIATION OF THE SCREETLY OTHER OTHER OTHER OTHER OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER OTHER OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER OTHER OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER OTHER OTHER OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER OTHER OTHER OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER OTHER OTHER OTHER OTHER OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER OTHER OTHER OTHER OTHER ADMENTIATION OF THE SCREETLY OTHER OTHER OTHER OTHER ADMENTIATION OF THE SCREETLY OTHER OT
		AUTHORITY SENSITIVE SECURITY INFORMATION UNDER 19 OFF PARTS IS AND 1320, NO PART OF THIS RECORD WAY BE OBSIGNED TO DEPOND THIS RECORD CONTAINS SEVENTIA SECURITY INFORMATION THAT IS CONTROLLED DEPOND THIS IS AND 1320, NO PART OF THIS RECORD WAY BE OBSIGNED TO DEPOND THIS IS AND 1320, NO PART OF THIS RECORD WAY BE OBSIGNED TO DEPOND THIS IS AND 1320, NO PART OF THIS RECORD WAY BE OBSIGNED TO PREMARE THIS IS AND 1320, NO PART OF THIS RECORD WAY BE OBSIGNED TO PREMARE THIS IS AND 1320, NO PART OF THIS RECORD WAY BE OBSIGNED TO PREMARE THIS IS AND 1320, NO PART OF THIS AND 1320. DESCRIPTION OF THE SECRETARY OF THE OBSIGNED TO THE OBSIGNED DECLOSURE IS CONTROL OF IT IS USE. SOOT AND 49 OFF PARTS IS AND 1320. DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR
		AUTHORITY SENSITIVE SECURITY INFORMATION UNDER A DRI NUT DI SC. NO THE OTHER TORMATION THAT IS CONTROLLED UNDER AS DRI NUT DI SC. NO THE OTHER TORMATION THAT IS CONTROLLED UNDER AS DRI NUT DI SC. NO THE OTHER TORMATION THAT IS CONTROLLED UNDER AS DRI NUT DI SC. NO THE OTHER THE OTHER THE OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER THE OTHER THE OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER THE OTHER THE OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER THE OTHER THE OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER THE OTHER THE OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER OTHER THE OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER OTHER THE OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER OTHER THE OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER OTHER THE OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER OTHER THE OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER OTHER THE OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER OTHER THE OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER OTHER OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER OTHER OTHER ADMENTIATION OF THE SCREETLY OTHER OTHER OTHER OTHER OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER OTHER OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER OTHER OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER OTHER OTHER OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER OTHER OTHER OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER OTHER OTHER OTHER OTHER OTHER ADMENTIATION OF THE SCREETLY OT THE OTHER OTHER OTHER OTHER OTHER ADMENTIATION OF THE SCREETLY OTHER OTHER OTHER OTHER ADMENTIATION OF THE SCREETLY OTHER OT
		AUTHORITY SENSITIVE SECURITY INFORMATION DECISION OF THE SECURITY INFORMATION THAT IS CONTIDUED THE RECORD GOVERNME SOUTH SECURITY INFORMATION THAT IS AND TABLE THE RECORD WITH A WEED TO INFORM A DUBLE OF THE THE SAME ISSUE DECISION THE RECORD WITH A WEED TO INFORM A DUBLE OF THE THE SAME ISSUE DECISION ADMINISTRATION OF THE SECURITY OF THE WARD THAT IS AND TABLE DECISION OF THE SECURITY OF THE ADMINISTRATION OF THE THE SAME ISSUE DECISION DECISION OF THE SECURITY OF THE ADMINISTRATION OF THE THE SAME ISSUE DECISION DECISION OF THE SECURITY OF THE ADMINISTRATION OF THE THE SAME ISSUE DECISION DECISION OF THE SECURITY OF THE ADMINISTRATION OF THE THE SAME ISSUE DECISION DECISION OF THE SECURITY OF THE ADMINISTRATION OF THE THE SAME ISSUE DECISION DECISION OF THE SECURITY OF THE ADMINISTRATION OF THE THE SAME ISSUE DECISION OF THE SECURITY OF THE SECURITY OF THE SAME ISSUE DECISION OF THE SECURITY OF THE SECURITY OF THE SAME ISSUE DECISION OF THE SECURITY OF THE SECURITY OF THE SAME ISSUE DECISION OF THE SECURITY OF THE SECURITY OF THE SAME ISSUE DECISION OF THE SECURITY OF THE SECURITY OF THE SAME ISSUE DECISION OF THE SECURITY OF THE SECURITY OF THE SAME ISSUE DECISION OF THE SECURITY OF THE SECURITY OF THE SAME ISSUE DECISION OF THE SECURITY OF THE SECURITY OF THE SAME ISSUE DECISION OF THE SECURITY OF THE SECURITY OF THE SAME ISSUE DECISION OF THE SECURITY OF THE SE
		AUTHORITY SENSITIVE SECURITY INFORMATION UNDER THE RECORD CONTARTS SINGING THE SECURITY INFORMATION INFORMATION OF THE SECURITY INFORMATION THAT IS CONTIDUED INFORMATION OF THE SECURITY OF THE RECORD WAT IS CONTIDUED WITH THE WITH A PUBLISHOP OF THE ADMENDIATION OF THE INFORMATION ADMENTION OF THE SECURITY OF THE ADMENDIATION OF THE INFORMATION ADMENTION OF THE SECURITY OF THE ADMENDIATION OF THE INFORMATION ADMENTION OF THE SECURITY OF THE ADMENDIATION OF THE INFORMATION ADMENTION OF THE SECURITY OF THE ADMENDIATION OF THE INFORMATION ADMENTION OF THE SECURITY OF THE ADMENDIATION OF THE INFORMATION ADMENTION OF THE SECURITY OF THE ADMENDIATION OF THE INFORMATION ADMENTION OF THE SECURITY OF THE ADMENDIATION OF THE INFORMATION ADMENTION OF THE SECURITY OF THE ADMENTION OF THE INFORMATION ADMENTION OF THE SECURITY OF THE ADMENTION OF THE INFORMATION ADMENTION OF THE SECURITY OF THE ADMENTION OF THE INFORMATION ADMENTION OF THE SECURITY OF THE ADMENTION OF THE INFORMATION ADMENTION OF THE SECURITY OF THE ADMENTION OF THE INFORMATION ADMENTION OF THE SECURITY OF THE ADMENTION OF THE INFORMATION ADMENTION OF THE SECURITY OF THE ADMENTION ADMENTION OF THE SECURITY OF THE ADMENTION OF THE ADMENTION ADMENTION OF THE SECURITY OF THE ADMENTION ADMENTION OF THE ADMENTION OF THE ADMENTION ADMENTION OF THE SECURITY OF THE ADMENTION ADMENTION OF THE SECURITY OF THE ADMENTION ADMENTION OF THE ADMENTION OF THE ADMENTION ADMENTION OF THE ADMENTION OF THE ADMENTION ADMENTION OF T
ACCESS		AUTHORITY SENSITIVE SECURITY INFORMATION DECISION OF THE SECURITY INFORMATION THAT IS CONTIDUED THE RECORD GOVERNME SOUTH SECURITY INFORMATION THAT IS AND TABLE THE RECORD WITH A WEED TO INFORM A DUBLE OF THE THE SAME ISSUE DECISION THE RECORD WITH A WEED TO INFORM A DUBLE OF THE THE SAME ISSUE DECISION ADMINISTRATION OF THE SECURITY OF THE WARD THAT IS AND TABLE DECISION OF THE SECURITY OF THE ADMINISTRATION OF THE THE SAME ISSUE DECISION DECISION OF THE SECURITY OF THE ADMINISTRATION OF THE THE SAME ISSUE DECISION DECISION OF THE SECURITY OF THE ADMINISTRATION OF THE THE SAME ISSUE DECISION DECISION OF THE SECURITY OF THE ADMINISTRATION OF THE THE SAME ISSUE DECISION DECISION OF THE SECURITY OF THE ADMINISTRATION OF THE THE SAME ISSUE DECISION DECISION OF THE SECURITY OF THE ADMINISTRATION OF THE THE SAME ISSUE DECISION OF THE SECURITY OF THE SECURITY OF THE SAME ISSUE DECISION OF THE SECURITY OF THE SECURITY OF THE SAME ISSUE DECISION OF THE SECURITY OF THE SECURITY OF THE SAME ISSUE DECISION OF THE SECURITY OF THE SECURITY OF THE SAME ISSUE DECISION OF THE SECURITY OF THE SECURITY OF THE SAME ISSUE DECISION OF THE SECURITY OF THE SECURITY OF THE SAME ISSUE DECISION OF THE SECURITY OF THE SECURITY OF THE SAME ISSUE DECISION OF THE SECURITY OF THE SECURITY OF THE SAME ISSUE DECISION OF THE SECURITY OF THE SECURITY OF THE SAME ISSUE DECISION OF THE SECURITY OF THE SE
		AUTHORITY SENSITIVE SECURITY INFORMATION UNIVERSITIVE SECURITY INFORMATION UNIVERSITIVE SECURITY INFORMATION THAT IS CONTROLLED UNIVERSITY AND TASK, NO THAT OF THIS RECORD WAY IS CONTROLLED UNIVERSITY OF NAMED AND TASK, NO THAT OF THIS RECORD WAY IS CONTROLLED UNIVERSITY OF THE SECURITY OF THE MANEDIATION TO THE RECORD WAY IS CONTROL ADMARTING THE SECURITY OF THE MANEDIATION OF THE RECORD WAY IS CONTROL ADMARTING THE SECURITY OF THE MANEDIATION OF THE RECORD WAY IS CONTROL ADMARTING THE SECURITY OF THE MANEDIATION OF THE RECORD ADMARTING THE SECURITY OF THE SECURITY OF THE ADMARTING THE SECURITY OF THE SECURITY OF THE SECURITY ADMARTING THE SECURITY OF THE SECURITY OF THE ADMARTING THE SECURITY OF THE SECURITY OF THE SECURITY ADMARTING THE SECURITY OF THE SECURITY OF THE SECURITY SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY SECURITY OF THE SECURITY OF T
		AUTHORITY SENSITIVE SECURITY INFORMATION UNIVERSITIES SECURITY INFORMATION UNIVERSITIES AND INC. NO INFO OF THE RECORD WAT IS CONTROLLED UNIVERSITIES AND INC. NO INFO OF THE RECORD WAT IS CONTROLLED UNIVERSITIES AND INC. NO INFO OF THE RECORD WAT IS CONTROLLED UNIVERSITIES AND INC. NO INFO OF THE RECORD WAT IS CONTROLLED WITH THE WITHIN APPLICATION OF ADMINISTRATION OF THE RECORD WAT IS CONTROL ADMINISTRATION OF THE SCIENCE OF ADMINISTRATION OF THE RECORD WAT IS CONTROL ADMINISTRATION OF THE SCIENCE OF A ADMINISTRATION OF THE RECORD WAT IS CONTROL ADMINISTRATION OF THE SCIENCE OF ADMINISTRATION OF THE RECORD WAT IS CONTROL ADMINISTRATION OF THE SCIENCE OF ADMINISTRATION OF THE RECORD WATER ADMINISTRATION OF THE SCIENCE OF ADMINISTRATION OF THE RECORD WATER ADMINISTRATION OF THE SCIENCE OF ADMINISTRATION OF THE RECORD WATER ADMINISTRATION OF THE SCIENCE OF ADMINISTRATION OF THE RECORD WATER ADMINISTRATION OF THE SCIENCE OF ADMINISTRATION OF THE RECORD ADMINISTRATION OF THE SCIENCE OF ADMINISTRATION OF THE RECORD WATER ADMINISTRATION OF THE SCIENCE OF ADMINISTRATION OF THE RECORD ADMINISTRATION OF THE SCIENCE OF ADMINISTRATION OF THE RECORD ADMINISTRATION OF THE SCIENCE OF ADMINISTRATION OF THE RECORD ADMINISTRATION OF THE SCIENCE OF ADMINISTRATION OF THE RECORD OF THE ADMINISTRATION OF THE SCIENCE OF ADMINISTRATION OF THE RECORD ADMINISTRATION OF THE SCIENCE OF ADMINISTRATION OF THE RECORD OF THE ADMINISTRATION OF THE SCIENCE OF THE SCIENCE OF ADMINISTRATION OF THE RECORD OF THE SCIENCE OF THE SCIENCE OF ADMINISTRATION OF THE SCIENCE OF ADMINISTRATION OF THE SCIENCE OF THE SCIENCE OF ADMINISTRATION OF THE RECORD OF THE SCIENCE OF
	MATCHLINE I-I MATC	AUTHORITY SENSITIVE SECURITY INFORMATION UNIVERSE THE RECORD CONTANT SCHEME SECHENT INTOMINION THAT IS CONTINUED UNIVERSE THE RECORD CONTANT SCHEME SECHENT INTOMINION THAT IS CONTINUED UNIVERSE THE RECORD CONTANT SCHEME SECHENT INTOMINION THAT IS CONTINUED UNIVERSE THE CONTANT SCHEME SECHENT INTOMINION THAT IS CONTINUED UNIVERSE THE CONTANT SCHEME SECHENT INTOMINION THAT IS CONTINUED INTO THE THE SECHENT IS AND TAKEN AND THE OFFICIAL OFFICIAL ADDRESS AND THE SECHENCY OF THE SECHENT IN AND THE OFFICIAL OFFICIAL ADMINISTRATION OF THE SECHENT OF THE SECHENT IS AND THE SECHENCE IS CONTANT OF THE SECHENT OF THE SECHENT IN AND THE DECLEMENT SCHEME IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE
		AUTHORITY SENSITIVE SECURITY INFORMATION UNIVERSE THE RECORD CONTAINS SENSITIVE SECURITY INFORMATION THAT IS CONTROLLED UNIVERSE THE RECORD CONTAINS SENSITIVE SECURITY IN TORMATION THAT IS CONTROLLED UNIVERSE THE RECORD ON AND SECURITY OF THE RECORD WITH IS CONTROLLED UNIVERSE THE RECORD ON THE SECURITY OF THE RECORD WITH IS CONTROLLED UNIVERSE THE RECORD OF AND SECURITY OF THE RECORD WITH IS CONTROLLED UNIVERSE THE RECORD OF THE SECURITY OF THEORY OF THE SECURITY ASSUMED TO THE SECURITY OF THEORY OF THE SECURITY OF THEORY OF THE SECURITY OF THE SECURITY OF THEORY OF THE SECURITY OF THE DECORDER IS CONTROL OF IT IS USED. SET AND IS AND THE DECORDER IS CONTROL OF IT IS USED. SET AND IS AND THE CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING.
		AUTHORITY SENSITIVE SECURITY INFORMATION UNIVERSE THE RECORD CONTAINS SCHEME SECURITY INFORMATION THAT IS CONTROLLED UNIVER AS DEFINITION AND THE SECURITY IN TOMATION THAT IS CONTROLLED UNIVERSE THE RECORD CONTAINS SCHEME SECURITY OF THE RECORD WIT BE DECORDED TO THE THE THE THE SECURITY OF THE SECURITY OF THE SECURITY OF ADMARTMENT AND THE SECURITY OF THE SECURITY OF THE SECURITY ADMARTMENT AND THE SECURITY OF THE SECURITY OF THE SECURITY ADMARTMENT AND THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE
		AUTHORITY SENSITIVE SECURITY INFORMATION UNIVERSE THE RECORD CONTAINS SENSITIVE SECURITY INFORMATION THAT IS CONTROLLED UNIVERSE THE RECORD CONTAINS SENSITIVE SECURITY IN TORMATION THAT IS CONTROLLED UNIVERSE THE RECORD ON AND SECURITY OF THE RECORD WITH IS CONTROLLED UNIVERSE THE RECORD ON THE SECURITY OF THE RECORD WITH IS CONTROLLED UNIVERSE THE RECORD OF AND SECURITY OF THE RECORD WITH IS CONTROLLED UNIVERSE THE RECORD OF THE SECURITY OF THEORY OF THE SECURITY ASSUMED TO THE SECURITY OF THEORY OF THE SECURITY OF THEORY OF THE SECURITY OF THE SECURITY OF THEORY OF THE SECURITY OF THE DECORDER IS CONTROL OF IT IS USED. SET AND IS AND THE DECORDER IS CONTROL OF IT IS USED. SET AND IS AND THE CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING.
		AUTHORITY SENSITIVE SECURITY INFORMATION UNREWE THE RECORD CONTAINS SENSITIVE SECURITY INFORMATION THAT IS CONTINUED UNREWE THE RECORD CONTAINS SENSITIVE SECURITY IN TORMATION THAT IS CONTINUED UNREWET THE RECORD ON TAKES SENSITIVE SECURITY AT THOMATION THAT IS CONTINUED UNREWET THE RECORD ON THE SECURITY OF THE RECORD WITH IS CONTINUED UNREWET THE RECORD ON THE SECURITY OF THE RECORD WITH IS CONTINUED SECURITY OF THE SECURITY OF THE RECORD WITH IS AND ISOL DECOMPLY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE DECOMPLY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE
		AUTHORITY SENSITIVE SECURITY INFORMATION UNREWE: THE RECORD GOVERNE SENETIVE SECURITY INFORMATION THAT IS CONTROLLED UNREWE: THE RECORD GOVERNE SENETIVE SECURITY INFORMATION THAT IS CONTROLLED UNREWE: THE RECORD GOVERNE SENETIVE SECURITY OF THE RECORD WITH & GOVERNMENT PERSON HOURD A THEOD TO GOVERNE SECURITY OF THE RECORD WITH & GOVERNMENT RESULT IN CONTROL & GOVERNMENT AND ISOL DECORDANCE DESCORDER & GOVERNED BY S USE. SET AND 44 OFF PARTS IS AND ISOL DECORDER & GOVERNED BY S USE. SET AND 44 OFF PARTS IS AND ISOL DECORDER & GOVERNED BY S USE. SET AND 44 OFF PARTS IS AND ISOL DECORDER & GOVERNED BY S USE. SET AND 44 OFF PARTS IS AND ISOL DECORDER & GOVERNED BY S USE. SET AND 44 OFF PARTS IS AND ISOL DECORDER & GOVERNED BY S USE. SET AND 44 OFF PARTS IS AND ISOL DECORDER & GOVERNED BY S USE. SET AND 44 OFF PARTS IS AND ISOL DECORDER & GOVERNED BY S USE. SET AND 44 OFF PARTS IS AND ISOL DECORDER & GOVERNED BY S USE. SET AND 44 OFF PARTS IS AND ISOL DECORDER & GOVERNED BY S USE. SET AND 44 OFF PARTS IS AND ISOL DECORDER & GOVERNED BY S USE. SET AND 44 OFF PARTS IS AND ISOL DECORDER & GOVERNED BY S USE. SET AND 44 OFF PARTS IS AND ISOL DECORDER & GOVERNED BY S USE. SET AND 44 OFF PARTS IS AND ISOL DECORDER & GOVERNED BY S USE. SET AND 44 OFF PARTS IS AND ISOL DECORDER & GOVERNED BY S USE. SET AND 44 OFF PARTS IS AND ISOL DECORDER & GOVERNED BY S USE. SET AND 44 OFF PARTS IS AND ISOL DECORDER & GOVERNED BY S USE. SET AND 44 OFF PARTS IS AND ISOL DECORDER & GOVERNED BY S USE. SET AND 44 OFF PARTS IS AND ISOL DECORDER & GOVERNED BY S USE. SET AND 44 OFF PARTS IS AND ISOL DECORDER & GOVERNED BY S USE. SET AND 44 OFF PARTS IS AND ISOL DECORDER & GOVERNED BY S USE. SET AND 44 OFF PARTS IS AND ISOL DECORDER & GOVERNED AND ALL TERNATIVE EAST SIDE OPTION - MAXIMUM PROFILE GRADES IN CHARGE
		AUTHORITY SENSITIVE SECURITY INFORMATION DESCRIPTION OF A CONTROL SOUTH SECURITY INFORMATION INFO TO THE RECORD SOUTH SECURITY INFORMATION IN IT IS AND INFORMATION PERSON WHICH A WEED TO INFORM A CONTROL IN OTHER INFORMATION SECURITY AND INFORMATION OF THE INFORMATION OF THE INFORMATION OF THE INFORMATION OF THE INFORMATION OF THE INFORMATION OF THE AND INFORMATION OF THE INFORMATION OF THE INFORMATION OF THE INFORMATION OF THE INFORMATION OF THE INFORMATION OF THE INFORMATION OF THE INFORMATION OF THE INFORMATION OF THE INFORMATION OF THE INFORMATION OF THE INFORMATION OF THE INFORMATION OF THE INFORMATION OF THE INFORMATION OF THE INFORMATION OF THE INFORMATION OF THE INFORMATION OF THE INFORMATION OF THE INFORMA
		AUTHORITY SENSITIVE SECURITY INFORMATION UNIVERSITY OF RECORD GOVERNES SOUTH SECURITY INFORMATION UNIVERSITY OF THE RECORD GOVERNES SOUTH SECURITY INFORMATION THAT IS CONTROLLED UNIVERSITY OF THE RECORD OF THE ADMENDIATION OF THE INFORMATION CONTROL UNIVERSITY OF THE SECURITY OF THE OPERATION SECURITY ADMENDIATE SOUTHOUT OF THE INFORMATION OF THE INFORMATION OF THE SECURITY OF THE SECURITY OF THE OPERATION OF THE INFORMATION DECOMPOSED BY SUBJECT OF THE OPERATION OF THE INFORMATION DECOMPOSED OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST SIDE OPTION - MAXIMUM PROFILE GRADES IN CHARGE APPROVED BY CHECKED BY
		AUTHORITY SENSITIVE SECURITY INFORMATION UNDER THE RECORD GOVERNE SOUTH SECURITY INFORMATION THAT IS CONTIDUED UNDER A DEFINITION DIALON OF THE INFORMATION THAT IS CONTIDUED UNDER A DEFINITION OF THE INFORMATION THAT IS CONTIDUED UNDER THE RECORD OF THE ADMENDIANCE OF THE INFORMATION THAT IS CONTIDUED UNDER THE RECORD OF THE ADMENDIANCE OF THE INFORMATION OF THE SECURITY OF THE SECURITY OF THEORETICAL UNDER CONTINUE SECURITY ADMENDIANCE OF THE INFORMATION THAT IS CONTIDUED DECIDENT OF THE SECURITY OF THEORETICAL OF THE INFORMATION DECIDENT OF THE SECURITY OF THEORETICAL OF THE INFORMATION OF THE SECURITY OF THE SECURITY OF THEORETICAL OF THE IS AND TOOL OF THE SECURITY OF THEORETICAL OF THE IS AND TOOL OF THE SECURITY OF THE SECURITY OF THEORETICAL OF THE IS AND TOOL OF THE SECURITY OF THE SECURITY OF THEORETICAL OF THE IS AND TOOL OF THE SECURITY OF THEORETICAL OF THE SECURITY OF THEORETICAL OF THE SECURITY OF THE SECURITY OF THEORETICAL OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THE SECURITY OF THEORETICAL OF THE SECURITY OF T
		AUTHORITY SENSITIVE SECURITY INFORMATION UNDERSTINE RECORD CONTAME SENSITIC SECURITY INFORMATION THAT IS CONTROLLED UNDERSTITUTES AND TAXOS AND THE SECURITY INFORMATION THAT IS CONTROLLED UNDERSTITUTES AND TAXOS AND THE SECURITY AND THE SECURITY ADMINISTRATION THAT IS CONTROLLED UNDERSTITUTES AND TAXOS AND THE SECURITY ADMINISTRATION THAT IS CONTROLLED DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST SIDE OPTION - MAXIMUM PROFILE GRADES IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DRAWN BY
		AUTHORITY SENSITIVE SECURITY INFORMATION UNRENET THE RECORD DOVIANE SEMETINE SECURITY INFORMATION THAT IS CONTROLLED UNRENT THE RECORD DOVIANE SEMETINE SECURITY INFORMATION THAT IS CONTROLLED UNRENT THE RECORD DOVIANE SEMETINE SECURITY INFORMATION THAT IS CONTROLLED UNRENT THE RECORD DOVIANE SEMETINE SECURITY OF THE MEMORYMANO OF THE RECORD WITH BE DEVICED RELIABLE SEMETINES OF THE RECORD DOVIES AND THE ADMENDIATION OF THE RECORD THAT IS CONTROLLED UNRENT THE RECORD DOVIANE SEMETINE SECURITY OF THE RECORD DOVIES AND THE RE
		AUTHORITY SENSITIVE SECURITY INFORMATION UNDER THE RECORD CONTARTS SIMILITY INFORMATION THAT IS CONTROLLED UNDER AS DRA NATES IN AND TADIO, NOT THE SECURITY INFORMATION THAT IS CONTROLLED UNDER THE RECORD CONTARTS SIMILITY FOR THE SECURITY OF SECURITY OF
		AUTHORITY SENSITIVE SECURITY INFORMATION UNRENET THE RECORD CONTAME SPREITLE SECURITY INFORMATION THAT IS CONTROLLED UNRENT THE RECORD CONTAME SPREITLE SECURITY INFORMATION THAT IS CONTROLLED UNRENT THE RECORD ON A SECURITY OF THE RECORD WAY BE CONTROLLED UNRENT THE RECORD OF THE SECURITY OF THE RECORD WAY BE CONTROLLED UNRENT THE RECORD OF THE SECURITY OF THE RECORD WAY BE CONTROLLED UNRENT THE RECORD OF THE SECURITY OF THE RECORD WAY BE CONTROLLED UNRENT THE RECORD OF THE SECURITY OF THE RECORD WAY BE CONTROLLED UNRENT THE RECORD OF THE SECURITY OF THE RECORD OF THE SECURITY ADMENTATION OF THE SECURITY OF THE RECORD WAY BE CONTROLLED DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING.  RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST SIDE OPTION - MAXIMUM PROFILE GRADES  IN CHARGE APPROVED BY CHECKED BY DESIGNED BY PROJECT NO CTA-RLE
		AUTHORITY SENSITIVE SECURITY INFORMATION Manual Sector of the Recent And Sector And Sect
		AUTHORITY SENSITIVE SECURITY INFORMATION SENSITIVE SECURITY INFORMATION MANY IS RECORD GOVERNESSNER, SECURITY, INFORMATION INFO IS CONTINUED PERSON WINDOW A WEED TO INCOME ALL OFFICIAL INFORMATION INFORMATION PERSON WINDOW AND ALL DECIDENCIAL ALL OPERATIONS IN AND INFORMATION AND INFORMATION OF THE INFORMATION OF THE INFORMATION SECURITY AND INFORMATION OF THE INFORMATION OF THE INFORMATION SECURITY AND INFORMATION OF THE INFORMATION OF THE INFORMATION SECURITY AND INFORMATION OF THE INFORMATION OF THE INFORMATION OF THE ADDITION OF THE SECURITY OF THE INFORMATION OF THE INFORMATION OF THE ADDITION OF THE SECURITY OF THE INFORMATION OF THE INFORMATION OF THE DECIDENCE IS NOT GUARANTEED. CITA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST SIDE OPTION - MAXIMUM PROFILE GRADES IN CHARGE APPROVED BY CHECKED BY DESIGNED BY PROJECT NO CIA-RLE FILE NAME EXH_MAXIMUMGRODEProfileOption.dgn 100_0_100_200 SCALE H 1″ = 100′-0″
		AUTHORITY AUTHORITY SENSITIVE SECURITY INFORMATION UNRENET THE RECORD OWNERS SHEITING EXCHEMY INFORMATION THAT IS CONTROLLED UNRENT THE RECORD OWNERS SHEITING EXCHEMY INFORMATION THAT IS CONTROLLED UNRENT THE RECORD OWNERS SHEITING EXCHEMY INFORMATION THAT IS CONTROLLED UNRENT THE RECORD OWNERS SHEITING EXCHEMY INFORMATION THAT IS CONTROLLED UNRENT THE RECORD OWNERS SHEITING EXCHEMY INFORMATION THAT IS CONTROLLED UNRENT THE RECORD OWNERS SHEITING EXCHEMY INFORMATION THAT IS CONTROLLED UNRENT THE RECORD OWNERS SHE IN AND INFORMATION OF THE RECORD OWNER IN ADMINISTRATION OF THE RECORD OWNER IN A MANAGEMENT OF THE RECORD OF
		AUTHORITY SENSITIVE SECURITY INFORMATION SENSITIVE SECURITY INFORMATION MANY IS RECORD GOVERNESSNER, SECURITY, INFORMATION INFO IS CONTINUED PERSON WINDOW A WEED TO INCOME ALL OFFICIAL INFORMATION INFORMATION PERSON WINDOW AND ALL DECIDENCIAL ALL OPERATIONS IN AND INFORMATION AND INFORMATION OF THE INFORMATION OF THE INFORMATION SECURITY AND INFORMATION OF THE INFORMATION OF THE INFORMATION SECURITY AND INFORMATION OF THE INFORMATION OF THE INFORMATION SECURITY AND INFORMATION OF THE INFORMATION OF THE INFORMATION OF THE ADDITION OF THE SECURITY OF THE INFORMATION OF THE INFORMATION OF THE ADDITION OF THE SECURITY OF THE INFORMATION OF THE INFORMATION OF THE DECIDENCE IS NOT GUARANTEED. CITA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST SIDE OPTION - MAXIMUM PROFILE GRADES IN CHARGE APPROVED BY CHECKED BY DESIGNED BY PROJECT NO CIA-RLE FILE NAME EXH_MAXIMUMGRODEProfileOption.dgn 100_0_100_200 SCALE H 1″ = 100′-0″
		AUTHORITY SENSITIVE SECURITY INFORMATION SENSITIVE SECURITY INFORMATION MANY IS RECORD GOVERNESSNER, SECURITY, INFORMATION INFO IS CONTINUED PERSON WINDOW A WEED TO INCOME ALL OFFICIAL INFORMATION INFORMATION PERSON WINDOW AND ALL DECIDENCIAL ALL OPERATIONS IN AND INFORMATION AND INFORMATION OF THE INFORMATION OF THE INFORMATION SECURITY AND INFORMATION OF THE INFORMATION OF THE INFORMATION SECURITY AND INFORMATION OF THE INFORMATION OF THE INFORMATION SECURITY AND INFORMATION OF THE INFORMATION OF THE INFORMATION OF THE ADDITION OF THE SECURITY OF THE INFORMATION OF THE INFORMATION OF THE ADDITION OF THE SECURITY OF THE INFORMATION OF THE INFORMATION OF THE DECIDENCE IS NOT GUARANTEED. CITA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST SIDE OPTION - MAXIMUM PROFILE GRADES IN CHARGE APPROVED BY CHECKED BY DESIGNED BY PROJECT NO CIA-RLE FILE NAME EXH_MAXIMUMGRODEProfileOption.dgn 100_0_100_200 SCALE H 1″ = 100′-0″
		AUTHORITY SENSITIVE SECURITY INFORMATION SENSITIVE SECURITY INFORMATION MANY IS RECORD GOVERNESSNER, SECURITY, INFORMATION INFO IS CONTINUED PERSON WINDOW A WEED TO INCOME ALL OFFICIAL INFORMATION INFORMATION PERSON WINDOW AND ALL DECIDENCIAL ALL OPERATIONS IN AND INFORMATION AND INFORMATION OF THE INFORMATION OF THE INFORMATION SECURITY AND INFORMATION OF THE INFORMATION OF THE INFORMATION SECURITY AND INFORMATION OF THE INFORMATION OF THE INFORMATION SECURITY AND INFORMATION OF THE INFORMATION OF THE INFORMATION OF THE ADDITION OF THE SECURITY OF THE INFORMATION OF THE INFORMATION OF THE ADDITION OF THE SECURITY OF THE INFORMATION OF THE INFORMATION OF THE DECIDENCE IS NOT GUARANTEED. CITA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST SIDE OPTION - MAXIMUM PROFILE GRADES IN CHARGE APPROVED BY CHECKED BY DESIGNED BY PROJECT NO CIA-RLE FILE NAME EXH_MAXIMUMGRODEProfileOption.dgn 100_0_100_200 SCALE H 1″ = 100′-0″
		AUTHORITY SENSITIVE SECURITY INFORMATION MANNEL INFORMATION SENSITIVE SECURITY INFORMATION MANNEL INFORMATION MANNEL INFORMATION INFORMATION MANNEL INFORMATION DECISIONER IS CONTANT SECURITY OF MANNEL INFO DECISIONER IS CONTANT OF THE INFORMATION INFORMATION DECISIONER IS CONTANT OF THE INFORMATION DECISIONER IS NOT GUARANTEED. CITA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING. RED LINE EXTENSION ENVIRONMENTAL IMPACT STUDY UNION PACIFIC RAILROAD ALTERNATIVE EAST SIDE OPTION - MAXIMUM PROFILE GRADES IN CHARGE APPROVED BY CHECKED BY DESIGNED BY DESIGNED BY DESIGNED BY PROJECT NO CIA-RLE FILE NAME EXH_MOXIMUMGrodeProfileOption.dgn 100 0 100 200 SCALE H 1" = 100'-0" SCALE H 1" = 100'-0"
		AUTHORITY AUTHORITY SENSITIVE SECURITY INFORMATION SERVICE SECURITY INFORMATION OF THE INFORMATION SECURITY SERVICE SECURITY INFORMATION OF THE INFORMATION SECURITY SERVICE SECURITY INFORMATION OF THE INFORMATION SECURITY SECURITY INFORMATION OF THE INFORMATION SECURITY SECURITY INFORMATION OF THE INFORMATION OF THE INFORMATION SECURITY SECURITY OF THE ADMENDIATION OF THE INFORMATION SECURITY SECURITY SECURITY OF THE ADMENDIATION OF THE INFORMATION SECURITY SECURITY SECURITY OF THE ADMENDIATION OF THE INFORMATION OF THE INFORMATION DECISIONE OF DUE SECURITY OF THE ADMENDIATION OF THE INFORMATION OF THE INFORMATION DECISIONE OF THE SECURITY OF THE ADMENDIATION OF THE INFORMATION SECURITY OF THE SECURITY OF THE ADMENDIATION OF THE INFORMATION DECISIONED BY DESIGNED

EXHIBIT C - MAXIMUM GRADE PROFILE OPTION ANALYSIS -2/2



**EXHIBIT D - RETAINING WALL OPTION ANALYSIS -1/2** 

014 1:12:56 PM 06-3690-08-RLE_EIS\CAD\Civil\Exhib	12/19/2014 1:12:56 PM G:\ENG\06-3690-08-RLE_EIS\CAD\CWi\Exhibits\EXH_RealisticGradeProfileOption.dgn			
MATCHLINE I-I	MATCHLINE H-H	MATCHLINE G-G	MATCHLINE F-F	MAICHLINE E-E (FROM PREVIOUS SHEET)
			CN	
			METRA	
				STH
				ST
				PRAI
	IWRD			



**EXHIBIT D - RETAINING WALL OPTION ANALYSIS -2/2** 



## Memorandum

To:	Sonali Tandon
From:	CWC, Wight & Company
Date:	December 2, 2014, revised June 11, 2015
Subject:	120th Street Rail Shop and Yard Program

The purpose of this document is to summarize program data collected for a Red Line Extension (RLE) yard for use in a future design phase of the project. This document was developed through meetings with CTA Rail Operations, Infrastructure, Planning, Maintenance, and Scheduling. The programming elements listed in this document indicate an ideal yard and are based on past experiences and lessons learned. The design and layout of the Midway yard was used as a guide when developing this document as Midway represents a yard on the current CTA system with good flow and operations. At the time of design, current CTA design criteria pertinent to yards (as found in the Infrastructure Design Criteria Manual) should be reflected in the layout and design.

Note that the conceptual layout related to this document was required to function with an intermediate RLE phase having service terminating at 115th Street and the ultimate RLE extension to 130th Street. In the intermediate phase, yard storage should accommodate a minimum of 288 cars on storage tracks. (Potential storage on the interior cleaning track, shop leads, or yard leads would not be included in this count.) In the ultimate build-out, yard storage should accommodate a minimum of 340 cars. See the yard capacity attachment to this memorandum for further detail.

The existing 98th Street Yard could be used to store extra trains or for sweeping cars. The use of the existing 98th Street Yard should be considered when designing the future yard. The 98th Street Yard should not be used as a maintenance facility after the new yard becomes active.

## GENERAL

This scope document was developed to indicate the overall scope of the Red Line Extension -120th Street Rail Shop and Yard and indicate the general design parameters. The scope does not indicate or describe all the work required for the full performance and completion of the project. The materials, details, sizes and quantities of the rail shop and yard are to be designed by a future designer and constructed by a contactor. The designer and contractor shall provide

all the requirements of the scope documents and shall include all items and services reasonably inferable from the scope documents. The designer and contractor shall only have contact with the firm retained to prepare these scope documents as authorized by the CTA. The preparer of the scope documents will not be responsible for the acts or omissions of the designer, contractor or any subcontractor or sub-consultant.

# **PROJECT LOCATION & EXISTING CONDITIONS:**

The site of the proposed Red Line Extension Rail Yard is generally industrial in nature. It is located west of the I-94 expressway and Lake Calumet at approximately 122nd Street. The site is accessible by Cottage Grove Avenue as well as local access drives. The Metropolitan Water Reclamation District currently owns the site as well as much of the adjacent properties to the south and east. Three existing train tracks directly to the east of the site are operated by the Northern Indiana Commuter Transportation District, Chicago South Shore and South Bend Railroads. The K-Five Materials Corporation plant is directly northwest of the proposed site.

# CODE COMPLIANCE

The entire project shall comply with the City of Chicago, State of Illinois and Federal Code requirements and the American with Disabilities Act (ADA) as applicable.

The entire project shall also comply with the Chicago Transit Authority standards, unless otherwise shown.

# ARCHITECTURAL SCOPE

### Site

Yard:

- 11 Storage tracks various combinations of train lengths for a total of 340 cars not including any shop tracks, shop leads, or yard leads
- Track 12 (adjacent to shop) shall have door level cleaning platform with area allocated for trash containers.
- Sidewalks between every storage track, or utility vehicle access width between every other storage track – see yard section alternatives attachment to this memorandum.
- The storage track shall accommodate two 16-foot drives crossing the storage tracks generally running southwest to northeast from each end of the shop building – see plan view.
- The yard should generally be level and should be raised above the potential flood elevation. The profile should include slight upward slopes at each end to prevent winddriven roll-away trains.

- The use of photovoltaic panels should be studied to cover and protect the parked trains or a
  portion of the parked trains from the elements and to offset the building electrical load.
- A yard master tower is required. Master tower is depicted in the exhibit documents on the roof of the shop.
- The site lighting should be carefully planned to be energy efficient, vandal resistant, easy to maintain, and meet safety regulations. American Railway Engineering Maintenance of Way Association (AREMA) policy should be used as a minimum standard and guideline for the lighting of the yard. Fixtures utilizing the latest available technologies should be considered.
- A secure fence shall be provided around the entire yard and coordinated with the security system. (Perimeter cameras with infrared and analytics should be considered.)

### Other Site Requirements (including exterior of shop):

- Minimum of 130 vehicle parking spaces including handicap accessible spots as required by code.
- All site utilities shall be coordinated for clearances and cover depths to meet each specific utilities requirement.
- Site drainage shall conform to the City of Chicago Stormwater Management Ordinance Manual and City of Chicago Regulations for Sewer Construction and Stormwater Management. Best Management Practices shall be utilized wherever possible.
- Loading dock is required for parts delivery. Truck turning radius required to be coordinated with CTA.
- Landscape shall be in accordance with the City of Chicago zoning code.
- Provide locations for snow and track/bulk material storage.
- Trash compactor located near loading dock.
- Trash bin near cleaning platform.
- Provide sections of short track for non-revenue equipment storage.

### Building

The floor area of the entire shop including the track bays, administration, maintenance and ancillary areas is depicted in the exhibit document as 85,000 SF. The final area should be coordinated with CTA to confirm all building requirements are met. The envelope design must properly address performance concerns including thermal, condensation, durability, constructability and economy. Daylighting and high performance glazing shall be utilized in the

shop and administration areas. The floor area depicted in the exhibit drawings show all the programed space located on the main level but multiple levels may be utilized if it is found to be economical. Stairs and an elevator would need to be added to the program if multiple floors are utilized. Depending on the type of hoists used, a partial basement may be required.

### Shop:

- Five required track bays All five tracks run all the way through shop
- Interior Wash Track:
  - No Pit
  - Minimum capacity of 10 cars
  - Pull Through access with overhead doors at both ends
  - Out-to-out width for the bay to accommodate the brushes and control room
  - Water treatment and water recycling equipment
- Pit Track:
  - Minimum capacity of 10 cars
  - Overhead door access on both ends
- Wheel Truing Track:
  - Truing machine (4-car length) with hoists on the remainder of the track
- Two (2) Shop Tracks with Hoists
  - Minimum capacity of 10 cars each
  - Overhead door access on both ends

#### General/Administration:

- Shop Office (must be located on 1st floor)
- Superintendent Office
- Conference/Classroom
- Lunch Room
- Pantry/Kitchen

- Janitor Closets (2)
- Building Maintenance
- Grounds Maintenance
- Track Maintenance
- Cab Signal Maintenance
- Yard Master Tower

#### Toilet/Lockers:

- Main Men's Locker Room
- Main Women's Locker Room
- Maintenance Men's Locker Room
- Maintenance Women's Locker
- Office Unisex Toilet

#### Mechanical/Electrical:

- Mechanical Equipment Room
- Electrical Equipment Room
- Communications/IT Room

#### Storage Areas:

- General Building Storage
- Small Part Storage
- Hazardous Material Storage
- Oil Storage

## STRUCTURAL

The foundation and slab system shall be designed by a structural engineer based on recommendations from the geotechnical engineering report. The structural framing system shall be designed in accordance with generally accepted engineering principles. The structural system shall be selected so that the chemicals and materials used in the shop and wash track shall not have a negative impact on the structural systems performance.

## MEP

### **Mechanical**

The HVAC system shall be selected to be energy efficient, meet all indoor air quality and thermal comfort requirements and must be straightforward to operate and maintain. Incorporation of renewable energy sources into the design of the building is recommended where possible. An appropriate system includes wind turbines and photovoltaic panels capable of generating electric energy to offset the building's electrical load.

## Electrical

Incoming electrical service should be discussed with the CTA and selected to meet the facility needs. The size of the electrical service equipment shall be determined by a Professional Engineer to conform to the City of Chicago Building Code and the National Electrical Code. Interior Lighting shall be provided to meet the requirements of the occupants and utilize a minimum number of separate lamps for building maintenance.

## **Plumbing/Fire Protection**

The building shall provide restrooms facilities capable of meeting CTA requirements and plumbing and accessibility codes. Low water consumption fixtures should be selected. Selection of toilet accessories, partitions and lockers should be durable and easy to maintain. The design shall include fire protection according to applicable codes including heat and smoke detectors, and associated alarm sirens and strobes. The fire alarm system shall be integrated with the City of Chicago security annunciation system to alert proper authorities. An automatic sprinkler system shall also be included. The fire alarm system will be of addressable type, and be in accordance with the City of Chicago Building Code, National Fire Protection Association and the requirements of the Bureau of Fire Prevention, whichever is more stringent.

### Communications

The provisions for information technology shall be coordinate with the CTA IT department. The data system shall be a combination of direct cable and wireless.

## Automation, Measurement & Verifications

The building shall include a Building Automation System (BAS). The systems that tie into the BAS system shall be discussed with the CTA. The controls system shall integrate to a Graphical User Interface (GUI). A measurement and verification system to monitor energy use should be considered and shall be discussed with CTA.

## SECURITY

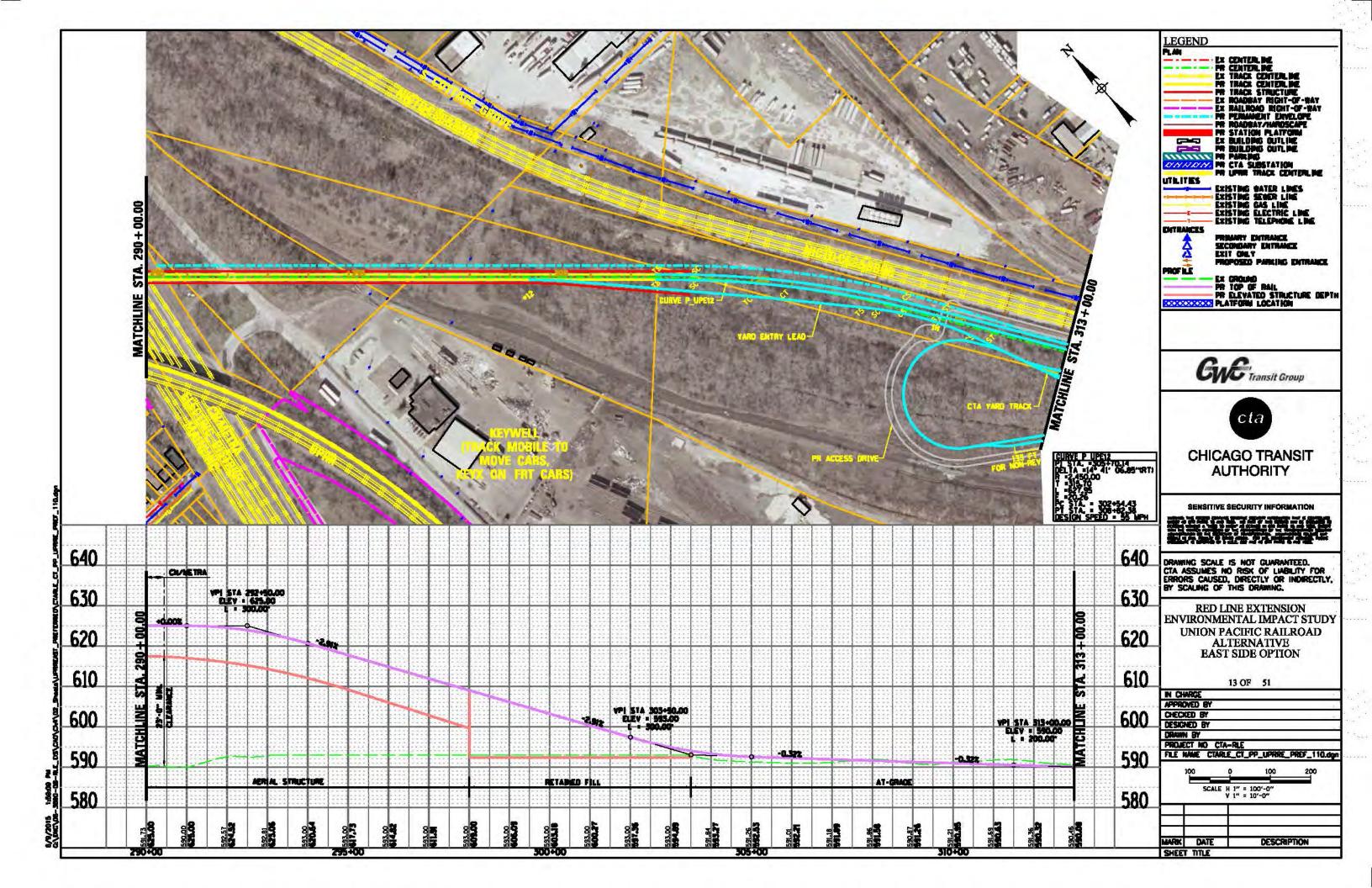
The security system shall consist of intrusion detection/access control and Surveillance System. The security system will be monitored, if required, by a third party security contract, or with a direct link to the Chicago Police Department local precinct. A direct link to the CTA Control Center will also be included. The surveillance system shall be provided with Raceways, Closed-

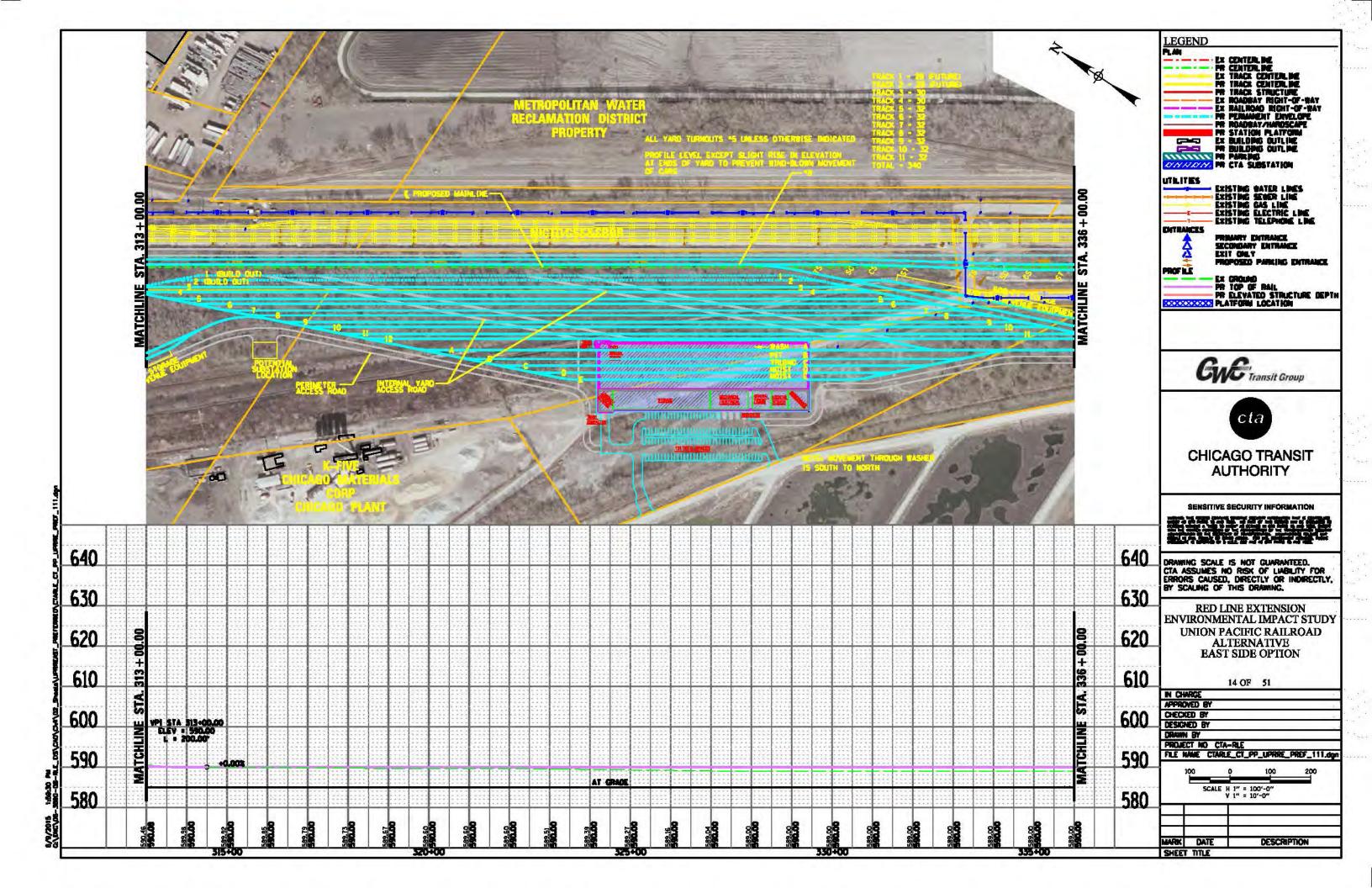
Circuit Television (CCTV)-UTP cables, jacks, patch panels, patch cords, high resolution Pan Tilt Zoom (PTZ) cameras, infrared cameras, multiplexes, monitors, Digital Video Recorder (DVR), interface modules, fiber distribution and all appurtenances for a complete surveillance system. The surveillance system coverage shall include but not be limited to, the building interior, the rail yard, loading areas and parking areas. Exterior cameras shall have the capability to send the video signal to the OEMC (Office of Emergency Management and Communications) upon request as a part of the Third Party Initiative Program. Camera selection shall be coordinated with OEMC for their compatibility.

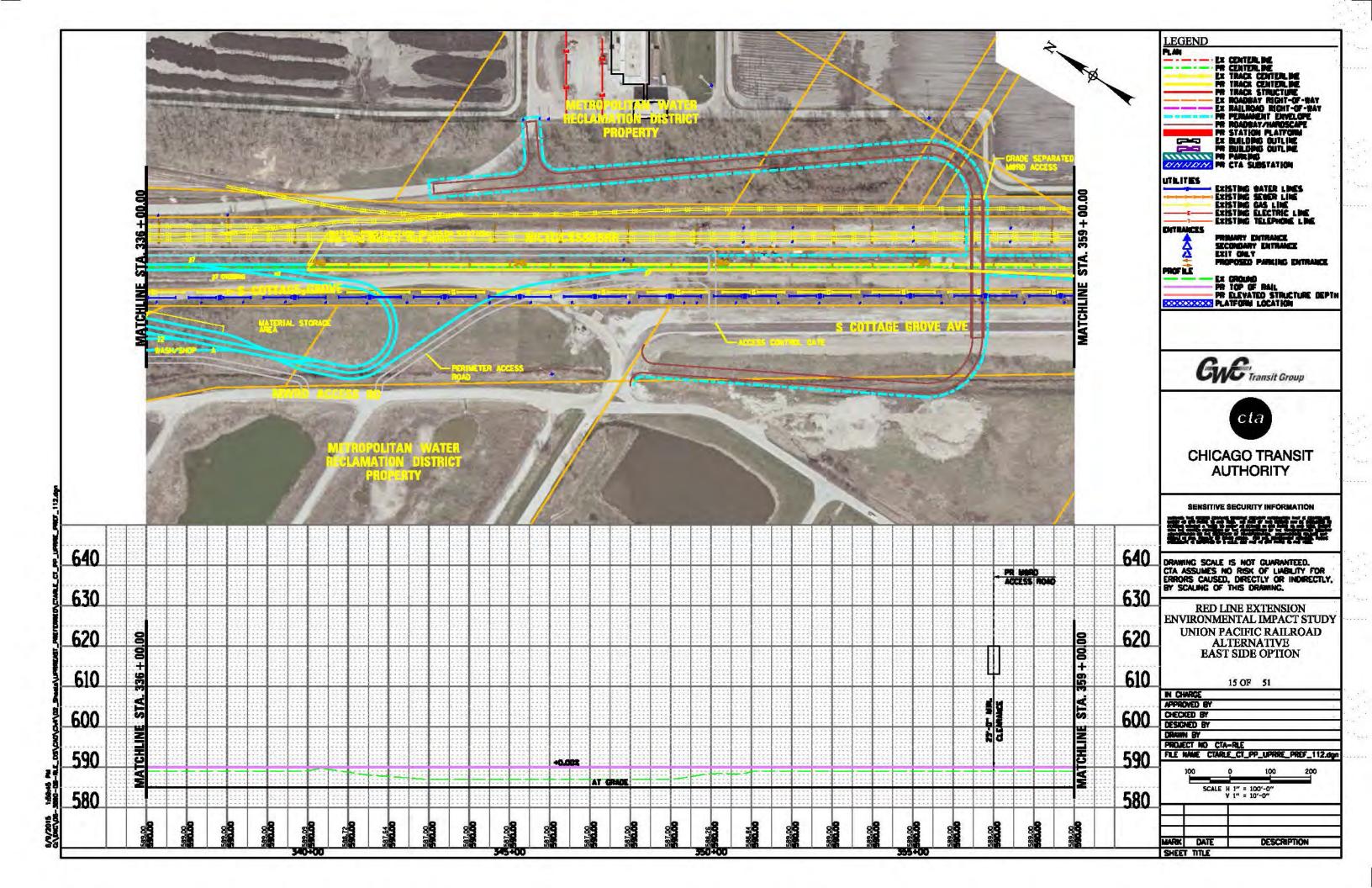
Attachments:

- Plan view conceptual schematics of yard and shop
- Yard capacity detail
- Flow schematics (into and out of the yard, and internal movements)
- Yard Storage Track Spacing Alternatives

cc: File







### **RLE Yard Storage Requirements**

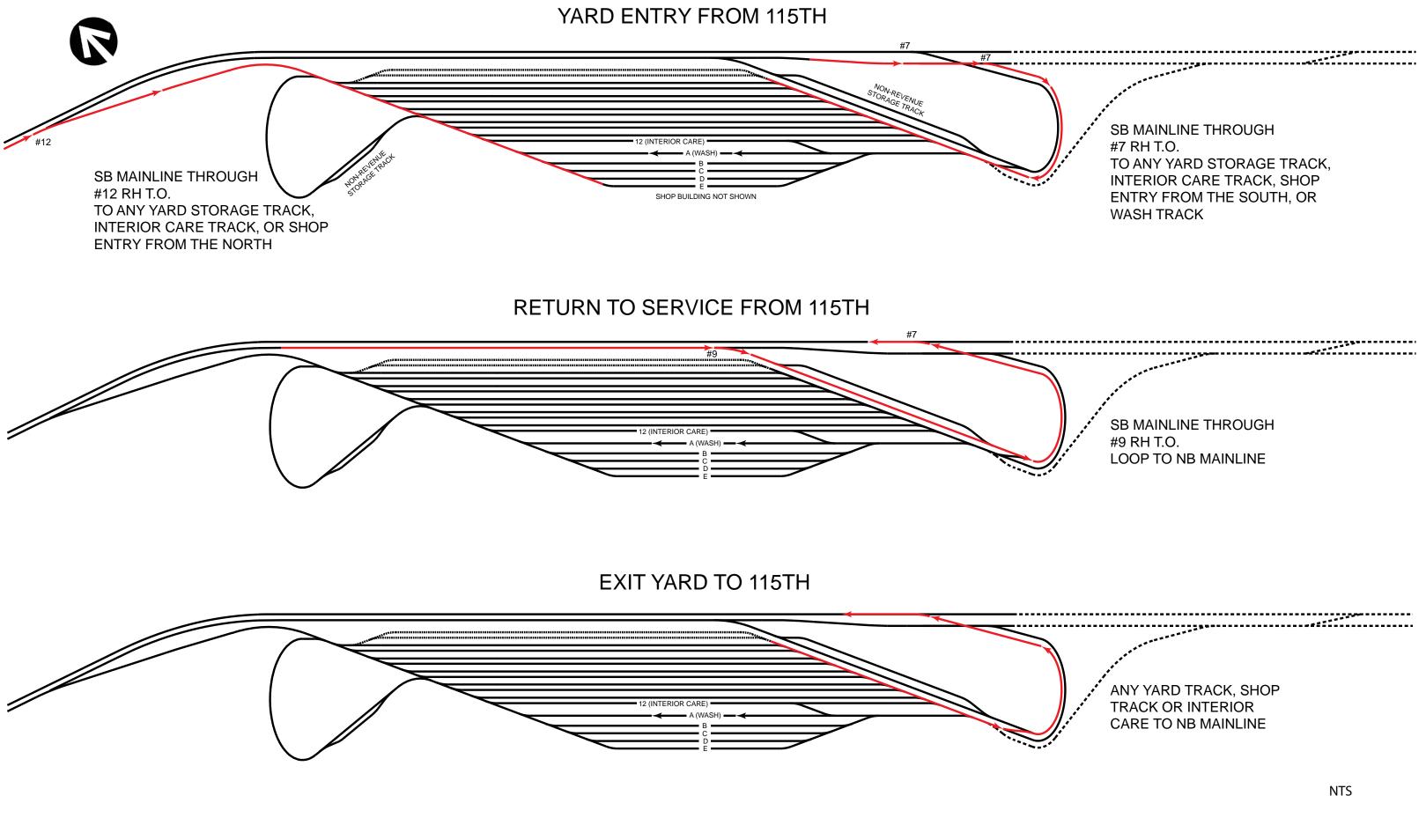
Ultimate Build-Out			
Assumption	IS:		
	Store 80 cars (8 x 10-car trains) fo	r RPM ultimate build-out	
	Replace maximum storage in 98th	n Yard	
	Store 78 additional cars for RLE op	peration	
	The minimum number of Red Line	e cars in operation (due to 7/24 service) is 40 cars (10 trains x 4 cars)	
	Do not assume any trains stored o	on yard leads	
	Discount storage requirements by	12 cars (~3%) for cars in shop undergoing maintenance, wheel truing, washing or interior of	leaning
RPM	80 cars		
98th Yard	234 cars		
RLE	78 cars		
Total	392 cars	5 Storage Tracks @ 32 cars (2 trains x 10 cars/train + 2 trains x 6 cars/train) +	
In Service	(40) cars	6 Storage Tracks @ 30 cars (3 trains x 10 cars/train) = 340 cars	
In Shop	(12) cars		
Storage	340 cars		

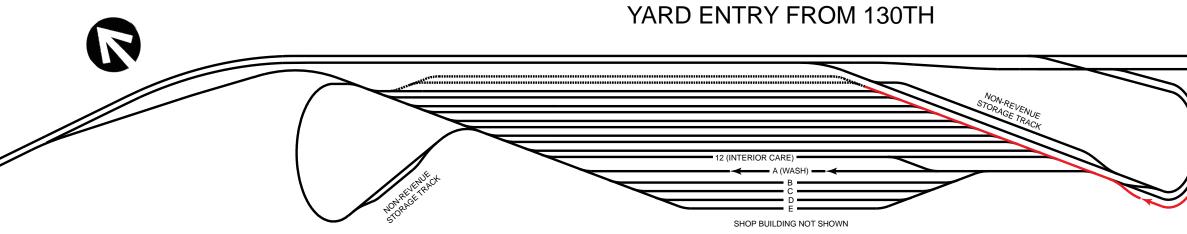
Assumptions	:	
St	ore 40 cars (5 x 8-car tra	ins) for RPM Phase 1
R	eplace maximum storage	e in 98th Yard
St	ore 64 additional cars fo	r RLE operation to 115th Street
Т	ne minimum number of	Red Line cars in operation (due to 7 day/24 hour service) is 40 cars (10 trains x 4 cars)
D	o not assume any trains	stored on yard leads
D	iscount storage requiren	nents by 10 cars (~3%) for cars in shop undergoing maintenance, wheel truing, washing or interior cleaning
RPM	40 cars	
98th Yard	234 cars	
98th Yard RLE	234 cars 64 cars	
		9 Storage Tracks @ 32 cars (4 trains x 8 cars/train) = 288 cars
RLE	64 cars	9 Storage Tracks @ 32 cars (4 trains x 8 cars/train) = 288 cars
RLE Total	64 cars 338 cars	9 Storage Tracks @ 32 cars (4 trains x 8 cars/train) = 288 cars

For Estimating Yard Size Only

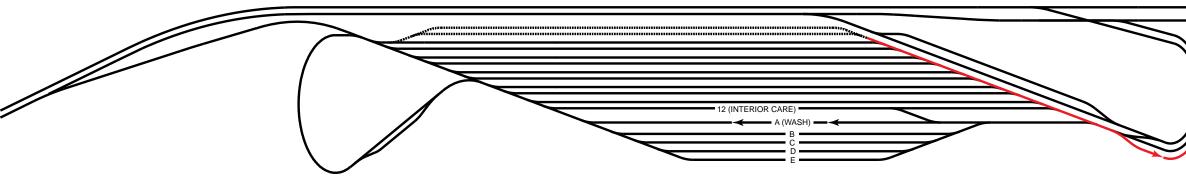
At night when 4-car trains are operating, ten 10-car trains are broken into ten 4-car trains (in service) plus ten 6-car trains which need to be stored

At night when 4-car trains are operating, five 8-car trains are broken into ten 4-car trains - no need to store partial trains

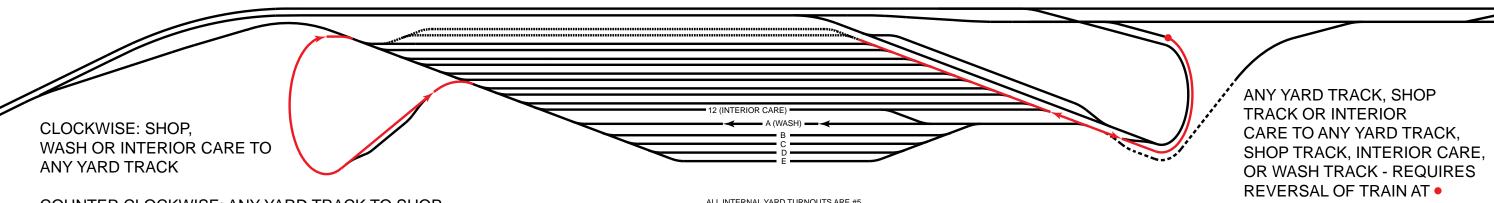




EXIT YARD TO 130TH



## **INTERNAL YARD MOVEMENTS**

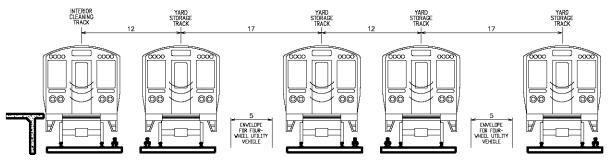


COUNTER CLOCKWISE: ANY YARD TRACK TO SHOP (NORTH ENTRANCE) OR INTERIOR CARE

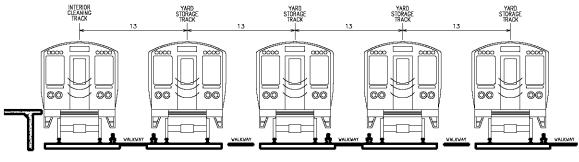
ALL INTERNAL YARD TURNOUTS ARE #5

NB MAINLINE THROUGH **#9 CROSSOVER TO TURNOUT** TO ANY YARD STORAGE TRACK, INTERIOR CARE TRACK, SHOP ENTRY FROM THE SOUTH, OR WASH TRACK

ANY YARD TRACK, SHOP TRACK, OR INTERIOR CARE TRACK TO SB MAINLINE TO 130TH STATION



POTENTIAL YARD TRACK LAYOUT WITH ROOM FOR UTILITY VEHICLE BETWEEN EVERY OTHER TRACK



POTENTIAL YARD TRACK LAYOUT WITH WALKWAY BETWEEN EACH TRACK



## Memorandum

То:	Sonali Tandon
From:	CWC, Wight & Company
Date:	February 19, 2015, revised March 16, 2015
Subject:	130th Street Terminal Station Options

The purpose of this memorandum is to provide a decision support document for the elimination of one of the station options for the extension of the Red Line to 130th Street. To date, the Union Pacific Railroad (UPRR) Rail Alternative has included two options for the 130th Street terminal station: the South and West Station Options. FTA has indicated that CTA should prepare the Draft Environmental Impact Statement (EIS) for the Red Line Extension (RLE) with the preferred alternative clearly identified to streamline the environmental process. To streamline the Draft EIS, CTA would like to present one terminal station in the Draft EIS; this memorandum serves as documentation.

This memorandum briefly describes the station options and evaluates the pros and cons of each option. This memorandum also provides a recommendation for the preferred station location.

### 130th South Station Option (Figure 1)

The South Station Option would be located immediately west of the Northern Indiana Commuter Transportation District/Chicago South Short & South Bend Railroad (NICTD/CSS&SBRR) right-of-way at 130th Street. The station and platform would be at grade. The station area would have three tracks running to the station. The platform would extend underneath 130th Street and would terminate before 130th Place, south of 130th Street. A total of 2,300 parking spaces are proposed in a seven-story parking structure. Pedestrians would access the station from 130th Place or from the parking structure. Vehicles would access the parking structure from 130th Street. Pedestrians from Altgeld Gardens would access the station from 130th Place, not needing to cross 130th Street.

### 130th West Station Option (Figure 2)

The West Station Option would be located along the north side of 130th Street at Evans Avenue. The station and platform would be at grade. A total of 2,300 parking spaces are proposed in a four-story parking structure plus a surface parking lot. Pedestrians from Altgeld Gardens would access the station through a pedestrian cross walk on 130th Street. A traffic signal at 130th Street, marked crosswalk, and curb ramps would be provided as part of the project. 130th Street Terminal Station Options Revised March 16, 2015 Page 2

#### **Evaluation of Station Options**

The City of Chicago Department of Transportation conducted a Market/Access study for the 130th Street Station in 2010 which evaluated the two station options. The study built upon CTA's Alternatives Analysis from 2009 but did not provide a recommendation for the final location of the station. The results of the study indicated that the South Station Option provided the best opportunity to meet the goals of the 130th Street Gateway Vision. The 130th Street Gateway Vision is to develop 130th Street into a gateway for the City of Chicago as well as the greater Calumet Area. The study stated that the South Station Option would:

- Promote possible development of a joint intermodal station with NICTD
- Support future studies to extend the Red Line across the Little Calumet River
- Encourage the visioning conducted by the Chicago Housing Authority for Altgeld Gardens
- Establish pedestrian and bicycle accessibility between 130th Street Station and the City's proposed Kensington Bikeway, Carver Military Academy and Altgeld Gardens
- Provide a potential catalyst for other public and private investment in the study area and as identified in the City's plans for the Calumet Region.

Stakeholders from the project area have indicated a preference for the South Station Option, as indicated in *Summary Report of the Developing Communities Project Community Visioning Session for the Proposed Red Line Extension*, compiled in September 2010. The master planning process undertaken by the Chicago Housing Authority (CHA) for the Altgeld Gardens Murray Homes in 2013 included evaluating the existing conditions, land uses in the planning area, and community facilities. The comprehensive master planning process recommended the South Station Option because it provided better connectivity to the Community Center and Ellis Avenue, which is a major north-south street that includes proposed retail. Stakeholders also indicated their preference for the South Station Option during the CTA public informational meeting held May 13, 2014.

The South Station Option is on tangent track leading to/from the proposed yard, which would be north of the station. To access the West Station Option, the tracks would curve to the west. In addition to the curved tracks, the cost of approximately 1100 feet of additional track needed for the West Station Option increases the cost over the South Station Option by nearly \$30 million, assuming all other amenities to the station would be of equal value. The additional track and curve leading to the West Station Option would create higher operational costs over the South Station Option.

Based on the results of the environmental analysis performed for the RLE Draft EIS in 2012–2015, the West Station Option would have an impact on pedestrian safety; however, the

130th Street Terminal Station Options Revised March 16, 2015 Page 3

proposed traffic signal at 130th Street and the pedestrian crosswalk would mitigate the impact. Both station options have the potential for wetland impacts. Fieldwork would be required at either location to determine if jurisdictional wetlands are present. Both station options would have traffic impacts related to the park & ride facilities off of 130th Street. Mitigation measures to minimize the traffic impacts include optimizing the traffic cycles and adding in turn-lanes. Both station options would also require land acquisition from the Metropolitan Water Reclamation District of Greater Chicago (MWRD).

The South Station Option would better serve the Carver Military Academy High School, located east of the station area, south of 130th Street. Students and faculty of the Carver Military Academy would be able to access the station through Doty Avenue. The South Station Option would be closer to the Ford Motor Company on Torrence Avenue.

The South Station Option would provide the best opportunity for a future connection to NICTD, because the NICTD tracks would parallel the proposed RLE at this station. Improving regional rail connections and linkages to other public transportation modes in the project area is one element of the RLE Project's purpose and need. The West Station Option would not accommodate a direct regional rail connection to other public transportation.

During a meeting with MWRD on September 16, 2013 regarding the proposed RLE yard and station options, MWRD indicated that they do not have future plans for any of the parcels where the station options are proposed. MWRD indicated that the vacant parcels are intended to act as buffers (both visually and for odors) for the lagoons to the surrounding communities. MWRD tentatively indicated that the South Station Option would be better than the West Station Option.

### Recommendations

Based on the evaluation of station options, input from the local community, and project costs, the South Station Option is the preferred station location for the 130th Street Terminal Station. The South Station Option would provide benefits to the project and would best meet the project's purpose and need of providing other transit connections to NICTD in the future.

cc: CWC, File